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BONITA SPRINGS, FLORIDA
COMMUNITY DEVELOPMENT DEPARTMENT
ZONING DIVISION
STAFF REPORT

TYPE OF CASE: REZONE – COMMERCIAL PLANNED
DEVELOPMENT/RESIDENTIAL PLANNED DEVELOPMENT
(CPD/RPD)

CASE NUMBER: PD21-83966-BOS

HEARING DATE: June 21, 2022

PLANNER: Mary Zizzo, Esq., AICP, Planner II

APPLICATION SUMMARY:

- A. Applicant: Madison Capital Group Management, LLC
- B. Agent: D. Wayne Arnold, Q. Grady Minor & Associates, P.A.
Neale Montgomery, Pavese Law Firm
- C. Request: A request to rezone 20.99± acres from Commercial Planned Development (CPD) to Commercial Planned Development/Residential Planned Development (CPD/RPD) to allow 252 multi-family dwelling units, including bonus density units, and up to 10,000 square feet commercial use.
- D. Location: The subject property is located at 27801, 27802, 27825 27853, 27877, 27901, 27925, 27952, 27977 Boondock Rd and 27750 Orr Rd, Bonita Springs, FL 34135. STRAPs: 31-47-26-B4-00007.0290, 31-47-26-B4-00007.0190, 31-47-26-B4-00007.0140, 31-47-26-B4-00007.0310, 31-47-26-B4-00007.0100, 31-47-26-B4-00007.0220, 31-47-26-B4-00007.0110, 31-47-26-B4-00007.0510, 31-47-26-B4-00007.0300, and 31-47-26-B4-00007.0130
- E. Future Land Use Map Designation: Interchange Commercial
- F. Current Zoning: Commercial Planned Development (CPD)
Interstate Zone of the Bonita Beach Road Corridor Overlay
- G. Current Land Use: Vacant Land

By this reference, the Applicant's application in its entirety and correspondence is made part of this record and is available at the City Clerk's and Community Development's Offices.

BACKGROUND:

History and Overview

The subject properties are currently vacant, within the Bonita 75 CPD. The Applicant is a contract purchaser requesting to rezone the property to Residential Planned Development/Commercial Planned Development (RPD/CPD), proposing a 252-unit apartment complex, including 47 density bonus units, and a commercial tract. Staff recommends denial of the density bonus units for lack of information at the time of preparation of this Report. The property is located within the Interstate Zone of the Bonita Beach Road Corridor Overlay.

The current zoning on the property was approved under Zoning Ordinance 08-10, as amended in ADD09-BOS00006 and ADD15-23355-BOS. The approved master concept plan has been extended through various emergency orders in accordance with Florida Statutes. Approved uses include multiple Fast Food Restaurants; Rental or Leasing Establishments, Group II; Repair Shops, Groups I, II, and III; Social Services, Group I; Indoor Storage and Used Merchandise Stores, Groups I and II.

Pursuant to LDC Section 4-272, residential uses within the Interchange Commercial Area of the Bonita Plan require a major planned development rezone. An approval of this rezone would create a new schedule of uses, master concept plan and property development regulations, voiding the previously approved zoning ordinance.

The proposed project is located within the Bonita Beach Road Corridor Overlay, which was adopted in Ordinance 19-10. Part of the stated purpose and intent of the overlay is to foster urbanized development patterns that focus on interconnectivity, mobility, human scale, and various design and architectural standards, among other items. Of the four zones created by the overlay, this project is within the Interstate Zone.

Development regulations are contained later in this report as part of **Condition 3**, and are conditioned to keep human scale in mind, as supported by the overlay.

The request seeks six (6) deviations, which are analyzed further below.

Uses

Requested uses are included in **Attachment "B."** The commercial tract uses are generally consistent with what would be permitted by right within the Interstate Zone of the Bonita Beach Road Corridor Overlay. The residential tract uses are those generally consistent with those permitted for residential developments. Staff's recommendations on the requested uses are enumerated in **Condition 2**.

Property Development Regulations and Deviations

Development is to be conducted in accordance with the Land Development Code (LDC) Chapters 3 (Development Standards) and Chapter 4 (Zoning). Conditions and deviations set forth in the adopted zoning ordinance may augment the standard development regulations such as property development standards, buffering, deviations and any associated conditions of approval.

The development regulations are included in **Condition 3**. Deviations may be requested during the review process in accordance with [LDC Sec. 4-326](#) and [LDC Sec. 3-81\(b\)](#). The current

planned development request includes six (6) deviations. Staff's analysis and recommendation on the deviation requests are included later in this Staff Report. The deviation request and associated justification by the Applicant is included in the Applicant's Analysis in **Attachment "B."** Of note is that the Applicant has requested to deviate from the Bonita Beach Road Corridor Overlay standards.

Comprehensive Plan Considerations

This project was reviewed for compliance with the City of Bonita Springs Comprehensive Plan, including future land use, transportation, conservation/coastal management, and infrastructure elements. The project is located within the Interchange Commercial future land use category. Additional Staff Analysis is included in **Attachment "A."** The Applicant's Analysis is included in **Attachment "B."**

Transportation Summary Analysis

The project is designed to provide multiple modes of transportation, including sidewalks and vehicular roads. The site is located near a bus stop. As conditioned and as consistent with Chapter 3, the Applicant will be required to improve Miller Road and portions of Orr Road for access to complete streets standards.

Infrastructure Summary Analysis

The City's potable water and sanitary sewer utility franchise, Bonita Springs Utilities, has reviewed the request and did not raise any concerns regarding potential burdens on infrastructure, or infrastructure availability. As a result, it is the Staff opinion that the project is consistent with the infrastructure element of the Bonita plan.

CONCLUSIONS:

The following conclusions are based upon the Applicant's application being reviewed for compliance with the City of Bonita Springs Comprehensive Plan and the LDC. **Attachment "A,"** which is attached hereto and made a part hereof, demonstrates the type of analysis that was done.

Pursuant to the City's LDC, the Applicant is required to hold two (2) neighborhood meetings. The first meeting was held on June 28, 2021, with four members of public present. The Applicant held their second meeting on May 17, 2022, with four members of public present. At both meetings, concerns of project access and construction timelines were discussed. Additional methods of public notice included mailed notices to properties within 375 feet of the property, property-posting signs along rights-of-way, and a legal ad in the Fort Myers News-Press for the Zoning Board public hearing.

The rezoning request was evaluated by Community Development for planning, zoning, engineering, environmental, and transportation impacts. Florida Department of Transportation, Lee County Department of Transportation, Bonita Springs Utilities and the Bonita Springs Tree Advisory Board also reviewed the request. The proposed development, as conditioned, is consistent with the requirements and standards of the City of Bonita Springs Comprehensive Plan and LDC. A detailed City Staff analysis is included in **Attachment "A"** of the Staff Report.

RECOMMENDATION:

Staff recommends **APPROVAL, with conditions**, of Petition PD21-83966-BOS, Bonita 75 Residential Planned Development/Commercial Planned Development (RPD/CPD), which proposes to rezone 20.99 +/- acres from Commercial Planned Development (CPD) to a Residential Planned Development/Commercial Planned Development (RPD/CPD). This recommendation of APPROVAL is subject to the following conditions:

Conditions:

1. The project shall be consistent with the Master Concept Plan titled “Bonita 75 CPD/RPD Master Concept Plan” prepared by Q. Grady Minor and Associates, P.A., dated June 1, 2022, attached hereto and made a part hereof (**Exhibit “B”**).

2. Schedule of Uses:

Residential Tract (RPD):

- a) Accessory Uses and Structures
- b) Administrative Offices (Leasing Office)
- c) Dwelling Units, Multiple-Family (not to exceed 205 units, unless a bonus density agreement is approved by City Council, not to exceed 252 units). The dwelling unit mix is limited to a maximum of 50% one-bedroom units, 40% two-bedroom units and 10% three-bedroom units.
- d) Essential Service Facilities, Group I
- e) Excavation, Water Retention
- f) Fences, Walls
- g) Garage, Private
- h) Gatehouse(s)
- i) Recreational Facilities, Private On-Site
- j) Signage
- k) Storage, Indoor

Commercial Tract (CPD) (not to exceed 10,000 square feet):

- a) Accessory Uses and Structures
- b) Administrative Offices
- c) ATM (Automatic Teller Machine)
- d) Auto Parts Store
- e) Banks and Financial Establishments, Group I
- f) Boat Parts Store
- g) Business Services, Groups I and II
- h) Clubs, Private
- i) Consumption on Premises, Indoor only, in conjunction with a full-service restaurant or micro-brewery. Outdoor consumption requires a special exception.
- j) Contractors and Builders, Groups I and II
- k) Daycare Centers, Child, Adult
- l) Drug Store, Pharmacy
- m) Dwelling Unit (ancillary only); Multiple-Family (not to exceed the maximum density permitted for the PD)
- n) Entrance Gates and Gatehouse
- o) Essential Service Facilities, Group I
- p) Excavation, Water Retention
- q) Fences, Walls
- r) Food Stores, Group I

- s) Hardware Store, no outdoor storage or display
- t) Healthcare Facilities, Groups I and II
- u) Household and Office Furnishing, Group I
- v) Laundry or Dry Cleaning, Group I
- w) Lawn and Garden Supply Store, no outdoor storage or display
- x) Micro-brewery
- y) Office, business
- z) Paint, Glass and Wallpaper, no outdoor storage or display
- aa) Parking lot, accessory and garage, private
- bb) Personal Services, Groups I, II, and III
- cc) Pet Service
- dd) Rental or Leasing Establishment, Group I
- ee) Residential Accessory Uses
- ff) Restaurant, Fast Food
- gg) Restaurant, Groups I, II, III and IV
- hh) Specialty Retail: Groups I, II and III
- ii) Storage, indoor only
- jj) Studios
- kk) Temporary Uses
- ll) Used Merchandise Store, Group I
- mm) Variety Store
- nn) Wholesale Establishment, Group III

3. Property Development Regulations:

a. RPD Area:

i. Minimum Lot Dimensions:

- 1. Minimum Lot Area: N.A.
- 2. Minimum Lot Depth: N.A.
- 3. Minimum Lot Width: N.A.

ii. Minimum Setbacks:

- 1. Street/Front (Public Road): 25 feet
- 2. Street/Front (Private Ingress/Egress Easement): 20 feet
- 3. Internal Driveway: 10 feet
- 4. Side Yard: 10 feet
- 5. Rear Yard: 20 feet
- 6. Northern RPD Boundary: 20 feet
- 7. Waterbody:

- a. Principal: 25 feet
- b. Accessory: 5 feet*

*(10 feet where bank protection is not present)

- 8. Preserve: 30 feet

iii. Maximum Building Height: 60 feet**, 4-stories

** (subject to LDC Section 4-1874, as amended)

iv. Minimum Open Space: 40 percent

b. CPD Area:

i. Minimum Lot Dimensions:

- 1. Minimum Lot Area: 10,000 square feet
- 2. Minimum Lot Depth: 100 feet
- 3. Minimum Lot Width: 100 feet

- ii. Minimum Setbacks:
 - 1. Street/Front (Public Road): 25 feet
 - 2. Street (Private Road): 20 feet
 - 3. Drive Aisle: 10 feet
 - 4. Side Yard: 15 feet
 - 5. Rear Yard: 20 feet
- iii. Maximum Building Height: 35 feet, 2-stories
- iv. Minimum Open Space: 20 percent
- 4. Parking. In support of deviation 6, 1.8 parking spaces per unit minimum are required. Five additional spaces are required for the leasing office.
- 5. Environmental
 - a. Twenty-two (22) heritage trees have been identified on the site. The Tree Advisory Board approved twelve (12) trees within the development area to be removed. The project is required to have a second Tree Advisory Board hearing to determine if any heritage or other trees can be retained within the parking or development area, and to determine the mitigation for any heritage trees that necessitate removal.
 - b. Native trees and palms found on site shall be preserved within the 2.64 acres marked Preserve/Open space on the Master Concept Plan. At time of the first local development order, the Developer shall demonstrate the retention of 45 native trees and the transplant and/or planting of 15 native trees within open space areas of the site per Right Tree Right Place practices. Additionally, a plan for the management of invasive non-native and nuisance vegetation and debris/structure removal shall be provided for these areas.
 - c. If any archeological/historical sites are uncovered during development activities, all work in the immediate vicinity of such sites shall cease. The Developer will immediately contact the Florida Department of State, Division of Historical Resources and the City of Bonita Springs and advise them of the discovery. The Developer will have a State-certified archaeologist determine the significance of the findings and recommend appropriate mitigation actions, if necessary.
 - d. In order to aid in reducing negative impacts to the water quality of the Imperial River, at time of local Development Order, the Applicant shall stabilize and revegetate the Floodplain Compensation Area(s) with approved native vegetation in a manner consistent with the provisions of UF/IFAS Florida-Friendly Landscaping Principles and Bonita Springs City Code 24-46(1) relating to activities requiring a FEMA No-Rise Certification.
- 6. Landscaping
 - a. No right-of-way easement shall overlap a designated landscape buffer or preserve area.
 - b. The type D buffer, as depicted on the Master Concept Plan, between Orr Road and Bonita Beach Road, and Orr Road and the I-75 northbound entrance and the I-75 rights-of-way, must be enhanced to provide minimum 14' canopy trees ranging from 4"-5" caliper, measured at 4.5' above the ground. Palms must be clustered and planted in staggered heights, a minimum of three palms per cluster, with a minimum of four (4') foot difference in height between each tree. Shrubs must be 24" in height at time of planting.

7. Transportation and Multimodal
 - a. Approval of this zoning request does not address mitigation of the project's vehicular or pedestrian traffic impacts. Additional conditions for on-site improvements consistent with the City of Bonita Springs Land Development Code may be required to obtain local development order; and
 - b. At the time of local Development Order, the Applicant shall provide an intersection study for Trade Way One and Bonita Beach Road to analyze the effect of the eastbound U-turns at Trade Way One with left-turns prohibited at Miller Road. The intersection study shall determine the maximum vehicle queue in the eastbound left-turn lane at Trade Way One at project build-out.
 - c. At the time of local Development Order, all required multimodal facilities (infrastructure, crossings, amenities, furnishings, access points, easements, etc.) both internal and external to the site, shall meet or exceed the intent of the design standards provided by Chapter 3 of the City of Bonita Springs Land Development Code, the City of Bonita Springs Bicycle Pedestrian Master Plan (PATH), the Bonita Beach Road Corridor Design Standards, and all applicable design standards except as modified herein, and as required by the Bonita Springs Fire Control and Rescue District and National Fire Protection Association (NFPA).
 - d. Should the project's Northwestern (emergency only) ingress/egress be utilized/alterd (either during the project's local Development Order design/construction process or later at/after project completion) for use by residents and/or the general public, or should another residential/public ingress/egress access point be created along Orr Road, the requisite complete streets facilities must be constructed along/within the otherwise not improved segment(s) of Orr Road (see Deviation #5), extending from the said ingress/egress access(es) to the proposed/existing complete streets infrastructure being constructed by the Applicant. The same shall apply if any physical work is commenced on the proposed relocated Orr Road.
 - e. At time of local Development Order, the required sidewalk connection along Bonita Beach Road shall be provided for, unless an alternative option is approved at that time.
8. Access.
 - a. At the time of local Development Order, the Applicant shall install a raised island at the intersection of Bonita Beach Road and Miller Road to provide for a right-in, right-out maneuver only.
 - b. The Applicant shall provide for an amended ingress/egress utility easement for Orr Road.
9. This Master Concept Plan is subject to conditions set forth herein and the rules, regulations laws and codes in place at the time of Development Order and Building Permit approval.
10. Approval of this Planned Development is not a guarantee of future approvals.

Deviations:

Deviations may be requested during the review process in accordance with [LDC Section 4-326](#). The Zoning Board may recommend to approve, approve with modification, or reject each requested deviation based upon a finding that each item:

1. Enhances the achievement of the objectives of the planned development; and

2. Preserves and promotes the general intent of this chapter to protect the public health, safety and welfare.

The current planned development request includes six (6) deviations.

1. Deviation (1) requests relief from LDC Section 3-331(d)(1), which states that no excavations are permitted within fifty feet of any private property line under separate ownership unless granted an administrative deviation in accordance with Section 3-81, in which a deviation to allow for up to a 25-foot setback for an excavation from a property line. The request is to allow an administrative deviation to allow a 15-foot setback for an excavation from a private property line.

Applicant's Justification: The property on the north side of the lake is zoned AG-2 and requires only a Type "B" buffer. In an effort to accommodate the land use to the north, the lake area will function as a buffer to the [proposed multiple-family buildings and] commercial [uses] to the south. The excavation will likely be necessary for floodplain compensation per the anticipated SFWMD [permit] consistent with the previously issued permit. Necessary structures, such as a fence, will be provided at the discretion of the City Manager or his designee.

Based on the Applicant's request and justification, Staff recommends **APPROVAL** of the deviation request, with the condition that a fence be required if this administrative action is sought.

2. Deviation 2 seeks relief from LDC Section 4-899, site design standards, to allow development within the CPD/RPD to be consistent with the Development Standards proposed for this CPD/RPD.

Applicant's Justification: The project physically cannot comply with the maximum building setback and other site design standards in LDC 4-899 due to the presence of Orr Road. The entrance to the CPD/RPD is at the intersection of Orr Road and Miller Road, which is approximately 85 feet to the north of the Bonita Beach Road travel lanes which exceeds the maximum setback of 65 feet permitted in the Bonita Beach Road Interstate Zone. The project will also not be developed with internal streets as the residential component will be a gated rental apartment complex. The proposed development standards will permit development of the proposed uses in a manner necessary for this type of mixed-use development and the deviation will have no negative impact on the public.

Based on the Applicant's request and justification, Staff recommends **APPROVAL** of the deviation request from street setbacks and block patterns. Staff recommends **DENIAL** of the request to deviate from the remainder of the Section. It is Staff's opinion that the project should make every effort to comply with the other portions of the Overlay to further the intent of the Overlay and to promote public health, safety and welfare with street amenities and safe pedestrian facilities to navigate between the residential and commercial components of the site. The Bonita Beach Road Corridor Overlay is intended to support urbanized development patterns that focus on human-scale development, an appropriate mixture of land uses, site design, interconnectivity, mobility, architectural standards, and a vibrant and aesthetically-pleasing streetscape. The project does have frontage along Bonita Beach Road and is visible from Bonita Beach Road. The Applicant

has failed to justify how this deviation works to preserve and promote the general intent of the chapter to protect the public health, safety and welfare. The City has worked tremendously on the Corridor regulations, and it is Staff's opinion that they should not all be dismissed based solely on product type chosen by an Applicant.

3. Deviation 3 seeks relief from LDC Sections 6-112 and 6-113, permanent signs in residential and commercial areas, to allow for up to two shared entrance signs/project signs for the residential and commercial tracts. The signs may be pole or monument signs, and if developed as a pole sign, the pole must have a cover no less than 50% of the width of the sign. The shared sign(s) may be a maximum of 15 feet in height as measured from the finished grade and the maximum area shall not exceed 240 square feet. The sign must be setback a minimum of 5 feet from Orr Road or Miller Road.

Applicant's Justification: The [Residential Planned Development/ Commercial Planned Development] will have limited visibility due to the physical location of the project which is separated from Bonita Beach Road by Orr Road and the northbound entrance ramp to I-75. The physical separation from the nearest travel lanes on Bonita Beach Road warrant signage that is greater in height than currently allowed for projects located in the Bonita Beach Road Corridor Overlay. Sharing the project signage for the residential and commercial components will help reduce overall signage and given the small view corridors to the site, sharing the signage at strategic locations makes logical sense.

Based on the Applicant's request and justification, Staff recommends **DENIAL** of the deviation request. The Applicant has failed to justify how this deviation preserves and promotes the general intent of the chapter to protect the public health, safety and welfare. The proposed signage location along the I-75 on-ramp is not an appropriate location for a sign of that height, as the cars who view the sign are not able to turn around once they have accessed that point. Further, pole signs are prohibited throughout the City to achieve an overall aesthetic quality of the community and provide a more attractive business climate and make the City a more desirable place in which to visit, trade, work and live. Additionally, project(s) similar in nature and location do not have the same entitlement.

4. Deviation 4 seeks relief from LDC Section 3-418(d)(4), which requires a 15-foot-wide type 'D' landscape buffer adjacent to a ROW and instead allow a 10-foot-wide type 'D' buffer adjacent to the Orr Road ROW and ingress/egress/utility easement area.

Applicant's Justification: At the request of staff, the Orr Road alignment was revised to accommodate a better intersection from Bonita Beach Road and to alleviate stacking concerns on Miller and Orr Roads. Based on the original easement area for Orr Road and the existing pavement area, approximately 10' in width is available for planting. The Applicant is agreeable to plant material in the reduced buffer width if requested by staff.

Based on the Applicant's request and justification, Staff recommends **APPROVAL** of the deviation request subject to Condition 6.b.

5. Deviation 5 seeks relief from LDC Section 3-303, Complete Streets Design, for that portion of Orr Road located within the boundary of the CPD/RPD. That portion of Orr Road located between the RPD and CPD areas shall be a 40-foot-wide cross-section, with closed drainage and six-foot sidewalks on each side of the roadway as depicted in the Orr

Road Cross-Section, Exhibit A. The remainder of Orr Road shall remain in its current design with open drainage and no sidewalks.

Applicant's Justification: The property owner has agreed to relocate Orr Road in order to increase the intersection spacing from Bonita Beach Road, while also reducing a significant amount of commercial square footage, thereby reducing vehicular traffic associated with the CPD/RPD. Orr Road beyond the CPD tract serves three existing home sites, retention ponds owned by FDOT and vacant lands owned by the State of Florida. The existing Orr Road is in good physical condition and for the very low volume of traffic utilizing this portion of Orr Road, it does not [make] economic sense to modify the existing cross-section. Further, the RPD/CPD will have access from Miller Road. Only an emergency vehicle access will be provided on the older section of Orr Road.

Based on the Applicant's request and justification, Staff recommends **APPROVAL** of the deviation request, as conditioned. As recommended in Condition 7.d., should any physical work be done to Orr Road, and/or a resident or guest entrance or exit be located off of the unimproved portion of Orr Road, Complete Streets shall apply, including closed drainage and sidewalks to protect public health, safety and welfare.

6. Deviation 6 seeks relief from LDC Section 4-1732(1)c. Required parking for Multiple-Family buildings, which requires parking at a ratio of 1.5 spaces per unit for each 1 bedroom (BR) unit, 1.75 spaces per unit for each 2BR unit and 2 spaces per unit for a 3BR unit, with an additional 10% of the total required units to be guest parking, to instead allow a parking ratio of 1.8 spaces per unit as the required parking for the project.

Applicant's Justification: Madison Capital Group has developed a number of rental communities throughout the United States and has successfully developed communities having an average parking ratio of 1.8 spaces per unit, with no additional parking for guest spaces. Typical rental communities developed by Madison Capital Group have approximately 50% of the units as 1BR, 40% as 2BR and 10% as 3BR units. The 1.8 parking space per unit average has been utilized in the following three communities in Florida: Tampa, Apopka and Wesley Chapel.

Based on the Applicant's request and justification, Staff recommends **APPROVAL** of the deviation request, provided that the units are comprised of 50% 1 BR, 40% 2 BR and 10% 3 BR with five (5) additional spaces are provided for the leasing office.

SUBJECT PROPERTY:

The Applicant indicates the STRAP numbers are:

- 31-47-26-B4-00007.0290,
- 31-47-26-B4-00007.0190,
- 31-47-26-B4-00007.0140,
- 31-47-26-B4-00007.0310,
- 31-47-26-B4-00007.0100,
- 31-47-26-B4-00007.0220,
- 31-47-26-B4-00007.0110,

31-47-26-B4-00007.0510,
31-47-26-B4-00007.0300, and
31-47-26-B4-00007.0130

EXHIBITS:

- A. Legal Description and Sketch of the Subject Properties
- B. Master Concept Plan
- C. Narrative
- D. Schedule of Deviations and Justifications with Cross-Sections

ATTACHMENTS:

- A. Staff Informational Analysis
 - i. Background and Informational Analysis
- B. Applicant's Informational Analysis
 - i. Application
 - ii. Previous Zoning Approvals
 - iii. Area Location Map
 - iv. Aerial
 - v. Right-of-Way and Easement Map
 - vi. Existing Zoning Map
 - vii. Public Transit Routes Map
 - viii. Applicant's Narrative
 - ix. LDC Sec. 4-899 Mark Up
 - x. Special Flood Hazard Maps
 - xi. Proposed Site Development Regulations
 - xii. Proposed Schedule of Uses
 - xiii. Schedule of Deviations and Justifications
 - xiv. Traffic Impact Statement
 - xv. Surface Water Management Plan
 - xvi. Environmental Assessment
 - xvii. Historical & Archaeological Map
 - xviii. Variance Map
 - xix. Neighborhood Information Meeting 1 Submittal
 - xx. Neighborhood Information Meeting 2 Submittal
 - xxi. Tree Advisory Board Meeting Minutes
 - xxii. Traffic Memo Received 6/13/2022
 - xxiii. Conceptual Plan Received 6/13/2022

PROPERTY DESCRIPTION

A PARCEL OF LAND LOCATED IN THE SOUTHWEST QUARTER OF SECTION 31, TOWNSHIP 47 SOUTH, RANGE 26 EAST, LEE COUNTY, FLORIDA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGIN AT THE SOUTHEAST CORNER OF THE SOUTHWEST QUARTER OF SECTION 31, TOWNSHIP 47 SOUTH, RANGE 26 EAST, LEE COUNTY, FLORIDA; THENCE RUN SOUTH 88°57'15" WEST, ALONG THE SOUTH LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 31, FOR A DISTANCE OF 68.51 FEET TO A POINT ON THE EASTERLY RIGHT-OF-WAY LINE OF INTERSTATE 75 (STATE ROAD 93), AS THE SAME IS SHOWN ON THE STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION RIGHT-OF-WAY MAP FOR STATE ROAD No. 93 (I-75), LEE COUNTY, FLORIDA, SECTION 12075-2401, SHEET 5 OF 9, DATED 12/7/76; THENCE RUN NORTH 01°03'33" WEST, ALONG SAID RIGHT-OF-WAY LINE, FOR A DISTANCE OF 54.73 FEET; THENCE RUN NORTH 83°56'32" WEST, ALONG SAID RIGHT-OF-WAY LINE, FOR A DISTANCE OF 508.51 FEET; THENCE RUN NORTH 58°29'27" WEST, ALONG SAID RIGHT-OF-WAY LINE, FOR A DISTANCE OF 233.01 FEET; THENCE RUN NORTH 25°56'09" WEST, ALONG SAID RIGHT-OF-WAY LINE, FOR A DISTANCE OF 348.59 FEET; THENCE RUN NORTH 23°58'20" WEST, ALONG SAID RIGHT-OF-WAY LINE, FOR A DISTANCE OF 279.75 FEET; THENCE RUN NORTH 21°07'25" WEST, ALONG SAID RIGHT-OF-WAY LINE, FOR A DISTANCE OF 446.23 FEET; THENCE RUN NORTH 15°25'36" WEST, ALONG SAID RIGHT-OF-WAY LINE, FOR A DISTANCE OF 83.89 FEET TO A POINT ON THE NORTHERLY BOUNDARY OF THAT PARCEL OF LAND DESCRIBED IN PARAGRAPH 1 OF PARCEL FOUR AS RECORDED IN OFFICIAL RECORDS BOOK 4549 AT PAGES 4351 AND 4352 OF THE PUBLIC RECORDS OF LEE COUNTY, FLORIDA; THENCE RUN NORTH 88°57'15" EAST, ALONG THE NORTHERLY BOUNDARY OF SAID PARCEL OF LAND, FOR A DISTANCE OF 217.22 FEET TO A POINT ON THE WESTERLY BOUNDARY OF THAT PARCEL OF LAND DESCRIBED IN PARAGRAPH 3 OF PARCEL FOUR AS RECORDED IN OFFICIAL RECORDS BOOK 4549 AT PAGES 4351 AND 4352 OF THE PUBLIC RECORDS OF LEE COUNTY, FLORIDA; THENCE RUN NORTH 00°47'44" WEST, ALONG THE WESTERLY BOUNDARY OF SAID PARCEL OF LAND, FOR A DISTANCE OF 1.60 FEET TO A POINT ON THE NORTH LINE OF THE SOUTHEAST QUARTER OF THE SOUTHWEST QUARTER OF SAID SECTION 31; THENCE RUN NORTH 88°45'30" EAST, ALONG THE NORTH LINE OF THE SOUTHEAST QUARTER OF THE SOUTHWEST QUARTER OF SAID SECTION 31, FOR A DISTANCE OF 30.00 FEET; THENCE RUN SOUTH 00°47'44" EAST FOR A DISTANCE OF 383.80 FEET; THENCE RUN NORTH 88°57'20" EAST FOR A DISTANCE OF 299.65 FEET; THENCE RUN NORTH 00°44'39" WEST FOR A DISTANCE OF 130.00 FEET; THENCE RUN NORTH 88°57'20" EAST FOR A DISTANCE OF 299.68 FEET; THENCE RUN NORTH 00°44'45" WEST FOR A DISTANCE OF 255.87 FEET; THENCE RUN NORTH 88°45'30" EAST ALONG THE NORTH LINE OF THE SOUTHEAST QUARTER OF THE SOUTHWEST QUARTER OF SAID SECTION 31, FOR A DISTANCE OF 30.00 FEET; THENCE RUN SOUTH 00°44'45" EAST, FOR A DISTANCE OF 256.12 FEET; THENCE RUN NORTH 88°57'15" EAST, FOR A DISTANCE OF 329.11 FEET TO A POINT ON THE EAST LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 31; THENCE RUN SOUTH 00°45'13" EAST, ALONG THE EAST LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 31, FOR A DISTANCE OF 1,065.00 FEET TO THE **POINT OF BEGINNING**;

CONTAINING 20.996 ACRES, MORE OR LESS.

NOT COMPLETE WITHOUT SHEETS 1 AND 2 OF 2

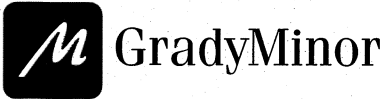
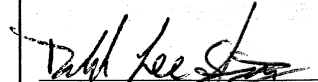
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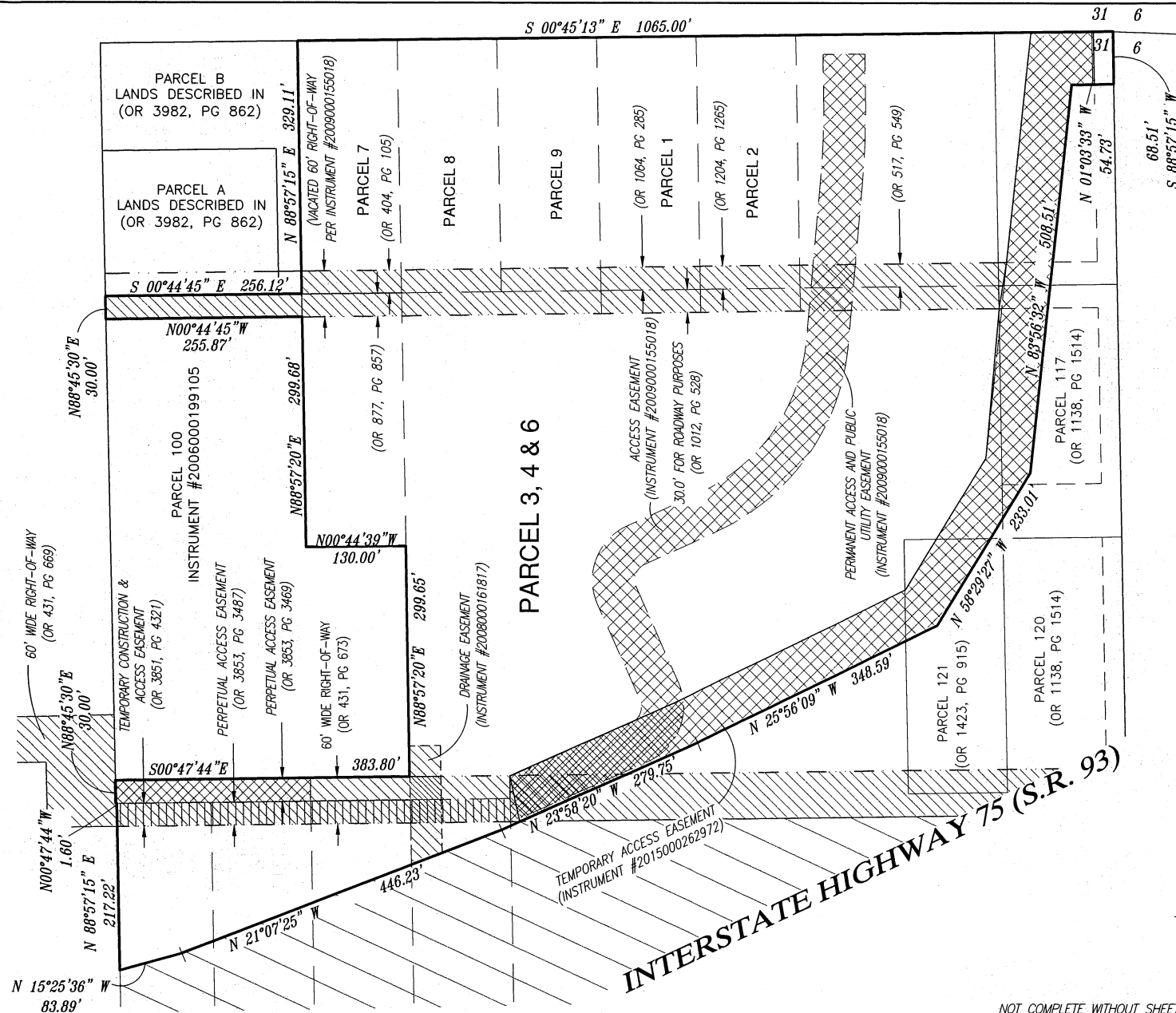
EXHIBIT A

NOTES:

1. BEARINGS SHOWN HEREON ARE BASED ON THE STATE PLANE COORDINATE SYSTEM ESTABLISHED BY THE NATIONAL GEODETIC SURVEY FOR FLORIDA WEST ZONE, 1983 DATUM WITH 2011 ADJUSTMENT OBTAINED UTILIZING RTK GPS OBSERVATIONS ON THE FDOT NETWORK AND REFER TO THE THE SOUTH LINE OF THE SOUTHWEST 1/4 OF SECTION 31, TOWNSHIP 47 SOUTH, RANGE 26 EAST, LEE COUNTY, FLORIDA, BEING SOUTH 88°57'15" WEST.
2. DIMENSIONS SHOWN HEREON ARE IN U.S. SURVEY FEET AND DECIMALS THEREOF.
3. THIS SKETCH AND DESCRIPTION IS NOT VALID WITHOUT THE ORIGINAL SIGNATURE AND SEAL OR THE DIGITAL SIGNATURE AND DIGITAL SEAL OF A LICENSED FLORIDA SURVEYOR AND MAPPER. NO ADDITIONS OR DELETIONS TO THIS SKETCH AND DESCRIPTION ARE PERMITTED WITHOUT THE EXPRESSED WRITTEN CONSENT OF THE SIGNING PARTY.

DRAWN BY:	KJG
CHECKED BY:	DLS
JOB CODE:	B75MC
SCALE:	N/A
DATE:	6/15/2021
FILE:	21-101-S&L
SHEET:	1 of 2

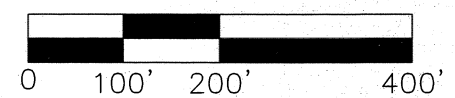
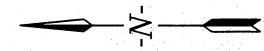
 <p>Q. Grady Minor and Associates, P.A. 3800 Via Del Rey Bonita Springs, Florida 34134</p> <p>Civil Engineers • Land Surveyors • Planners • Landscape Architects Cert. of Auth. EB 0005151 Cert. of Auth. LB 0005151 Bonita Springs: 239.947.1144 www.GradyMinor.com Fort Myers: 239.690.4380</p>	<p>SKETCH AND DESCRIPTION</p> <p>BONITA I-75</p> <p>LYING IN SECTION 31, TOWNSHIP 47 SOUTH, RANGE 26 EAST LEE COUNTY, FLORIDA</p>	<p>6/21/21 DATE SIGNED</p> <p> DONALD L. SAINTENBY III, P.S.M. FL LICENSE #1861 FOR THE FIRM</p>
	<p>G:\SURVEY\PROJECT SURVEY 2021\101 - BONITA 75 (B75MC)\SURVEY\SKETCH & LEGALS\21-101-S&L.DWG</p>	



POB
SE CORNER OF
THE SW 1/4 OF
SECTION 31

LEGEND

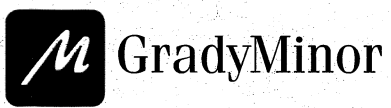
POB POINT OF BEGINNING
OR OFFICIAL RECORDS BOOK
PG PAGE(S)



SCALE: 1" = 200'

THIS PLAN MAY HAVE BEEN ENLARGED OR REDUCED FROM INTENDED DISPLAY SCALE FOR REPRODUCTION REASONS

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 3800 Via Del Rey
 Bonita Springs, Florida 34134

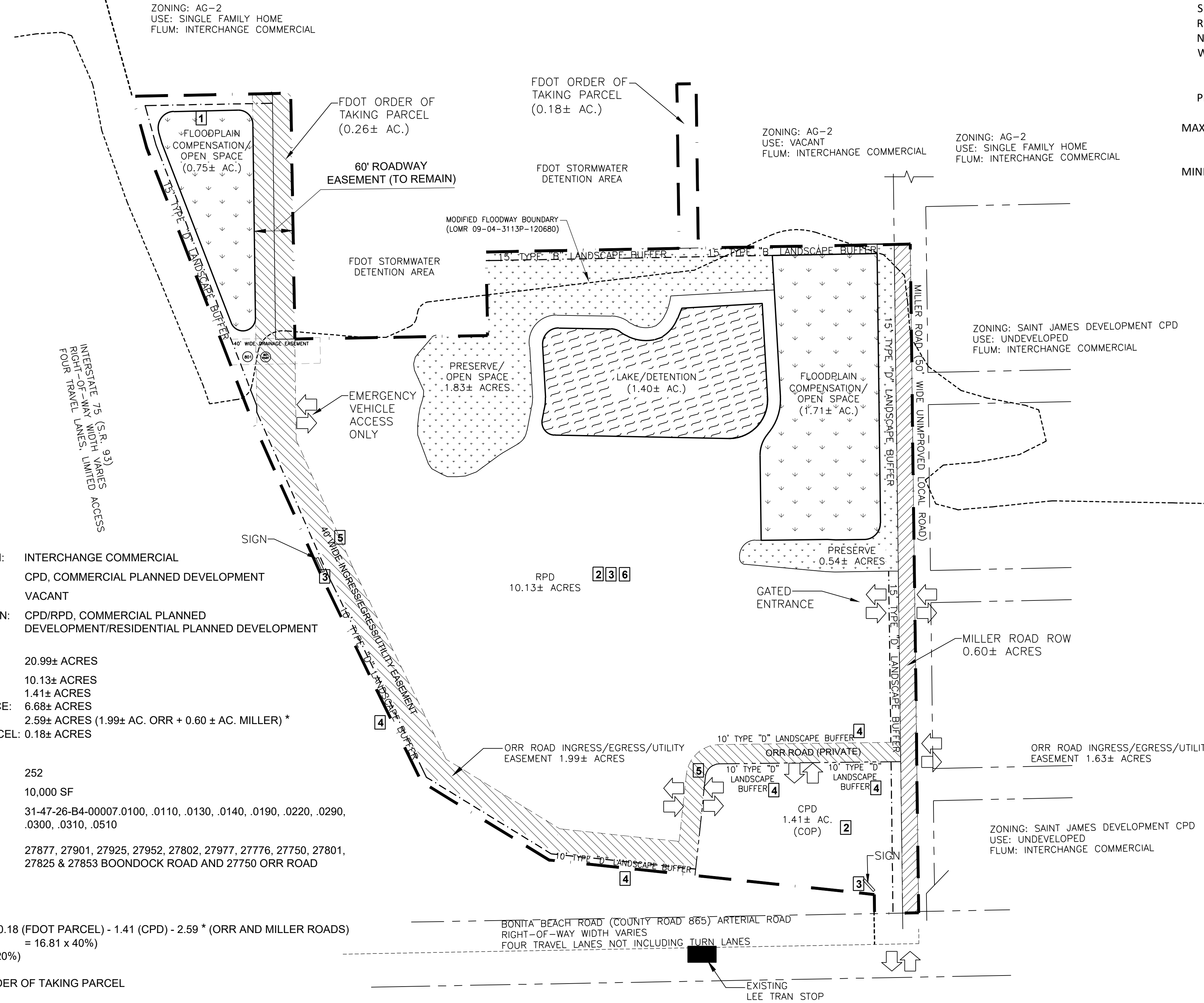
SKETCH AND DESCRIPTION

BONITA I-75
 LYING IN
 SECTION 31, TOWNSHIP 47 SOUTH, RANGE 26 EAST
 LEE COUNTY, FLORIDA

NOT A SURVEY

DRAWN BY:	KJG
CHECKED BY:	DLS
JOB CODE:	B75MC
SCALE:	1" = 200'
DATE:	6/15/2021
FILE:	21-101-S&L
SHEET:	14 2 of 2

EXHIBIT B



SITE DEVELOPMENT REGULATIONS:

RPD:
 MINIMUM AREA DIMENSION:
 MINIMUM LOT AREA: N.A.
 MINIMUM LOT DEPT: N.A.
 MINIMUM LOT WIDTH: N.A.

MINIMUM BUILDING SETBACKS:
 STREET/FRONT (PUBLIC ROAD): 25 FEET
 STREET/FRONT (PRIVATE INGRESS/EGRESS EASEMENT): 20 FEET
 INTERNAL DRIVEWAY: 10 FEET
 SIDE YARD: 10 FEET
 REAR YARD: 10 FEET
 NORTHERN RPD BOUNDARY: 20 FEET
 WATER BODY:
 PRINCIPAL: 25 FEET
 ACCESSORY: 0 FEET
 PRESERVE: 30 FEET

MAXIMUM BUILDING HEIGHT: 60 FEET (AS MEASURED FROM FIRST FINISHED FLOOR TO BUILDING EAVES), NOT TO EXCEED 4-STORIES

MINIMUM OPEN SPACE: 40%

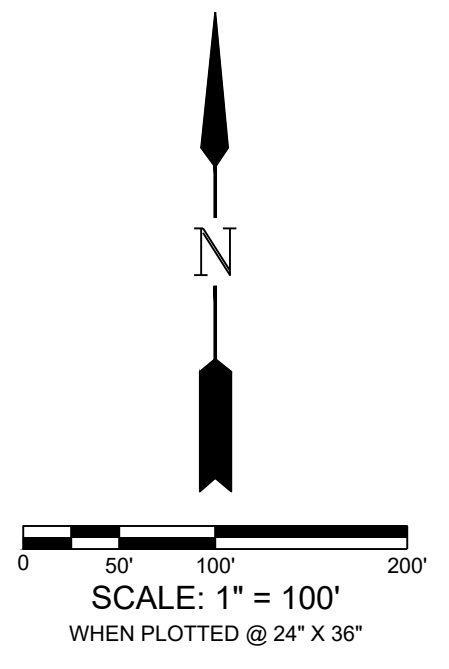
SITE DEVELOPMENT REGULATIONS:

CPD:
 MINIMUM AREA DIMENSION:
 MINIMUM LOT AREA: 10,000 SQ. FT.
 MINIMUM LOT DEPT: 100 FEET
 MINIMUM LOT WIDTH: 100 FEET

MINIMUM BUILDING SETBACKS:
 STREET/FRONT (PUBLIC ROAD): 25 FEET
 STREET (PRIVATE ROAD): 10 FEET
 DRIVE AISLE: 10 FEET
 SIDE YARD: 15 FEET
 REAR YARD: 25 FEET

MAXIMUM BUILDING HEIGHT: 35 FEET, NOT TO EXCEED 2-STORIES

MINIMUM OPEN SPACE: 20%



SITE SUMMARY

FUTURE LAND USE DESIGNATION: INTERCHANGE COMMERCIAL
EXISTING ZONING DESIGNATION: CPD, COMMERCIAL PLANNED DEVELOPMENT
EXISTING LAND USE: VACANT
PROPOSED ZONING DESIGNATION: CPD/RPD, COMMERCIAL PLANNED DEVELOPMENT/RESIDENTIAL PLANNED DEVELOPMENT

GROSS AREA: 20.99± ACRES
 RESIDENTIAL TRACT: 10.13± ACRES
 COMMERCIAL TRACT: 1.41± ACRES
 PRESERVE/LAKES/OPEN SPACE: 6.68± ACRES
 ROW ORR/MILLER ROAD: 2.59± ACRES (1.99± AC. ORR + 0.60± AC. MILLER) *
 FDOT ORDER OF TAKING PARCEL: 0.18± ACRES

MAXIMUM DWELLING UNITS: 252
MAXIMUM SQUARE FOOTAGE: 10,000 SF
STRAP NUMBERS: 31-47-26-B4-00007.0100, .0110, .0130, .0140, .0190, .0220, .0290, .0300, .0310, .0510

STREET ADDRESS: 27877, 27901, 27925, 27952, 27802, 27977, 27776, 27750, 27801, 27825 & 27853 BOONDOCK ROAD AND 27750 ORR ROAD

OPEN SPACE (SEE NOTE #4):
 REQUIRED: 7.0 AC.
 PROVIDED: 7.0 AC.
 RPD: 6.72± AC (20.99 - 0.18 (FDOT PARCEL) - 1.41 (CPD) - 2.59 * (ORR AND MILLER ROADS) = 16.81 x 40%)
 CPD: 0.28± AC (1.41 X 20%)

* INCLUDES 0.26 ACRE FDOT ORDER OF TAKING PARCEL

NOTES:

- ALL INTERNAL ROADS ARE EITHER PRIVATE LOCAL ROADS OR DRIVEWAYS.
- THE INTERNAL SUBDIVISION LAYOUT INCLUDING ROADS DEPICTED ABOVE ARE CONCEPTUAL AND WILL BE FINALIZED DURING THE DEVELOPMENT ORDER PROCESS.
- BUFFERS PROVIDED PER LDC OR AS APPROVED BY DEVIATION.
- SEE CONDITION #1 REGARDING TREE PRESERVATION IN LIEU OF INDIGENOUS PRESERVE.
- SERVICE AREAS FOR THE CPD WILL BE SHIELDED OR GENERALLY ORIENTED TO THE SIDE OR REAR OF THE BUILDING.

DEVIATIONS FROM LDC:

- RELIEF FROM LDC SECTION 3-331(D)(1), WHICH PROVIDES THAT NO EXCAVATIONS WILL BE ALLOWED WITHIN FIFTY FEET OF ANY PRIVATE PROPERTY LINE UNDER SEPARATE OWNERSHIP UNLESS GRANTED AN ADMINISTRATIVE DEVIATION IN ACCORDANCE WITH SECTION 3-81. THE SETBACK FOR AN EXCAVATION FROM A PRIVATE PROPERTY LINE MAY NOT BE LESS THAN 25 FEET TO ALLOW FOR 15 FT.
- RELIEF FROM LDC SEC. 4-899, SITE DESIGN STANDARDS, TO ALLOW DEVELOPMENT WITHIN THE CPD/RPD TO BE CONSISTENT WITH THE DEVELOPMENT STANDARDS PROPOSED FOR THIS CPD/RPD.
- RELIEF FROM LDC SEC. 6-112 AND 6-113, PERMANENT SIGNS IN RESIDENTIAL AND COMMERCIAL AREAS, TO ALLOW FOR UP TO TWO SHARED ENTRANCE SIGNS/PROJECT SIGNS FOR THE RESIDENTIAL AND COMMERCIAL TRACTS. THE SIGNS MAY BE POLE OR MONUMENT SIGNS AND IF DEVELOPED AS A POLE SIGN THE POLE MUST HAVE A POLE COVER NO LESS THAN 50% OF THE WIDTH OF THE SIGN. THE SHARED SIGN(S) MAY BE A MAXIMUM OF 15 FEET IN HEIGHT AS MEASURED FROM THE FINISHED GRADE AND THE MAXIMUM AREA SHALL NOT EXCEED 240 SQUARE FEET. THE SIGN MUST BE SETBACK A MINIMUM OF 5 FEET FROM ORR ROAD OR MILLER ROAD. LOCATIONS SHOWN ARE APPROXIMATE.
- RELIEF FROM LDC SEC. 3-418(D)(4), WHICH REQUIRES A 15-FOOT-WIDE TYPE 'D' LANDSCAPE BUFFER ADJACENT TO A ROW TO INSTEAD ALLOW A 10-FOOT-WIDE TYPE 'D' BUFFER ADJACENT TO THE ORR ROAD ROW AND INGRESS/EGRESS/UTILITY EASEMENT AREA.
- RELIEF FROM LDC SEC. 3-303, COMPLETE STREETS DESIGN, FOR THAT PORTION OF ORR ROAD LOCATED WITHIN THE BOUNDARY OF THE CPD/RPD. THAT PORTION OF ORR ROAD LOCATED BETWEEN THE RPD AND CPD AREAS SHALL BE A 40 FOOT WIDE CROSS-SECTION, WITH CLOSED DRAINAGE AND SIX FOOT WIDE SIDEWALKS ON EACH SIDE OF THE ROADWAY AS DEPICTED IN THE ORR ROAD CROSS-SECTION, EXHIBIT A. THE REMAINDER OF ORR ROAD SHALL REMAIN IN ITS CURRENT DESIGN WITH OPEN DRAINAGE AND NO SIDEWALKS.
- RELIEF FROM LDC SEC. 4-1732(1)C REQUIRED PARKING FOR MULTIPLE-FAMILY BUILDINGS, WHICH REQUIRES PARKING AT A RATIO OF 1.5 SPACES PER UNIT FOR EACH 1BR UNIT, 1.75 SPACES PER UNIT FOR EACH 2BR UNIT AND 2 SPACES PER UNIT FOR A 3BR UNIT, WITH AN ADDITIONAL 10% OF THE TOTAL REQUIRED UNITS TO BE GUEST PARKING, TO INSTEAD ALLOW A PARKING RATIO OF 1.8 SPACES PER UNIT AS THE REQUIRED PARKING FOR THE PROJECT.

SCHEDULE OF USES:

RPD (NOT TO EXCEED 252 UNITS): MINIMUM 40% OPEN SPACE - 6.72± ACRES
 ACCESSORY USES AND STRUCTURES
 ADMINISTRATIVE OFFICES (LEASING OFFICE)
 DWELLING UNITS, MULTIPLE FAMILY
 ESSENTIAL SERVICE FACILITIES: GROUP I
 EXCAVATION, WATER RETENTION
 FENCES, WALLS
 GARAGE, PRIVATE
 GATEHOUSE(S)
 RECREATIONAL FACILITIES, PRIVATE ON-SITE
 SIGNAGE
 STORAGE, INDOOR

CPD (NOT TO EXCEED 10,000 SQUARE FEET): MINIMUM 20% OPEN SPACE - 0.28± ACRES
 ACCESSORY USES AND STRUCTURES
 ADMINISTRATIVE OFFICES
 ATM (AUTOMATIC TELLER MACHINE)
 AUTO PARTS STORE
 BANKS AND FINANCIAL ESTABLISHMENTS: GROUP I
 BOATS PARTS STORE
 BUSINESS SERVICES: GROUP I AND II
 CLUBS, PRIVATE
 CONSUMPTION ON PREMISES, INDOOR ONLY, IN CONJUNCTION WITH A FULL-SERVICE RESTAURANT, MICRO BREWERY. OUTDOOR CONSUMPTION REQUIRES A SPECIAL EXCEPTION.
 CONTRACTORS AND BUILDERS: GROUP I AND II
 DAYCARE CENTERS, CHILD, ADULT
 DRUG STORE, PHARMACY
 DWELLING UNIT (ANCILLARY ONLY); MULTIPLE-FAMILY BUILDING (NOT TO EXCEED 252 UNITS FOR THE ENTIRE CPD/RPD)
 ENTRANCE GATES AND GATEHOUSE
 ESSENTIAL SERVICE FACILITIES: GROUP I
 EXCAVATION: WATER RETENTION
 FENCES, WALLS
 FOOD STORES, GROUP I
 HARDWARE STORE - NO OUTDOOR STORAGE OR DISPLAY
 HEALTHCARE FACILITIES: GROUP I AND II

HOUSEHOLD AND OFFICE FURNISHING: GROUP I
 LAUNDRY OR DRY CLEANING: GROUP I
 LAWN AND GARDEN SUPPLY STORES, NO OUTDOOR STORAGE OR DISPLAY.
 MICRO-BREWERIES
 OFFICE - BUSINESS
 PAINT, GLASS AND WALLPAPER - NO OUTDOOR STORAGE OR DISPLAY
 PARKING LOT: ACCESSORY AND GARAGE, PRIVATE
 PERSONAL SERVICES, GROUP I, II AND III
 PET SERVICE
 RENTAL OR LEASING ESTABLISHMENT: GROUP I
 RESIDENTIAL ACCESSORY USES
 RESTAURANT, FAST FOOD
 RESTAURANTS: GROUPS I, II, III AND IV
 SPECIALTY RETAIL: GROUPS I, II AND III
 STORAGE: INDOOR ONLY
 STUDIOS
 TEMPORARY USES
 USED MERCHANDISE STORES: GROUP I
 VARIETY STORE
 WHOLESALE ESTABLISHMENTS: GROUP III

LEGEND

	ACCESS		DEVIATION
	PRESERVE/OPEN SPACE		FLOODPLAIN COMPENSATION/OPEN SPACE
	EASEMENT		LAKE

Revision	Date	Description	By

DESIGNED BY: DWA
 DRAWN BY: SU
 APPROVED: DWA
 JOB CODE: B75MC
 SCALE: 1" = 100'

GradyMinor
 Civil Engineers • Land Surveyors • Planners • Landscape Architects
 Cert. of Auth. EB 0005151 Cert. of Auth. LB 0005151
 Bonita Springs: 239.947.1144 www.GradyMinor.com Fort Myers: 239.690.4380

Q. Grady Minor and Associates, P.A.
 3800 Via Del Rey
 Bonita Springs, Florida 34134

BONITA 75 CPD/RPD

EXHIBIT IV-E
 MASTER CONCEPT PLAN
 REVISED: JUNE 1, 2022

MUNICIPALITY: BONITA SPRINGS
 SEC./TWINSHIP/RNG: 31/475/26E
 DATE: JULY 2021
 SUBMITTAL TYPE: SHEET 1 OF 1

G:\PLANNING\PROJ - PLANNING\B75MC_BONITA_75_CPDA (P021-83966-805)\DRAWINGS\B75MC_MCP (REVISED)_JUNE_1_2022.DWG 6/1/2022 2:40 PM

EXHIBIT C

Sec. 4-295. Application.

- (a) *Minimum required information for all planned development zoning applications.* Rezoning applications for planned developments must include the following information, supplemented, where necessary, with written material, maps, plans, or diagrams. Wherever this section calls for the exact or specific location of anything on a map or plan, the location must be indicated by dimensions from an acceptable reference point, survey marker or monument.
- (5) *Explanation.* A narrative explanation as to how the proposed development complies with the Bonita Plan, the design standards set forth in section 4-325, and the guidelines for decision-making embodied in section 4-131.

The subject project, which is located in the northeast quadrant of Interstate 75 and Bonita Beach Road, is currently zoned Bonita 75 CPD (ZO-08-10).

The property is designated Interchange Commercial on the City of Bonita Springs Future Land Use Map, which permits a wide range of commercial land uses. Policy 1.1.15, Interchange Commercial, of the Future Land Use Element indicates that this land use category is intended for uses that serve the traveling public, including gas/convenience, restaurants and a range of tourist related commercial uses. Policy 1.1.15 also permits multi-family residential development at a standard maximum density of 10 du/ac and up to 15 du/ac with the use of bonus density. The proposed density is approximately 12 du/ac. The property is also located within the Interchange Zone of the Bonita Beach Road Corridor Overlay. The proposed CPD/RPD includes residential and commercial uses consistent with those permitted in Section 4-898 of the LDC.

The applicant, Madison Capital Group Management, LLC, is proposing to rezone the CPD to CPD/RPD and reduce the overall commercial square footage and add multi-family residential dwelling units. The application will seek 252 multi-family dwelling units, which is approximately 12 du/ac. An affordable housing bonus density agreement will be filed separately for the 47 bonus units. A maximum of 10,000 square feet of commercial uses consistent with those permitted in Section 4-898 of the Bonita Beach Road Corridor Overlay are proposed. The RPD portion of the project will include an amenity area, which will include a clubhouse containing the rental apartment leasing offices, as well as indoor fitness facilities, meeting space and restrooms. The outside of the amenity area will feature a swimming pool with shade structures. The amenity area is designed to be accessible to all residents of the community via internal pedestrian facilities. The amenities are not designed to serve the CPD portion of the project; however, having commercial in close proximity to the residential use is seen as beneficial for the residents. Having the commercial component easily accessible by foot or automobile provides convenient employment or shopping opportunities for the residents of the RPD.

The proposed rental apartments within the RPD will be a maximum of 4 stories in height, although some 2 story buildings may be constructed depending on final site design. The amenity building

will be a single-story building. The CPD has a proposed height limitation of 35 feet which will provide for 2 story maximum heights. These proposed heights are compatible with the limited residential dwellings located at the terminus of Orr Road and Miller Road north of the RPD/CPD due to the strategic location of the proposed open space areas on the site and the presence of lands owned by the State located between the RPD/CPD and these home sites.

The property is located in the Interchange Commercial FLU Category, and it is located at the I-75 northbound entrance ramp. While the project does not have direct frontage on Bonita Beach Road, it does abut Orr Road and Miller Road. The proposed uses are oriented to Bonita Beach Road as close as feasible given the separation from Bonita Beach Road by the I-75 on ramp and Orr Road.

The site consists of approximately 20.99 acres. The entire site has been previously impacted by past usage. Historical aerials from 1995 and 2004 have been included. These aerials show the site containing single family uses. In addition, they show parked tractor trailers, cleared areas and accessory structures. As such, most of the site lacks any native habitat. Most of the site has been altered. The site does have some scattered canopy trees of live oaks, Slash pines and cabbage palms. However, there is little to no ground cover and/ or midstory. A tree survey was conducted. To date, the site has cleared areas with remnants of past structures and debris.

A proposed condition for revegetation within the floodway is anticipated and consistent with the proposed MCP.

Sec. 4-131. Zoning board standard for review.

(d) *Zoning matters.*

- (3) *Findings.* Before preparing their recommendation to the city council on a rezoning, the zoning board must find that:
 - a. The applicant has proved entitlement to the rezoning or special exception by demonstrating compliance with the Bonita Plan, this Land Development Code, and any other applicable code or regulation;

The proposed rezone to add multifamily residential uses is consistent with the Bonita Springs Comprehensive Plan.

Policy 1.1.15, Interchange Commercial provides for a wide variety of commercial land uses and also provides for multifamily residential development. The proposed PD rezone will reduce the amount of commercial square footage. The current zoning permits a maximum of 158,800 square feet of commercial uses and this will be reduced to a maximum of 10,000 square feet of commercial land uses. The schedule of uses has been amended to add 252 multifamily dwelling units. Development standards for the residential dwellings have also been added to the PD. The proposed 12 du/ac density is consistent, with the FLUC density with the use of bonus units, is within the permissible maximum density per Policy 1.1.15.

Future Land Use Policy 1.1.2, Density, Affordable Housing Density Bonus and Wetlands Transfer identifies that property designated Interchange Commercial are eligible for up

to 5 du/ac bonus units. The applicant intends to use the payment in lieu of process to obtain the 47 bonus units. The project at the standard maximum density of 10 du/ac is eligible for 205 dwelling units. This PD application proposes a total of 252 dwelling units and is within the allowable density range. A separate application for the bonus density will be filed for the project.

Future Land Use Policy 1.7.6 supports protection of environmentally sensitive lands. The property is not adjacent to the Imperial River/Keel Canal; however, the northernmost portion of the PD is within a FEMA designated floodway. This area is identified on the MCP and no structures will be placed within this portion of the site. This area will be utilized for floodplain compensation and replanting with native vegetation in accordance with the LDC provisions for open space. The use of this area is consistent with Policy 1.7.6.

Policy 1.11.1 establishes LOS standards. There are no existing or projected LOS deficiencies for public facilities associated with this PD rezone. A Traffic Impact Analysis has been prepared for the project and concludes that the reduction of commercial uses and addition of the proposed 252 multifamily dwelling units reduces traffic over that for the commercial only planned development.

Objective 1.16. and Policies 1.16.1 - 1.16.6 reference the establishment of the Bonita Beach Road Corridor and Bonita Beach Road Corridor Map and implementing regulations in the Land Development Code. The PD is within the Interstate Zone on the Corridor Map. This area is also referred to as the "Gateway Zone".

The proposed commercial and residential uses are consistent with the permitted uses for the corridor as identified in Section 4-898, Table 4-898 of the LDC.

The PD has requested a deviation from the design standards outlined in Section 4-899 to provide development standards as proposed for the CPD/RPD. Most of the Section 4-899 standards are not applicable as the site does not have frontage on Bonita Beach Road.

The residential portion of the site is proposed to be developed as a gated multifamily community that will not have platted streets. There is one commercial tract proposed at the Orr Road intersection with Miller Road. Future interconnections to Miller Road are shown on the MCP. No blocks within the PD will be provided; however, the residential component of the project is walkable and will feature central amenities designed to be easily accessible to all residents including the commercial tract.

Pedestrian access from the project entrance to the pathway located in the Bonita Beach Road ROW will be provided.

Transportation Element

Policy 1.1.3 establishes minimum acceptable Level of Service Standards for roadways. A Traffic Impact Analysis has been prepared by a registered traffic engineer, which finds that the adopted LOS standards will be met based on the intensity of development proposed in the CPD/RPD.

Housing Element

Policy 1.3.2.C and 1.3.2.d acknowledges the use of bonus densities by making a cash contribution dedicated to the provisions of affordable housing. The developer is proposing to make the cash contribution consistent with this policy for 47 bonus density units. The resulting project will not have substantially increased intensities of land use on the perimeter. The project will have open space and indigenous preservation along the northern property boundary nearest the residential neighboring property. The traffic associated with the project will not be required to travel through areas with significantly lower densities. Access to the project will be via Bonita Beach Road/Miller Road. There are adequate public facilities available to service the proposed 252-unit apartment complex. There will be no decrease in open space areas that have any adverse impact to an existing or approved development. The preserve areas are largely located in the areas previously identified as preserves for the commercial-only development scenario. The project is not located in a Category 1 storm surge area; therefore, no storm shelters or other mitigation is required.

Conservation and Coastal Management Element

Policy 7.2.2 provides that large developments will provide 50% of their open space requirement using existing indigenous native vegetation. A scaled credit as identified in LDC Section 3-417 is eligible as an incentive to preserve indigenous habitat. The subject property has been largely cleared in the past and while there are indigenous trees, there is no indigenous habitat. The existing CPD was previously approved with a preservation area that included native trees. The proposed RPD/CPD increases the preservation area required due to the requirement to provide 40% open space for the residential component of the RPD and 20% for the CPD. A condition is proposed to address restoration and heritage tree preservation.

Objective 7.4 requires that the City protect habitats of endangered and threatened species. A listed species survey was conducted on the property as recently as May 2021. No listed species were observed on the property. As discussed in the report the site does not contain indigenous habitat and it is located at the intersection of I-75 and Bonita Beach Road, making the site not conducive to attracting listed species.

- b. The request will meet or exceed all performance and locational standards set forth for the potential uses allowed by the request;

There are no performance or locational standards for the uses. The proposed uses are to be sited in a compatible manner with the surrounding community but taking into account the relationship with the adjacent roads and private access.

- c. The request, including the use of TDR or affordable housing bonus density units, is consistent with the densities, intensities and general uses set forth in the Bonita Plan;

The proposed use of affordable housing bonus density units will be in compliance with the LDC and Comprehensive Plan. Staff has provided direction to the applicant for their use and such application is a companion item to this zoning application. A narrative and draft agreement is provided separately.

- d. The request is compatible with existing or planned uses in the surrounding area;

The surrounding areas are low density residential to the north. A large open space buffer is provided to the adjoining FDOT lake which separates farther the residential areas to this site. The area to the east is an existing commercial PD. Shared access is planned with that use through improvement to Miller Road.

The proposed CPD/RPD is also consistent with the required criteria in LDC 4-299(2)a-c. The proposed mix of uses are appropriate at the subject location. All uses are consistent with the Interchange Commercial Future Land Use Category, and the density is within the density range associated with this land use category. The location is appropriate for higher density rental apartments given the proximity to I-75 and Bonita Beach Road, allowing for easy access to employment and close proximity to local goods and services.

The proposed development standards, buffers, and location of open spaces provides sufficient safeguards to the public interest. The RPD portion of the site will be separated from the few single-family homes located at the terminus of Orr Road by an FDOT water management area, a large flood plain compensation area and re-created preservation area. The staff recommended conditions are related to the impacts on the public interest created by the proposed CPD/RPD. The commercial use will also be accessible to the residents of the CPD/RPD and is designed in a manner to capture their use by pedestrian and vehicular access.

- e. Approval of the request will not place an undue burden upon existing transportation or planned infrastructure facilities and will be served by streets with the capacity to carry traffic generated by the development;

The traffic impact statement (TIS) submitted with this application confirms the proposed uses will not have an adverse impact on capacity.

- f. Where applicable, the request will not adversely affect environmentally critical areas and natural resources;

The site was previously cleared for homesteads in the 1970s and 1980s. No indigenous area remains, however individual indigenous trees do exist. The limits of the floodway have been identified on the MCP. No development will occur within the floodway with the exception of revegetation with ground cover.

- g. In the case of a planned development rezoning, the decision of the zoning board must also be supported by the formal findings required by section 4-299(a)(2) and (4);

Acknowledged.

- h. The zoning board must also find that public facilities are, or will be, available and adequate to serve the proposed land use.

Acknowledged.

Chapter 4-325 Consistency

The CPD/RPD as proposed is in compliance with the General Standards for Planned Developments found in Chapter 4-325.

- a. All planned developments shall be consistent with the provisions of the Bonita Plan.

As discussed in the Chapter 4—295 analysis, the project is consistent with the Bonita Springs Comprehensive Plan.

- b. All planned developments, unless otherwise excepted, shall be designed and constructed in accordance with the provisions of all applicable city development regulations in force at that time.

Except as approved by deviation, the planned development is designed and will be consistent with applicable City regulations in effect at the time of Development Order approval.

- c. The tract or parcel proposed for development under this article must be located so as to minimize the negative effects of the resulting land uses on surrounding properties and the public interest generally, and must be of such size, configuration and dimension as to adequately accommodate the proposed structures, all required open space, including private recreational facilities and parkland, bikeways, pedestrian ways, buffers, parking, access, on-site utilities, including wet or dry runoff retention, and reservations of environmentally sensitive land or water. In large residential or commercial planned developments, the site planner is encouraged to create subunits, neighborhoods or internal communities which promote pedestrian and cyclist activity and community interaction.

The MCP identifies the areas to be developed for MF residential and commercial uses consistent with the Bonita Beach Road Corridor Overlay. The MCP also identifies areas of preservation, open space, and water management. Sidewalks will be provided on Miller Road and the portion of Orr Road that lies between the CPD and RPD tracts.

- d. The tract or parcel shall have access to existing or proposed roads:

- (1) In accordance with chapter 3 and as specified in the Bonita Plan traffic circulation element or the official trafficways map of the county;
- (2) That have either sufficient existing capacity or the potential for expanded capacity to accommodate both the traffic generated by the proposed land use and that traffic expected from the background (through traffic plus that generated by surrounding land uses) at a level of service D or better on an annual average basis and level of service E or better during the peak season, except where higher levels of service on specific roads have been established in the Bonita Plan; and
- (3) That provide ingress and egress without requiring site-related industrial traffic to move through predominantly residential areas.

Primary project access will be via Miller Road, which will be improved from Bonita Beach Road to the proposed residential project entry. Miller Road connects to Bonita Beach Road at an existing location matching that shown on the MCP. The TIS prepared for the proposed RPD/CPD concludes that far few trips are generated from the proposed project that includes MF residential dwellings versus the existing PD which has no residential component. No LOS roadway issues result from the project.

- e. If within the Lee Tran public transit service area, the development shall be designed to facilitate the use of the transit system.

There is a Lee Tran stop just west of the Miller Road connection on Bonita Beach Road. The Bonita 75 CPD/RPD will provide a pedestrian connection to the transit stop.

- f. Development and subsequent use of the planned development shall not create or increase hazards to persons or property, whether on or off the site, by increasing the probability or degree of flood, erosion or other danger, nor shall it impose a nuisance on surrounding land uses or the public's interest generally through emissions of noise, glare, dust, odor, air or water pollutants.

The proposed CPD/RPD will not create hazards to persons or property off-site. The site has previously been issued an ERP through the SFWMD and has obtained a FEMA map amendment which has been noted on the MCP. Glare, dust and odor will not be issues as the site is less intense than originally approved.

- g. Every effort shall be made in the planning, design and execution of a planned development to protect, preserve or to not unnecessarily destroy or alter natural, historical or archaeological features of the site, particularly mature native trees and other threatened or endangered native vegetation. Alteration of the vegetation or topography that unnecessarily disrupts the surface water or groundwater hydrology, increases erosion of the land, or destroys significant wildlife habitat is prohibited. That habitat is significant that is critical for the survival of rare, threatened or endangered species of flora or fauna.

There are no known archeological sites on the property. The site has been previously developed and there are no native vegetation communities remaining on the site. The applicant will be working with staff and the Tree Advisory Board to address the retention and/or relocation of native trees on-site. the Tree Advisory Board meeting was held on May 9, 2022 and they approved the plan as proposed.

- h. A fundamental principle of planned development design is the creative use of the open space requirement to produce an architecturally integrated human environment. This shall be coordinated with the achievement of other goals, e.g., the preservation or conservation of environmentally sensitive land and waters or archaeological sites.

The site has been designed to utilize open spaces for integration of the human environment. The dry detention areas will be designed to provide areas for human and pet use during the dry season.

- i. Site planning and design shall minimize any negative impacts of the planned development on surrounding land and land uses.

The site has been designed to have no impact on surrounding uses. Building heights, setbacks and placement of open space areas are utilized to reduce any negative impacts.

- j. Where a proposed planned development is surrounded by existing development or land use with which it is compatible and of an equivalent intensity of use, the design emphasis shall be on the integration of this development with the existing development, in a manner consistent with current regulation.

The PD has been designed to share the primary access (Miller Road) with the neighboring St. James PD. Orr Road has been realigned in order to provide for convenient access to the small number of properties located north of the site and to increase the throat depth from Bonita Beach Road.

- k. Where the proposed planned development is surrounded by existing development or land use with which it is not compatible or which is of a significantly higher or lower intensity of use (plus or minus ten percent of the gross floor area per acre if a commercial or industrial land use, or plus or minus 20 percent of the residential density), or is surrounded by undeveloped land or water, the design emphasis will be to separate and mutually protect the planned development and its environs.

The uses located within the PD have been designed to lessen any impact to neighboring properties by placing the residential component in closer proximity to surrounding homes and the commercial component has been significantly reduced in square footage and overall intensity. The residential remains separated from homes at the north end of Orr Road by lakes and open space/preservation areas.

- I. In large residential or commercial planned developments, the site planner is encouraged to create subunits, neighborhoods or internal communities which promote pedestrian activity and community interaction.

The project is approximately 20 acres in size and due to the residential use as an apartment complex it is not possible to create subunits. The site will be arranged so that all residents will have good access to project amenities.

- m. In order to enhance the viability and value of the resulting development, the designer shall ensure the internal buffering and separation of potentially conflicting uses within the planned development.

Buffers will be provided as identified on the MCP. The master developer intends to develop an integrated community with the small commercial property being developed in a complementary manner for the residents.

- n. Density or type of use, height and bulk of buildings and other parameters of intensity should vary systematically throughout the planned development. This is intended to permit the location of intense or obnoxious uses away from incompatible land uses at the planned development's perimeter, or, conversely, to permit the concentration of intensity where it is desirable, e.g., on a major road frontage or at an intersection.

The perimeter of the PD is protected by the placement of open space areas including buffers, vegetation preservation and lakes.

- o. Unless otherwise provided for in this article, minimum parking and loading requirements shall be as set forth in article VI, divisions 25 and 26, of this chapter. Where it can be reasonably anticipated that specified land uses are generators of occasional peak demand for parking space, a portion of the required parking may be pervious or semi-pervious surfaces subject to the condition that it be constructed and maintained so as to prevent erosion of soil. In all cases, however, sufficient parking shall be provided to prevent the spilling over of parking demand onto adjacent properties or rights-of-way at times of peak demand.

Parking is intended to be developed at a ratio consistent with that needed for the development. A parking deviation has been requested consistent with other rental apartments they have developed in Florida.

- p. Joint use of parking by various land uses within the planned development may be permitted by special condition where it can be demonstrated or required that the demand for parking by the various uses will not conflict. Joint parking agreements between uses within and uses without the planned development shall be governed by agreement per general regulation (see section 4-1730), without exception.

All areas within the residential development area will share joint use of the parking lots. No parking agreement is necessary as it will be under singular ownership.

- q. Internal consistency through sign control, architectural controls, uniform planting schedules and other similar controls is encouraged.

The Developer will establish the signage, architecture and planning schedule which will insure internal consistency.

EXHIBIT D

1. Deviation 1 seek relief from LDC Sec. 3-331(d)(1) which provides that no excavations will be allowed within fifty feet of any private property line under separate ownership unless granted an administrative deviation in accordance with section 3-81. The setback for an excavation from a private property line may not be less than 25 feet to allow for 15 ft.

Justification:

The property on the north side of the lake is zoned AG-2 and requires only a Type “B” buffer. In an effort to accommodate the land use to the north, the lake area will function as a buffer to the commercial to the south. The excavation will likely be necessary for floodplain compensation per the anticipated SFWMD consistent with the previously issued permit. Necessary security, such as a fence, will be provided at the discretion of the City Manager or his designee.

2. Deviation 2 seeks relief from LDC Sec. 4-899, site design standards, to allow development within the CPD/RPD to be consistent with the Development Standards proposed for this CPD/RPD.

Justification:

The project physically cannot comply with the maximum building setback and other site design standards in LDC Sec. 4-899 due to the presence of Orr Road. The entrance to the CPD/RPD is at the intersection of Orr Road and Miller Road, which is approximately 85 feet north of the Bonita Beach Road travel lanes which exceeds the maximum setback of 65 feet permitted in the Bonita Beach Road Interstate Zone. The project will also not be developed with internal streets as the residential component will be a gated rental apartment complex. The proposed development standards will permit development of the proposed uses in a manner necessary for this type of mixed-use development and the deviation will have no negative impact on the public.

3. Deviation 3 seeks relief from LDC Sec. 6-112 and 6-113, permanent signs in residential and commercial areas, to allow for up to two shared entrance signs/project signs for the residential and commercial tracts. The signs may be pole or monument signs and if developed as a pole sign the pole must have a pole cover no less than 50% of the width of the sign. the shared sign(s) may be a maximum of 15 feet in height as measured from the finished grade and the maximum area shall not exceed 240 square feet. The sign must be setback a minimum of 5 feet from Orr Road or Miller Road.

Justification:

The Mixed Use Planned Development will have limited visibility due to the physical location of the project which is separated from Bonita Beach Road by Orr Road and the northbound entrance ramp to I-75. The physical separation from the nearest travel lanes on Bonita Beach Road warrant signage that is greater in height than currently allowed for projects located in the Bonita Beach Road Corridor Overlay. Sharing the project signage for the residential and

commercial components will help reduce overall signage and given the small view corridors to the site, sharing the signage at strategic locations makes logical sense.

4. Deviation 4 seeks relief from LDC Sec. 3-418(d)(4), which requires a 15-foot-wide Type 'D' landscape buffer adjacent to a ROW to instead allow a 10-foot-wide Type 'D' buffer adjacent to the Orr Road ROW and ingress/egress/utility easement area.

Justification:

At the request of staff, the Orr Road alignment was revised to accommodate a better intersection spacing from Bonita Beach Road and to alleviate stacking concerns on Miller and Orr Roads. Based on the original easement area for Orr Road and the existing pavement area, approximately 10' in width is available for planting. The applicant is agreeable to plant additional plant material in the reduced buffer width if requested by staff.

5. Deviation 5 seeks relief from LDC Sec. 3-303, Complete Streets Design, for that portion of Orr Road located within the boundary of the CPD/RPD. That portion of Orr Road located between the RPD and CPD areas shall be a 40 foot wide cross-section, with closed drainage and six foot wide sidewalks on each side of the roadway as depicted in the Orr Road Cross-Section, Exhibit A. The remainder of Orr Road shall remain in its current design with open drainage and no sidewalks.

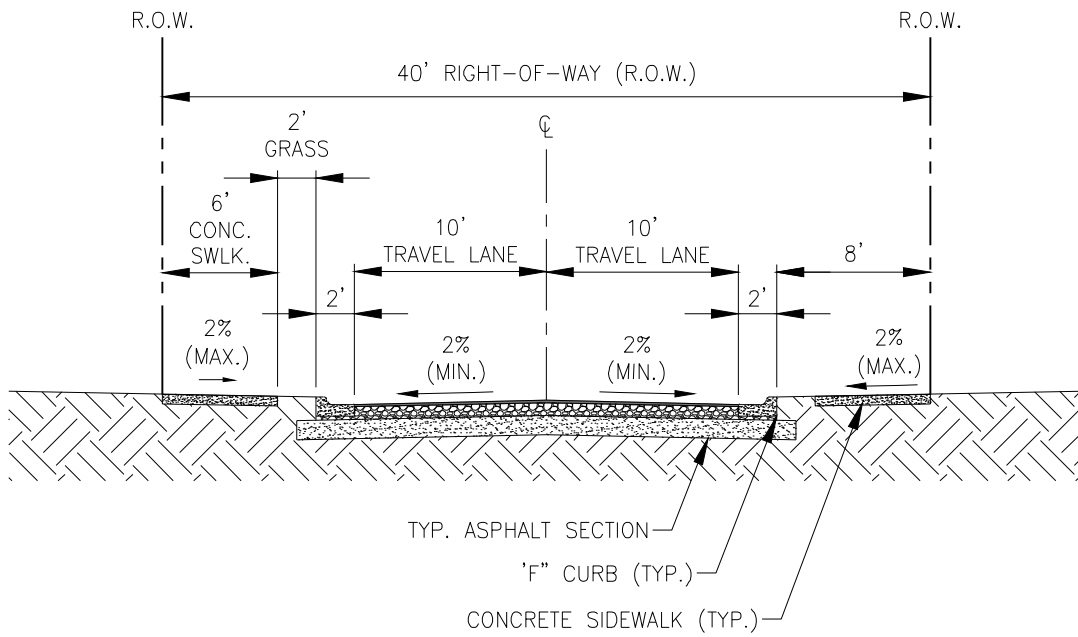
Justification:

The property owner has agreed to re-locate Orr Road in order to increase the intersection spacing from Bonita Beach Road, while also reducing a significant amount of commercial square footage, thereby reducing vehicular traffic associated with the CPD/RPD. Orr Road beyond the CPD tract serves three existing home sites, retention ponds owned by FDOT and vacant lands owned by the State of Florida. The existing Orr Road is in good physical condition and for the very low volume of traffic utilizing this portion of Orr Road it does not make economic sense to modify the existing cross-section. Further, the RPD/CPD will have access from Miller Road. Only an emergency vehicle access will be provided on the older section of Orr Road.

6. Deviation 6 seeks relief from LDC Sec. 4-1732(1)c Required Parking for Multiple-family buildings, which requires parking at a ratio of 1.5 spaces per unit for each 1BR unit, 1.75 spaces per unit for each 2BR unit and 2 spaces per unit for a 3BR unit, with an additional 10% of the total required units to be guest parking, to instead allow a parking ratio of 1.8 spaces per unit as the required parking for the project.

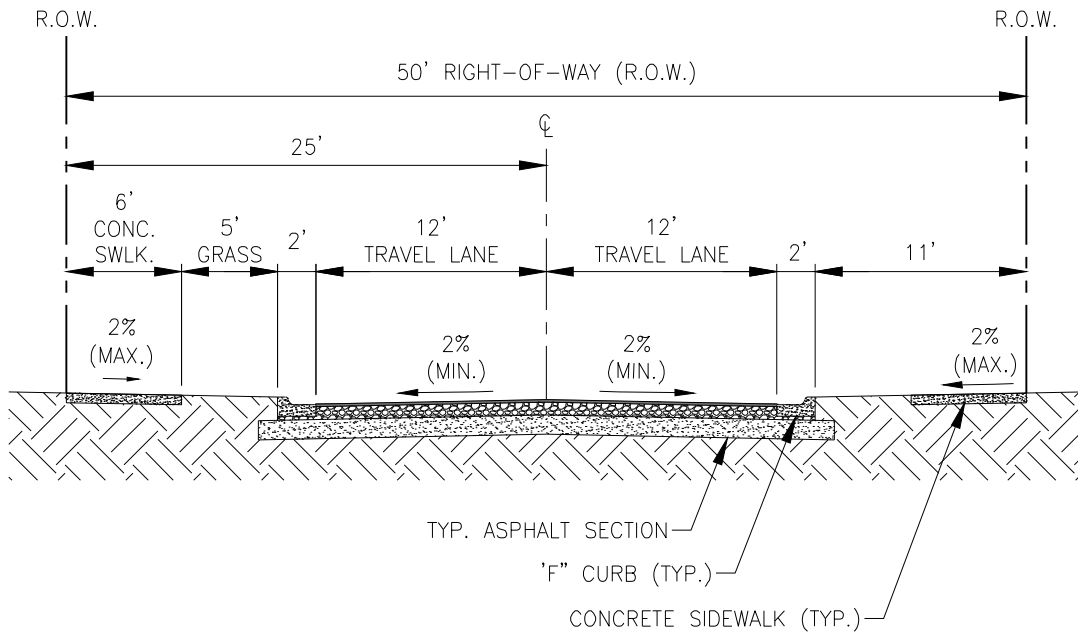
Justification:

Madison Capital Group has developed a number of rental communities throughout the United States and has successfully developed communities having an average parking ratio of 1.8 spaces per unit, with no additional parking for guest spaces. Typical rental communities developed by Madison Capital Group have approximately 50% of the units as 1BR, 40% as 2BR and 10% as 3BR units. The 1.8 parking space per unit average has been utilized in the following three communities in Florida: Tampa, Apopka and Wesley Chapel.



TYPICAL 40' ROW SECTION (INTERNAL TO PROJECT - COMMERCIAL EAST END)

N.T.S.



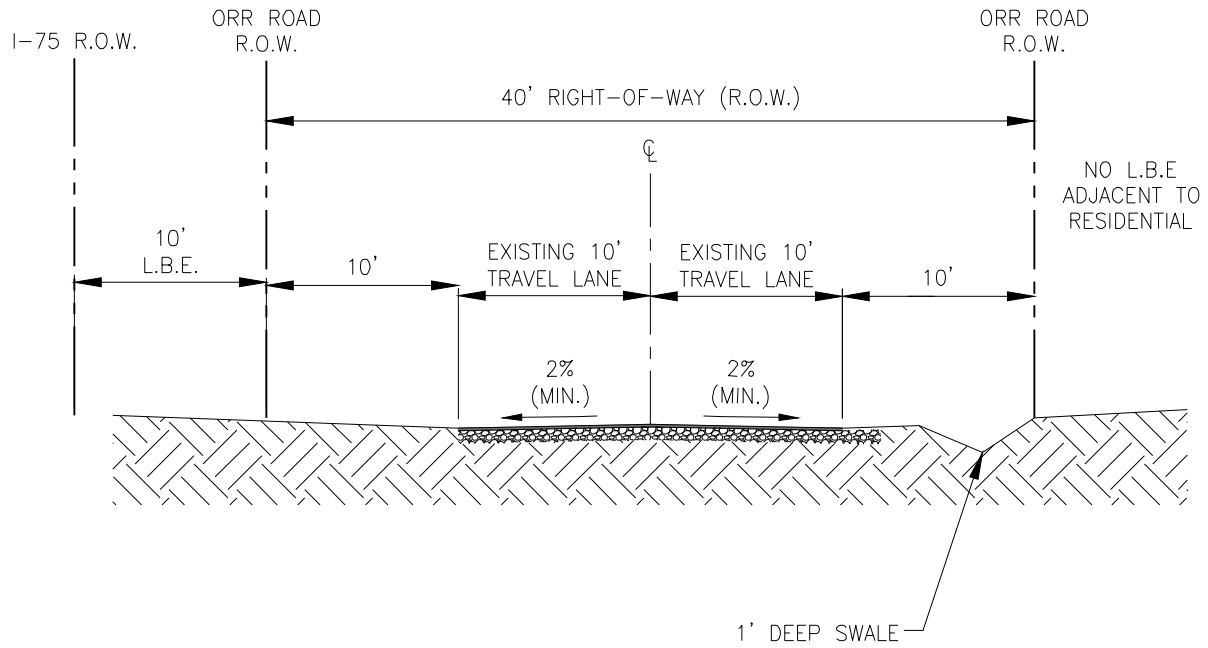
TYPICAL 50' ROW SECTION (MILLER ROAD)

N.T.S.

ROAD CROSS SECTION EXHIBIT (PAGE 1 OF 2)



Civil Engineers • Land Surveyors
 Planners • Landscape Architects
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TYPICAL 40' ROW SECTION (ORR ROAD AT I-75)
N.T.S.





Image capture: Dec 2007 © 2022 Google

Bonita Springs, Florida

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Street View - Dec 2007

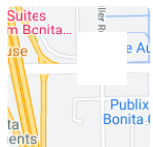




Image capture: Feb 2022 © 2022 Google

Bonita Springs, Florida

Google

Street View - Feb 2022

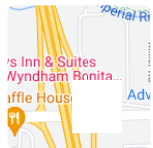




Image capture: Feb 2022 © 2022 Google

Google

Street View - Feb 2022



ATTACHMENT "A"
BACKGROUND & INFORMATIONAL ANALYSIS

Surrounding Land Use

<u>Existing Zoning & Land Use</u>	<u>Future Land Use Map</u>
Subject Parcel: Bonita 75 CPD; Vacant	Interchange Commercial (10.0 dwelling units/acre, 1.2 FAR)
North: Smith Nursery CPD; Vacant, AG-2; FDOT Retention Ponds, Single-Family Residential	Interchange Commercial (10.0 dwelling units/acre, 1.2 FAR)
East: St. James CPD, Bonita Beach Road Corridor Overlay-Interstate Zone; Vacant, AG-2; Single-Family Residential	Interchange Commercial (10.0 dwelling units/acre, 1.2 FAR)
South: Bonita Beach Road Corridor Overlay- Interstate Zone, I-75 On-Ramp and Bonita Beach Road Right-of-way	Interchange Commercial (10.0 dwelling units/acre, 1.2 FAR)
West: I-75 Interstate, Bonita Beach Road Corridor Overlay- Interstate Zone	Interchange Commercial (10.0 dwelling units/acre, 1.2 FAR)

Environmental Considerations

The Applicant provided an environmental assessment that was reviewed by City Staff. The assessment included surveys of the project site in accordance with the City's Administrative Code and the general methodology provided by the Florida Fish and Wildlife Conservation Commission. Included in the assessment was a listed species survey, a plant survey, and a habitat/vegetation survey.

There were no jurisdictional wetlands and no listed protected species observed on-site. The site contains native trees but lacks native land cover due to historical and present-day disturbance. Indigenous vegetation preservation is not required, but Staff does recommend Conditions 5.a. and 5.b. The site contains a 0.47 acre man-made ditch considered Other Surface Waters by the South Florida Water Management District (SFWMD), which will be reviewed during permitting by that agency.

Traffic

The City's transportation engineer reviewed the Applicant's Traffic Impact Statement (TIS) as part of this rezoning request. Throughout the review period, Lee County Department of Transportation (LCDOT) has indicated that the median opening at Bonita Beach Road and Miller Road/Hunter's Ridge Blvd. will be closed, impacting the future traffic maneuvers for this project. More specifically, when the median is closed, Miller Road will be limited to right-in/right-out only traffic maneuvers. All traffic accessing Miller Road from eastbound Bonita Beach Road, including project traffic, will be required to make a U-turn to travel westbound on Bonita Beach Road followed by a right turn to access Miller Road and the project site. It is assumed that all eastbound Bonita Beach Road traffic accessing Miller Road will use the left-turn lane at the Trade Way One median opening to make the required U-turn. The City's transportation engineer specifically requested the Applicant to analyze the maximum peak queuing for the eastbound left-turn lane at Trade Way One, assuming the median closing at Bonita Beach Road and Miller Road/Hunter's Ridge Blvd., to gauge the project's impact on operations and safety. The analysis must incorporate all vested trips and trips generated by the Horizon Park CPD build-out. At the time this report was prepared, this information had not been received. Without this information, it is not possible to determine the impact on the road network, and not possible to recommend approval of the bonus density units.

The Applicant does indicate that a westbound right turn lane is warranted. If this application is approved, Staff recommends Condition 8.a., requiring the Applicant to install a raised island median to prevent eastbound left turns into Miller Road. Staff also recommends Condition 7.b. to assess the impacts of the median closing.

The Applicant's TIS (original and amended versions) are available for review as part of the Application backup.

Stormwater/Drainage

The existing property generally drains from the south frontage (Bonita Beach Road), north and outfalls to the Imperial River. There are no offsite flows entering the property. The post development flows will emulate and maintain the general characteristics of the predevelopment historic flow. The proposed water management system will consist of either wet and/or dry detention to treat and attenuate the 25-year storm event. The project will be designed in accordance with South Florida Water Management District (SFWMD) regulatory requirements and, as outlined in the City's Comprehensive Plan, will provide water quality treatment for one hundred and fifty percent (150%) of the SFWMD requirement. Lakes and/or dry detention will be created and incorporated into the design. Consistent with the City of Bonita Springs Land Development Code, the Applicant will be required to demonstrate no adverse offsite stormwater impacts by incorporating the project into the appropriate City of Bonita Springs Interconnected Pond and Channel Routing (ICPR) Model. ICPR flood routing will need to be provided at the time of Development Order Permitting.

Floodplain

The project is located in the mapped special flood hazard area (100-year flood zone) identified as zones AE with a Regulatory Floodway. As part of the SFWMD permitting, two floodplain compensation areas are proposed with shallow excavation of existing higher areas. These areas will not be utilized for onsite attenuation or treatment, only for river overbank flooding.

Proposed structures, utilities and equipment shall be reviewed for elevation and flood design compliance at time of permit application. Any development within the floodway will require a Certificate of No Rise.

Comprehensive Plan Considerations

Future Land Use:

The proposed project is located in the Interchange Commercial future land use category. In the Bonita Plan, the Interchange Commercial future land use category is described as follows:

Policy 1.1.15: Interchange Commercial - Intended for uses that serve the traveling public such as automobile service/gas stations, hotel/motel, restaurants and gift shops; and a broad range of tourist-oriented, general commercial, light industrial, commercial office, and multi-family residential up to 10 dwelling units per acre with the approximately 385 acres of gross land area in the land use category.

- a. *If affordable housing is provided, residential density may be increased by up to five additional dwelling units per acre.*
- b. *Maximum allowable height of structures shall be 75 feet from the base flood elevation to the eaves.*
- c. *Nonresidential uses shall be limited to a maximum floor area ratio (FAR) of 1.2*

The proposed project is residential and commercial in nature and includes uses that are listed in Policy 1.1.15.

Bonus Density Units

As stated previously, this RPD/CPD allows for up to 205 multi-family dwelling units (standard density) and up to 10,000 sf of commercial floor area. The Applicant is requesting an additional 53 bonus density units Pursuant to Bonita Plan Future Land Use Policy 1.1.2 and 1.15.b. The Applicant has the right to apply for the 205 standard units, but the 47 bonus density units cannot be awarded until formally applied for, reviewed, and approved by City Council. In nature, the language set forth for bonus density in the Bonita Plan is general, but it is the City's LDC and Administrative Codes that set forth the implementation language. The Interchange Commercial Future Land Use Area allows for up to 15 dwelling units per acre if certain criteria are met per Future Land Use Policy 1.15.b.4. These are:

4. (a) The resulting development does not have substantially increased intensities of land use along its perimeter unless it is adjacent to an existing or approved development of a similar intensity;

(b) The additional traffic will not be required to travel through areas with significantly lower densities before reaching the nearest collector or arterial road;

(c) Existing and committed public facilities are not so overwhelmed that a density increase would be contrary to the overall public interest;

(d) There will be no decrease in required open space, buffering, landscaping and preservation areas or cause adverse impacts on surrounding land uses; and,

(e) Storm shelters or other appropriate mitigation is provided if the development is located within the Category 1 Storm Surge Area for a land-falling storm as defined by the most current Hurricane Storm Tide Atlas for Lee County prepared by the Florida Department of Emergency Management.

The Bonita Plan defines *public facilities* as transportation systems or facilities, sewer systems or facilities, solid waste systems or facilities, drainage systems or facilities, potable water systems or facilities, educational systems or facilities, parks and recreation systems or facilities and public health systems or facilities.

The Applicant has requested 47 bonus density units. The application has satisfied the intensity

and density criterion. Open space, buffering and landscaping are not deviated because of the requested density, and storm shelters are not required since the property is located outside of the Category 1 Storm Surge Area. However, as previously discussed in the “Traffic” portion, insufficient information was provided at the time this report was prepared to determine the effect on the public facilities.

Therefore, Staff is recommending denial of the bonus density units at this time. Should additional information be provided to allow for Staff analysis of the proposed effects on existing and committed public facilities, Staff may support with the bonus density request.

Transportation Element

The Applicant provided a TIS, which was reviewed by the City’s transportation engineer and analyzed earlier in this Report. Additionally, Objective 1.2 of the Transportation Element provides for the City’s Complete Streets Policy, which is designed to provide for multi-modal transportation opportunities. The project will be providing multi-modal transportation opportunities, including sidewalks on both sides of Miller Road and on the portion of Orr Road used for resident access.

The proposed RPD/CPD will result in less traffic when compared to the CPD that is currently approved for the site. There is sufficient reserve capacity on Bonita Beach Road to accommodate the trips associated with the proposed RPD/CPD. Therefore, Bonita Beach Road is expected to operate at an acceptable level of service with this proposed project with the exception of the excessive queueing that is likely in the eastbound left-turn lane at Trade Way One.

To further the intent of Complete Streets, Staff is proposing Condition 7.d. to further demonstrate consistency with the transportation policies of the City.

Conservation and Coastal Management Element

The proposed CPD/RPD will provide 40% open space for the residential component and 20% open space for the commercial tract, and will be consistent with Policy 7.2.2.

Planned Development Analysis, Formal Findings LDC 4-131 and LDC 4-299

Review criteria	Yes – Mostly - Partly - No
Demonstrate compliance with the Bonita Plan, this Land Development Code, and any other applicable code or regulation; and	Mostly; The Applicant has not provided the necessary information to assess bonus density at this time.
The request meets or exceeds performance and location standards set forth for the proposed uses; and	Partly; The Applicant requested several deviations from performance and location standards. Staff Analysis and recommendations are provided in the Staff Report.

Including the use of TDR or affordable housing bonuses are the densities or intensities (general uses) consistent with the Comprehensive Plan; and	Mostly; The maximum density with bonus is 15 dwelling units/acre. The request is for around 12 dwelling units/acre, which is below the allotted maximum based on the applicable future land use categories. The Applicant has not provided the necessary information to assess bonus density units at this time.
The request is compatible with existing or planned uses in the surrounding area; and	Yes; The proposal is for residential and commercial development. The proposed residential development is located closest to the nearby residential uses. The proposed commercial development is located where commercial is intended and is located near and across from other commercial uses.
Approval of the request will not place an undue burden upon existing transportation or planned infrastructure facilities and will be served by streets with the capacity to carry traffic generated by the development; and	Mostly; It is Staff's opinion that this project carries a less intense trip generation than the currently approved CPD for multiple fast-food restaurants. The conditions will provide the necessary improvements.
Will the request adversely affect environmentally critical areas and natural resources; and	No; the site has previously been disturbed.
Public facilities are, or will be, available and adequate to serve the proposed land use; and	Yes, as conditioned.
The proposed use or mix of uses is appropriate at the subject location; and	Yes
The recommended conditions to the concept plan and other applicable regulations provide sufficient safeguards to the public interest; and	Yes
The recommended conditions are reasonably related to the impacts on the public's interest created by or expected from the proposed development; and	Yes
Deviations enhance the achievement of the objectives of the planned development and preserve and promote the general intent of this	Mostly; Staff analysis and recommendations of the deviations are found in the Staff report. In summary, there were six (6) deviations requested; Staff is recommending denial of one

chapter to protect the public health, safety and welfare	(1). Staff is recommending partial denial of one (1). Staff is recommending approval of four (4).
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PUBLIC HEARING APPLICATION FOR PLANNED DEVELOPMENT

Community Development Department | 9220 Bonita Beach Road, Suite 111 | Bonita Springs, FL 34135 | Phone: (239) 444-6150 | Fax: (239) 444-6140

Applicant's Name: Madison Capital Group Management, LLC

Project Name: Bonita 75 CPD/RPD

STRAP Number(s): Please see Exhibit I-B-3 List of Property Owners

Application Form: X **Computer Generated*** **City Printed**

* By signing this application, the applicant affirms that the form has not been altered.

STAFF USE ONLY

Case Number: _____ Date of Application: _____

Fee: _____

Current Zoning: _____

Land Use Classification(s): _____ Comp. Plan Density: _____

Date of Zoning Public Hearing: _____ Date of City Council Public Hearing: _____

Planner Assigned: _____

Staff Recommendation: _____

TYPE OF APPLICATION

_____ DRI _____ PD – Existing Development _____ PD – Amendment

X _____ Option 1 _____ Option 2

**PART I
APPLICANT\PROPERTY OWNERSHIP INFORMATION**

A. Name(s) of applicant(s): Madison Capital Group Management, LLC
Mailing Address: Street: C/O Todd M. Bleakley, CCIM, 6805 Morrison Blvd , Suite 250
City: Charlotte State: NC Zip: 28211
Phone Number: Area Code: 407 Number: 756-9081 Ext: _____
E-mail: Tbleakley@madisoncapgroup.com

B. Relationship of applicant to property:
_____ Owner _____ Trustee _____ Option holder _____ Lessee Contract Purchaser
_____ Other (indicate): _____

*If applicant is NOT the owner or the person authorized by the Covenant of Unified Control, submit a **Notarized Authorization Form** from the owner or his authorized representative. Label as Exhibit I-B.*

** If the application is City-initiated, enter the date the action was initiated by the Council: N.A. Attach a copy of the "green sheet" and a list of all property owners, and their mailing addresses, for all properties within the area described. Names and addresses must be those appearing on the latest tax rolls of Lee County. Label the "green sheet" as "Exhibit I-B-2" and the list as "Exhibit I-B-3". **[Sec. 4-193]***

C. Name of owner(s) of property: BONITA CYPRESS LLC
Mailing Address: Street: 1110 EUCLID AVE STE 300
City: Cleveland State: OH Zip: 44115
Phone Number: Area Code: 407 Number: 756-9081 Ext: _____
Fax Number: Area Code: _____ Number: _____

D. Date property was acquired by present owner(s): 2015

E. Is the property subject to a sales contract or sales option? _____ NO YES

F. Is owner(s) or contract purchaser(s) required to file a disclosure form? _____ NO YES. If yes, please complete and submit Exhibit I-F (attached).

G. Are there any existing deed restrictions or other covenants on this property which may affect this request?
 NO _____ YES. If yes, submit a copy of the deed restrictions or other covenants and a statement explaining how the restrictions may affect the requested action. Label as "Exhibit I-G".

H. Authorized Agent(s): List names of authorized agents (submit additional sheets if necessary).
Name: Q. Grady Minor & Associates, P.A. and Pavese Law Firm
Address: 3800 Via Del Rey, Bonita Springs and 1833 Hendry Street, Ft. Myers
Contact Person: D. Wayne Arnold and Neale Montgomery
Phone: 239-947-1144 / 239-336-6235 E-mail: warnold@gradyminor.com / NealeMontgomery@Paveselaw.com

**PART II
GENERAL INFORMATION**

A. Request:

1. Rezoning from CPD TO:(check all applicable)

<u>X</u>	RPD - Residential	_____	MPD - Mixed Use
_____	MHPD - Mobile Home	_____	RVPD - Recreational Vehicle
<u>X</u>	CPD - Commercial	_____	CFPD - Community Facilities
_____	IPD - Industrial	_____	AOPD - Airport Operations

2. Option Chosen: X Option 1 _____ Option 2

3. Other - Provide specific details. N.A.

B. Legal Description and Boundary Sketch: Is property within a platted subdivision recorded in the official Plat Books of Lee County?

X NO. Attach a legible copy of the legal description (label it Exhibit II-B-1.) and Certified sketch of description as set out in chapter 5J-17.053. (labeled Exhibit II-B-2.). **If the legal description is available on computer disc (Word or Word Perfect) please provide a copy at time of application.**

_____ YES. Property is identified as:

Subdivision Name: _____

Plat Book: _____ Page: _____ Unit: _____ Block: _____ Lot: _____

Section: _____ Township: _____ Range: _____

Attach a copy of the Plat Book page with subject property clearly marked. Label this Exhibit II-B-3.

C. Project Street Address: Boondock Road and Orr Road

D. General Location Of Property (referenced to major streets): _____

Northeast quadrant of Bonita Beach Road and Interstate 75

E. City of Bonita Springs Plan Information

1. City of Bonita Springs Land Use Classification: Interchange Commercial

2. Are you proposing any City of Bonita Springs amendments which could affect the subject property?
X NO _____ YES If yes, submit a copy of the proposed amendment (labeled as "Exhibit II-E-1") along with a statement as to how the proposed amendment will affect your property (labeled as "Exhibit II-E-2").

F. Drainage, Water Control and Other Environmental Issues

1. Is the property within an Area of Special Flood Hazard as indicated in the Flood Insurance Rate Maps (FIRM)s?

_____ NO X YES. If yes, specify the minimum elevation required for the first habitable floor).
AE 14' NGVD (MSL) NAVD

2. Are there any environmentally sensitive lands such as, but not limited to: wetlands, mangrove forests, creek & river shorelines, sand dunes, xeric scrub, mature pine forests, or other unique land forms as defined in the Bonita Plan Goal 15 and it's Objectives and Policies, Objective 4.1, Policies 7.1.1 d. 2, 7.2.3, Goal 14 and Policies 14.1.1 through 14.3.5 and applicable sections of the Land Development Code (LDC). Are there any listed species occupied habitat as defined in the Bonita Plan or LDC on the subject property, Bonita Plan Policy 7.1.1 d. 2, 7.4.1 through 7.10.3, Objective 7.12 and Policies 7. 12.1 through 7.12.3, and applicable sections of the LDC?

X NO _____ YES If yes, delineate these areas on a map or aerial photo and label it Exhibit II-F-1. Also, complete Exhibit II-F-2 attached hereto.

G. Present Use of Property: Is the property vacant? _____ NO X YES

If the property is not vacant, the owner or applicant's signature on this application indicates that the Owner agrees to either remove all existing buildings and structures, OR that the proposed use of the building or structure(s) will be in compliance with all applicable requirements of the Land Development Regulations. **[Sec. 4-194(b)(3)]**

Briefly describe current use of the property: The property is currently undeveloped.

H. Property Dimensions

- 1. Width (average if irregular parcel): 1,100+/- Feet
- 2. Depth (average if irregular parcel): 1,000+/- Feet
- 3. Frontage on road or street: 640+/- Feet on Bonita Beach Road
(Name of street)
- 4. Total land area: 21+/- Acres or Square Feet

I. Land Area Calculations

- 1. Undevelopable Areas:
 - a. Freshwater wetland areas 0
 - b. Other wetland areas 0
 - c. Submerged land subject to tidal influence: 0
 - d. Total (a + b + c): 0
- 2. Remaining developable land (H.4 less I.1.d): 21+/-

**PART III
PROPOSED DEVELOPMENT**

A. Nature of Request

1. Will the development contain living units? _____ NO YES. If the answer is yes, please indicate the total number of living units proposed, by type:

_____ Single Family	_____ Mobile Homes	_____ Recreational Vehicles
_____ Zero-Lot-Line	_____ Duplex/Two Family	_____ Townhouses
<input checked="" type="checkbox"/> _____ Multiple Family	252	_____ TOTAL ALL TYPES

2. If the development will contain living units, please complete Exhibit III-A-2 (attached) and enter the following information:

a. PERMITTED total units (from Exhibit III-A-2): _____

b. PROPOSED total units (from A-1. above): 252

c. PROPOSED density (from Exhibit III-A-2): _____

3. Will the development contain non-residential areas? _____ NO YES. If the answer is yes, please indicate the size [gross square footage (gsf) unless indicated otherwise] of each general class of uses below:

Retail: 10,000 Total gsf

Offices: _____ Total gsf

Medical: _____ gsf Non-medical: _____ gsf

Hotel/Motel: 0 Total units

Size of units: _____ 0-425 sq. Ft. _____ 426-725 sq. Ft. _____ 726 or more sq. Ft.

Industrial: 0 Total gsf

Under roof: _____ gsf Not under roof: _____ gsf

Mines, Quarries, or General Excavation: Acres to be excavated: 0

Other-specify: _____

Number of Beds (if applicable): 0 OR: 0 gsf

4. Building Height

55 Maximum height of buildings (in feet above grade)

2 Number of Habitable Floors

5. Aviation Hazard: Do you propose any structures, lighting, or other features that might affect safe flight conditions?

NO _____ YES. If yes, please submit an explanation and label it Exhibit III-A-5.

B. Facilities

1. Fire District: Bonita Springs Fire District

2. Water Supply

a. Estimated daily consumption of potable water:

1. Residential units: 63000 gpd

2. Mobile Home units: _____ gpd

3. Rec. Vehicle units: _____ gpd

4. Commercial: 1000 gpd

5. Industrial: _____ gpd

b. Source of potable water: Bonita Springs Utilities

c. Do you have a written agreement from the utility company to serve your project?

NO _____ YES. If yes, please submit a copy of the agreement.

d. Source of Non-potable water: Bonita Springs Utilities

3. Sanitary Sewer Service

a. Estimated daily production of wastewater:

1. Residential units: 63000 gpd

2. Mobile Home units: _____ gpd

3. Recreational Vehicles: _____ gpd

4. Commercial: 1000 gpd

5. Industrial: _____ gpd

b. Is any special effluent anticipated? NO _____ YES. If yes, please complete Exhibit III-B-3 (attached).

c. Source of sanitary sewer service: Bonita Springs Utilities

d. Do you have a written agreement from the utility company to serve your project?

NO _____ YES. If yes, please submit a copy of the agreement.

e. Will a private on-site disposal facility be used? NO _____ YES. If yes, please complete Exhibit III-B-3 (attached).

f. Are individual sewage disposal systems proposed? NO _____ YES.

C. Transportation

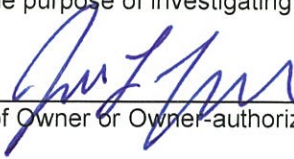
1. Has this project been exempted from filing a Traffic Impact Statement?

NO _____ YES _____ NOT REQUIRED (Exist. development). If it has been exempted, attach a copy of the exemption and label it Exhibit III-C.

PART V

AFFIDAVIT

I, Joe F. Teague, Jr., Manager certify that I am the owner or authorized representative of the property described herein, and that all answers to the questions in this application and any sketches, data or other supplementary matter attached to and made a part of this application, are honest and true to the best of my knowledge and belief. I also authorize the staff of the City of Bonita Springs Community Development to enter upon the property during normal working hours for the purpose of investigating and evaluating the request made thru this application.


Signature of Owner or Owner-authorized Agent

9.14.21
Date

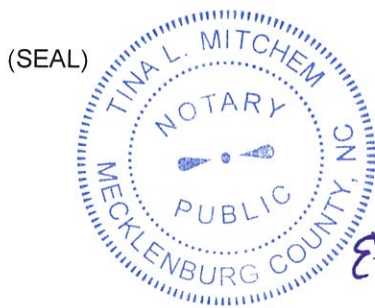
Joe F. Teague, Jr. as Manager of Madison Capital Group Management, LLC
Typed or printed name and title

STATE OF FLORIDA) NORTH CAROLINA
COUNTY OF LEE) MECKLENBURG

The foregoing instrument was certified and subscribed before me this 14 day of September 20²¹, by Joe F. Teague, Jr. as Manager, who is personally known to me or who has produced _____ as identification.


Signature of notary public

Tina L Mitchem
Printed name of notary public



Expires 4-15-24

**EXHIBIT I-F
DISCLOSURE OF INTEREST FORM FOR:**

STRAP NO. See Exh I-B-3 List of Property Owners **CASE NO.** _____

1. If the property is owned in fee simple by an INDIVIDUAL, tenancy by the entirety, tenancy in common, or joint tenancy, list all parties with an ownership interest as well as the percentage of such interest.

Name and Address	Percentage of Ownership
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____

2. If the property is owned by a ^{LIMITED LIABILITY COMPANY} CORPORATION, list the officers and stockholders and the percentage of stock owned by each.

Name, Address, and Office	Percentage of ^{MEMBERSHIP} Stock ^{INTEREST}
CRM Development IV, 24870 Burnt Pine Drive #1, 34134	50
MANBRO R.E. IV, LLC , 1000 Lakeside Avenue, Cleveland, Ohio 44114	50
_____	_____
_____	_____
_____	_____
_____	_____

3. If the property is in the name of a TRUSTEE, list the beneficiaries of the trust with percentage of interest.

Name and Address	Percentage of Interest
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____

4. If the property is in the name of a GENERAL PARTNERSHIP OR LIMITED PARTNERSHIP, list the names of the general and limited partners.

Name and Address	Percentage of Ownership
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____

5. If there is a CONTRACT FOR PURCHASE, whether contingent on this application or not, and whether a Corporation, Trustee, or Partnership, list the names of the contract purchasers below, including the officers, stockholders, beneficiaries, or partners.

Name, Address, & Office (if applicable)	Percentage of Stock
Madison Capital Group Management, LLC	100

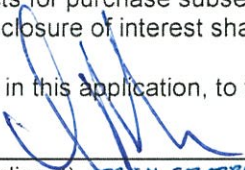
Date of Contract: _____

6. If any contingency clause or contract terms involve additional parties, list all individuals or officers, if a corporation, partnership, or trust.

Name and Address

For any changes of ownership or changes in contracts for purchase subsequent to the date of the application, but prior to the date of final public hearing, a supplemental disclosure of interest shall be filed.

The above is a full disclosure of all parties of interest in this application, to the best of my knowledge and belief.

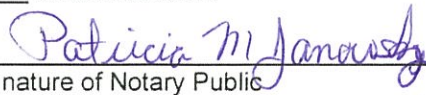
Signature: 
 (Applicant) STEVEN CALABRESE, PRESIDENT OF T&F CORP., G.P. OF CCA G L.P.,
MANAGING MEMBER OF CRT DEVELOPMENT IV, LLC,
MANAGING MEMBER OF BONITA CYPRESS, LLC

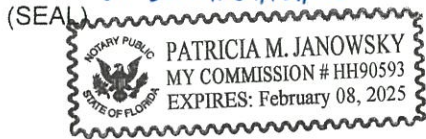
 (Printed or typed name of applicant)

STATE OF FLORIDA
 COUNTY OF LEE

The foregoing instrument acknowledged before me this 8 day of July 2021, by Steven Calabrese, who is personally known to me or who has produced Lisena as identification.

→ STEVEN CALABRESE, PRESIDENT OF T&F CORP., G.P. OF CCA G L.P., MANAGING MEMBER OF CRT DEVELOPMENT IV, LLC, MANAGING MEMBER OF BONITA CYPRESS, LLC


 Signature of Notary Public



Patricia M Janowsky
 Printed Name of Notary Public

5. If there is a CONTRACT FOR PURCHASE, whether contingent on this application or not, and whether a Corporation, Trustee, or Partnership, list the names of the contract purchasers below, including the officers, stockholders, beneficiaries, or partners.

Name, Address, & Office (if applicable)	Percentage of Stock
Madison Capital Group Management, LLC	100
Madison Capital Group Holdings, LLC	100
Ryan Hanks, President - 6805 Morrison Blvd., Suite 250, Charlotte, NC 28211	80
Joe F. Teague, Jr., VP - 6805 Morrison Blvd., Suite 250, Charlotte, NC 28211	20

Date of Contract: April 16, 2021

6. If any contingency clause or contract terms involve additional parties, list all individuals or officers, if a corporation, partnership, or trust.

Name and Address

For any changes of ownership or changes in contracts for purchase subsequent to the date of the application, but prior to the date of final public hearing, a supplemental disclosure of interest shall be filed.

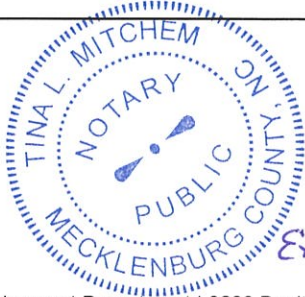
The above is a full disclosure of all parties of interest in this application, to the best of my knowledge and belief.

Signature: [Handwritten Signature]
 (Applicant)

Joe F. Teague, Jr., Manager
 (Printed or typed name of applicant)

STATE OF NORTH CAROLINA
 COUNTY OF MECKLENBURG

The foregoing instrument acknowledged before me this 7 day of July, 2021, by Joe F. Teague, Jr., who is personally known to me or who has produced _____ as identification.



[Handwritten Signature]
 Signature of Notary Public

Tina L Mitchem
 Printed Name of Notary Public

(SEAL)

Bonita 75

Exhibit I-B-3 List of Property Owners

STRAP	STREET NUMBER	STREET NAME	OWNER NAME	ADDRESS	CITY	STATE	ZIP
314726B4000070100	27877	BOONDOCK RD	BONITA CYPRESS LLC	1110 EUCLID AVE STE 300	CLEVELAND	OH	44115
314726B4000070110	27901	BOONDOCK RD	BONITA CYPRESS LLC	1110 EUCLID AVE STE 300	CLEVELAND	OH	44115
314726B4000070130	27925	BOONDOCK RD	BONITA CYPRESS LLC	1110 EUCLID AVE STE 300	CLEVELAND	OH	44115
314726B4000070140	27952	BOONDOCK RD	BONITA CYPRESS LLC	1110 EUCLID AVE STE 300	CLEVELAND	OH	44115
314726B4000070190	27802	BOONDOCK RD	BONITA CYPRESS LLC	1110 EUCLID AVE STE 300	CLEVELAND	OH	44115
314726B4000070220	27977	BOONDOCK RD	BONITA CYPRESS LLC	1110 EUCLID AVE STE 300	CLEVELAND	OH	44115
314726B4000070290	27801	BOONDOCK RD	BONITA CYPRESS LLC	1110 EUCLID AVE STE 300	CLEVELAND	OH	44115
314726B4000070300	27825	BOONDOCK RD	BONITA CYPRESS LLC	1110 EUCLID AVE STE 300	CLEVELAND	OH	44115
314726B4000070310	27853	BOONDOCK RD	BONITA CYPRESS LLC	1110 EUCLID AVE STE 300	CLEVELAND	OH	44115
314726B4000070510	27750	ORR RD	BONITA CYPRESS LLC	1110 EUCLID AVE STE 300	CLEVELAND	OH	44115

**EXHIBIT II-F-2
ENVIRONMENTAL ISSUES**

A. Topography: Describe the range of surface elevations of the property:

10.02' to 14.02' NAVD

B. Sensitive Lands: Identify any environmentally sensitive lands, including, but not limited to, wetlands (as defined in the Comprehensive Plan), flow ways, creek beds, sand dunes, other unique land forms [see the Comprehensive Plan for listed species occupied habitat (see Sec. 4-1337 et seq. of the Land Development Code).

N.A.

C. Preservation/Conservation of Natural Features: Describe how the lands listed in B. above will be protected by the completed project:

N.A.

D. Shoreline Stabilization: If the project is located adjacent to navigable natural waters, describe the method of shoreline stabilization, if any, being proposed:

N.A.

**EXHIBIT III-A-2
PRELIMINARY DENSITY CALCULATIONS***

*****Included in residential density calculation**

A. Gross Residential Acres

1. Total land area:	20.996+/- acres
2. Area to be used for non-residential uses: (Line A.2.a. plus A.2.b.):	2+/-*** acres
a. R-O-W providing access to non-residential uses:	0.5+/- acres
b. Non-residential use areas:	1.5+/- acres
3. Gross residential acres (Line A.1 less A.2):	20.55+/- acres
a. Uplands areas	20.55+/- acres
b. Freshwater Wetlands areas	0 acres
c. Other Wetland areas	0 acres

less FDOT Parcels (0.44 AC)

B. Comprehensive Plan Land Use Classification: IC (If more than one classification, calculations for each classification must be submitted)

Density Standards (from the Comprehensive Plan)

1. Maximum density for Land Use Classification:	10 units/gross res. acre
2. Maximum total density for Land Use Classification:	15 units/gross res. acre

MAXIMUM PERMITTED DWELLING UNITS

C. High Density Residential, High Density Mixed Use/Village, "Old 41" Town Center Mixed Use Redevelopment Overlay Area. (Interchange Commercial)

1. Standard density uplands units (A.3.a. times B.1)	205 units
2. Standard density wetlands units (A.3.b. & A.3.c. times B.1)	0 units
3. Total standard density units (sum of C.1 & C.2)	205 units
4. Max. Total density units [A.3.a. times ((B.1 plus 1/2 of (B.2 less B.1)))]	256 units
5. Sub-total permitted std. density units (line C.3 or C.4 - whichever is less):	<u>205</u> Units Sub-total
6. BONUS UNITS (REQUESTED)	
a. Low-moderate housing density:	47 units
b. TDR units:	0 units
c. Sub-total (C.6.a plus C.6.b)	47 units
7. Total Permitted Units (C.5. plus C.6.c):	<u>252</u> Units Total

NOTE: may not exceed (A.3.a. times B.2) plus (A.3.b. and A.3.c. times .05).

* Subject to staff review and correction.

D. Moderate Density Mixed Use/Planned Development, Medium Density Multi-Family Residential, Medium Density Residential, Moderate Density Residential

1. Standard density uplands units (A.3.a. times B.1)	_____ units
2. Standard density freshwater wetlands units (A.3.b. times B.1)	_____ units
3. Total standard density units (sum of D.1 & D.2)	_____ units
4. Maximum upland density (A.3.a. times 8)	_____ units
5. Total permitted units (line D.3 or D.4 - whichever is less):	_____ Units

E. Suburban Density Residential, Low Density Residential, Estate Residential

- 1. Standard density uplands units (A.3.a. times B.1) _____ units
- 2. Standard density freshwater wetlands units (A.3.b times B.1) _____ units
- 3. Total standard density (sum of E.1 & E.2) _____ units
- 4. Maximum upland density (A.3.a. times 4) _____ units
- 5. **Total permitted units** (line E.3 or E.4 - whichever is less): _____ **Units Total**

F. Conservation, Resource Protection, DRGR

- 1. Total acres of "Open Land" _____ acres
- 2. Maximum density (F.1 times 0.2*) _____ units
- 3. Total permitted units: _____ units

**EXHIBIT III-B-3
SANITARY SEWER FACILITIES**

A. **Special Effluent:** If special effluent is anticipated, please specify what it is and what strategies will be used to deal with its' special characteristics:

N.A.

B. **Private On-site Facilities:** If a private on-site wastewater treatment and disposal facility is proposed, please provide a detailed description of the system including:

1. Method and degree of treatment:

N.A.

2. Quality of the effluent:

N.A.

3. Expected life of the facility:

N.A.

4. Who will operate and maintain the internal collection and treatment facilities:

N.A.

5. Receiving bodies or other means of effluent disposal:

N.A.

C. **Spray Irrigation:** If spray irrigation will be used, specify:

1. The location and approximate area of the spray fields:

N.A.

2. Current water table conditions:

N.A.

3. Proposed rate of application:

N.A.

4. Back-up system capacity:

N.A.


**EXHIBIT I-B-4
COVENANT OF UNIFIED CONTROL**

The undersigned do hereby swear or affirm that they are the fee simple title holders and owners of record of property commonly known as Bonita 75 CPD (see Exhibit I-B-3) and legally described in exhibit A attached hereto.
(street address)

The property described herein is the subject of an application for planned development zoning. We hereby designate Madison Capital Group Management, LLC, Q. Grady Minor & Associates, P.A. and Pavese Law Firm as the legal representative of the property and as such, this individual is authorized to legally bind all owners of the property in the course of seeking the necessary approvals to develop. This authority includes but is not limited to the hiring and authorization of agents to assist in the preparation of applications, plans, surveys, and studies necessary to obtain zoning and development approval on the site. This representative will remain the only entity to authorize development activity on the property until such time as a new or amended covenant of unified control is delivered to the City of Bonita Springs.

The undersigned recognize the following and will be guided accordingly in the pursuit of development of the project:


1. The property will be developed and used in conformity with the approved master concept plan including all conditions placed on the development and all commitments agreed to by the applicant in connection with the planned development rezoning.
2. The legal representative identified herein is responsible for compliance with all terms, conditions, safeguards, and stipulations made at the time of approval of the master concept plan, even if the property is subsequently sold in whole or in part, unless and until a new or amended covenant of unified control is delivered to and recorded by the City of Bonita Springs.
3. A departure from the provisions of the approved plans or a failure to comply with any requirements, conditions, or safeguards provided for in the planned development process will constitute a violation of the Land Development Code.
4. All terms and conditions of the planned development approval will be incorporated into covenants and restrictions which run with the land so as to provide notice to subsequent owners that all development activity within the planned development must be consistent with those terms and conditions.
5. So long as this covenant is in force, City of Bonita Springs can, upon the discovery of noncompliance with the terms, safeguards, and conditions of the planned development, seek equitable relief as necessary to compel compliance. The City of Bonita Springs will not issue permits, certificates, or licenses to occupy or use any part of the planned development and the City may stop ongoing construction activity until the project is brought into compliance with all terms, conditions and safeguards of the planned development.



Owner
Steven Calabrese, President of TGF Corporation, the General Partner of CCAG Limited Partnership, the Managing Member of CRM Development IV, LLC the Managing Member of Bonita Cypress, LLC
Printed Name and Title

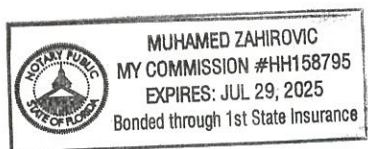
STATE OF FLORIDA
COUNTY OF LEE

Sworn to (or affirmed) and subscribed before me this 10th day of September 2021, by Steve Calabrese*, who is personally known to me or who has produced Driver License as identification.



Notary Public
Muhammed Zahirovic

(Name typed, printed or stamped)
(Serial Number, if any)



*Steven Calabrese, President of TGF Corporation, the General Partner of CCAG Limited Partnership, the Managing Member of CRM Development IV, LLC the Managing Member of Bonita Cypress, LLC

EXHIBIT A
LEGAL DESCRIPTION

PARCEL 1:

Commence at the Southeast corner of the SW 1/4 of Section 31, Township 47 South, Range 26 East, thence along the South line of said Section, West 329.26 feet, thence North 00°18' East, 545 feet for a **Point of Beginning**, from the Point of Beginning continue North 00°18' East, 130.00 feet, thence East 329.55 feet to the East line of said Southwest 1/4; thence along said East line of said SW 1/4, South 00°19'30" West 130 feet; thence West 329.49 feet to the **Point of Beginning**; Being a part of the SE 1/4 of the SW 1/4 of said Section, reserving the West 30 feet for roadway purposes.

PARCEL 2:

Commencing at the Southeast corner of the SW 1/4 of Section 31, Township 47 South, Range 26 East; thence along the South line of said Section, West 329.26 feet; thence North 00°18' East 415 feet for a **Point of Beginning**; from the **Point of Beginning** continue North 00°18' East 130 feet; thence East 329.49 feet to the East line of said SW 1/4, thence along said East line of said SW 1/4 South 00°19'30" West 130 feet; thence West 329.44 feet to the **Point of Beginning**, being a part of the SE 1/4 of the SW 1/4 of said Section, reserving the West 30 feet for roadway purposes.

PARCELS 3, 4 and 6:

A parcel of land located in the SW 1/4 of Section 31, Township 47 South, Range 26 East, Lee County, Florida, being more particularly described as follows:

Commence at the Southeast corner of the SW 1/4 of Section 31, Township 47 South, Range 26 East, Lee County, Florida; thence run North 90°00'00" West, along the South line of the SW 1/4 of said Section 31, for a distance of 329.90 feet; thence run North 00°18'00" East for a distance of 155.00 feet to the **Point of Beginning** of the Parcel of land herein described; thence run North 90°00'00" West for a distance of 30.30 feet to a point on the Northerly right of way line of Orr Road, a 60.00 foot right of way; thence run North 83°52'05" West, along the Northerly right of way line of said Orr Road, for a distance of 194.44 feet; thence run North 57°26'42" West, along the Northeasterly right of way line of said Orr Road, for a distance of 201.40 feet; thence run North 24°53'24" West along the Easterly right of way line of said Orr Road, for a distance of 330.05 feet; thence run North 22°55'35" West long the Easterly right of way line of said Orr Road, for a distance of 240.11 feet; thence run North 00°15'00" East along the Easterly right of way line of said Orr Road, for a distance of 0.32 feet; thence run North 90°00'00" West for a distance of 30.00 feet; thence run North 00°15'00" East for a distance of 130.00 feet; thence run South 90°00'00" East for a distance of 659.34 feet; thence run South 00°18'00" West for a distance of 520.01 feet; thence run South 90°00'00" East for a distance of 329.85 feet; thence run South 00°17'33" West for a distance of 308.31 feet; thence run North 82°52'59" West for a distance of 301.45 feet; thence run North 79°43'30" West for a distance of 31.04 feet; thence run North 00°18'00" East for a distance of 5.42 feet to the **Point of Beginning** of the Parcel of land herein described.

PARCEL 5:

Commencing at the Southeast corner of the SW 1/4 of Section 31, Township 47 South, Range 26 East, thence along the South line of said Section 31, West 329.26 feet, thence North 00°18' East, 935 feet for the **Point of Beginning**; from the **Point of Beginning**, run West 329.67 feet, thence North 00°16'30" East 130 feet, thence East 329.72 feet, thence South 00°18' West 130 feet to the **Point of Beginning**, being a part of the SE 1/4 of the SW 1/4 of said Section, reserving the East 30 feet for roadway purpose.

PARCEL 7:

Commencing at the Southeast corner of the SW 1/4 of Section 31, Township 47 South, Range 26 East, thence along the South line of said Section West 329.26 feet from the Point of Beginning continue North 0°18' East 935 feet for a **Point of Beginning**; thence continue North 00°18' East 130 feet; thence East 329.73 feet to the East line of said SW 1/4; thence along said East line of said SW 1/4, South 00°19'30" West 130 feet; thence West 329.67 feet to the **Point of Beginning**, being a part of the SE 1/4 of the SW 1/4 of said Section, reserving the West 30 feet for roadway purposes.

PARCEL 8:

Commencing at the Southeast corner of the SW 1/4 of Section 31, Township 47 South, Range 26 East, thence along the South line of said Section West 329.26 feet; thence North 00°18' East 805 feet for a **Point of Beginning**; from the Point of Beginning continue North 00°18' East 130 feet; thence East 329.67 feet to the East line of said SW 1/4; thence along said East line of said SW 1/4, South 00°19'30" West 130 feet; thence West 329.61 feet to the **Point of Beginning**, being a part of the SE 1/4 of the SW 1/4 of said Section, reserving the West 30 feet for roadway purposes.

PARCEL 9:

Commencing at the Southeast corner of the SW 1/4 of Section 31, Township 47 South, Range 26 East, thence along the South line of said Section West 329.26 feet; thence North 0°18' East 675 feet for a **Point of Beginning**; thence continue North 00°18' East 130 feet; thence East 329.61 feet to the East line of said SW 1/4; thence along said East line of said SW 1/4, South 00°19'30" West 130 feet; thence West 329.55 feet to the **Point of Beginning**, being a part of the SE 1/4 of the SW 1/4 of said Section, reserving the West 30 feet for roadway purposes.

PARCEL 10:

Parcel A:

Commencing at the SE corner of the SW 1/4 of Section 31, Township 47 South, Range 26 East, thence along the South line of said Section West 987.78 feet; thence North 0°15 East 805.00 feet for a **Point of Beginning**; thence West 329.61 feet to the West line of the Southeast 1/4 of the Southwest 1/4 of said Section; thence along said West line North 0°13'30" East 130.0 feet; thence East 329.67 feet; thence South 0°15' West 130 feet to the **Point of Beginning**, being a part of the Southeast 1/4 of the Southwest 1/4 of said Section, reserving the East 30 feet for roadway purposes.

Parcel B:

Commencing at the SE corner of the SW 1/4 of Section 31, Township 47 South, Range 26 East, thence along the South line of said Section West 987.78 feet; thence North 0°15 East 935.00 feet for a **Point of Beginning**; thence West 329.67 feet to the West line of the Southeast 1/4 of the Southwest 1/4 of said Section; thence along said West line North 0°13'30" East 130.0 feet; thence East 329.72 feet; thence South 0°15' West 130.0 feet to the **Point of Beginning**, being a part of the Southeast 1/4 of the Southwest 1/4 of said Section, reserving the East 30 feet for roadway purposes.

Parcel C:

Commencing at the SE corner of the SW 1/4 of Section 31, Township 47 South, Range 26 East, thence along the South line of said Section West 987.78 feet; thence North 0°15 East 1065.00 feet for a **Point of Beginning**; thence West 329.72 feet to the West line of the Southeast 1/4 of the Southwest 1/4 of said Section; thence along said West line North 0°13'30" East 130.0 feet; thence East 329.78 feet; thence South 0°15' West 130.0 feet to the **Point of Beginning**, being a part of the Southeast 1/4 of the Southwest 1/4 of said Section, reserving the East 30 feet for roadway purposes.

Parcel D:

Commencing at the SE corner of the SW 1/4 of Section 31, Township 47 South, Range 26 East, thence along the South line of said Section West 987.78 feet; thence North 0°15 East 1195 feet for a **Point of Beginning**; thence West 329.38 feet to the West line of the Southeast 1/4 of the Southwest 1/4 of said Section; thence along said West line North 0°13'30" East 122.25 feet; thence East 329.84 feet; thence South 0°15' West to the **Point of Beginning**, being a part of the Southeast 1/4 of the Southwest 1/4 of said Section, reserving the East 30 feet for roadway purposes.

LESS AND EXCEPT that portion of Parcel 124, included in that certain **ORDER OF TAKING** recorded in Official Records Book 1157, Page 13, pertaining to the above referenced Parcels A, B, C and D.

PARCEL 12:

All that portion of Orr Road vacated pursuant to Resolution No. 09-49 by the City of Bonita Springs, Florida, located in the Southeast 1/4 of the Southwest 1/4 of Section 31, Township 47 South, Range 26 East, Lee County, Florida, being more particularly described as follows:

Commence at the Southeast corner of the Southwest 1/4 of Section 31, Township 47 South, Range 26 East, Lee County, Florida; Thence run North 00°44'34" West, along the East line of the Southwest 1/4 of said Section 31, for a distance of 26.91 feet to a point on the North Right-of-Way of Bonita Beach Road (f/k/a Carrell Road), also being the **Point of Beginning** of the parcel of land herein described; Thence run South 88°57'15" West, along the said North Right-of-Way for a distance of 68.66 feet to the intersection of said North Right-of-Way line and the West Right-of-Way line of Access Road No. 3 - Orr Road (60 foot Right-of-Way); Thence run along said Right-of-Way for the following five (5) courses: Thence run North 01°03'33" West, for a distance of 27.82 feet; Thence run North 83°56'32" West, for a distance of 508.51 feet; Thence run North 58°29'27" West, for a distance of 233.00 feet; Thence run North 25°56'09" West, for a distance of 348.59 feet; Thence run North 23°58'20" West, for a distance of 266.87 feet to a point on the South line of the lands described in Official Records Book 4549, Page 4351; Thence run along said South line North 88°57'16" East, for a distance of 65.95 feet to a point on the Northeasterly Right-of-Way of Access Road No. 3 - Orr Road; Thence run along said Right-of-Way for the following five (5) courses: Thence run South 00°47'44" East for a distance of 1.85 feet; Thence run South 23°58'20" East, for a distance of 238.45 feet; Thence run South 25°56'09" East, for a distance of 330.05 feet; Thence run South 58°29'27" East, for a distance of 201.94 feet; Thence run South 83°56'32" East, for a distance of 557.11 feet to a point on the East line of the Southwest 1/4 of said Section 31; Thence run South 00°44'34" East, along said East line, for a distance of 79.68 feet to the **Point of Beginning** of the parcel of land herein described.

ADMINISTRATIVE AMENDMENT ADD15-23355-BOS

ADMINISTRATIVE ACTION
CITY OF BONITA SPRINGS, FLORIDA

WHEREAS, Q. Grady Minor & Associates, P.A. filed an application for administrative approval to a Commercial Planned (CPD) on a project known as Bonita 75 to amend the Schedule of Uses of the planned development (PD) to increase the number of free-standing fast food restaurants from 2 to 3 and eliminate several other uses on property located within Bonita 75 CPD; and

LEGAL DESCRIPTION: In Section 31, Township 47 South, Range 26 East, Lee County, Florida:

“See Exhibit A”

WHEREAS, the subject property is located at 27952, 27925 and 27977 Boondock Road in Bonita Springs; and

WHEREAS, the property was originally rezoned in 2008, through Lee County case DCI2004-00061, Bonita Springs Zoning Ordinance No. 08-10 and the Master Concept Plan was reinstated in 2015, through Green Sheet No. 15-08-0263; and

WHEREAS, the subject property is located in the Interchange Commercial Future Land Use Category as designated by the City of Bonita Springs Comprehensive Plan; and

WHEREAS, the Bonita Springs Land Development Code (LDC) provides for certain administrative changes to an approved final planned unit development plan for a planned development; and

WHEREAS, the Bonita 75 CPD currently allows two free-standing fast food restaurants; and

WHEREAS, the property owner desires to increase the number of free-standing fast food restaurants to a maximum of three; and

WHEREAS, the property owner also agrees to eliminate several other uses that are no longer appropriate for the proposed development; and

WHEREAS, the amendments to the Schedule of Uses as proposed are limited to the following:

- Rental or leasing establishments: Group II ~~and III~~
- Repair shops: Groups I, II, and III, ~~IV~~ (no outdoor storage or display)
- Restaurant, fast food – maximum 2-3 stand-alone fast food restaurants. No limit on fast food restaurants within a multi-occupancy building
- Social services: Group I, ~~II and III~~
- Storage: indoor ~~and outdoor~~

Used merchandise stores, Group I ~~and II, III, and IV~~ (excluding Pawnshops; no outdoor storage or display); and

WHEREAS, the subject application and plans have been reviewed by the City of Bonita Springs Community Development Department in accordance with the applicable regulations for compliance with all terms of the administrative approval procedures; and

WHEREAS, it is found that the proposed amendment does not increase density or intensity within the development; does not decrease buffers or open space required by the Land Development Code or Zoning Resolution; does not underutilize public resources or infrastructure; does not reduce total open space, buffering, landscaping to preservation areas; and does not otherwise adversely impact surrounding land uses; and

NOW, THEREFORE, IT IS HEREBY DETERMINED that the application for administrative amendment to a Commercial Planned Development (CPD) on a project known as Bonita 75 CPD to amend the Schedule of Uses of the planned development (PD) to increase the number of free-standing fast food restaurants from 2 to 3 and eliminate several other uses on property located within Bonita 75 is APPROVED subject to the following conditions:

1. The Development must be in substantial compliance with the four page Master Concept Plan, entitled "Bonita 75 CPD" stamped received May 23, 2008, by Lee County Development Services ("Attachment A"). A reduced copy is attached hereto.

2. The Schedule of Uses has been amended to :

Rental or leasing establishments: Group II

Repair shops: Groups I, II, and III (no outdoor storage or display)

Restaurant, fast food – maximum 3 stand-alone fast food restaurants. No limit on fast food restaurants within a multi-occupancy building

Social services: Group I

Storage: Indoor

Used merchandise stores, Group I and II (excluding Pawnshops; no outdoor storage or display); and

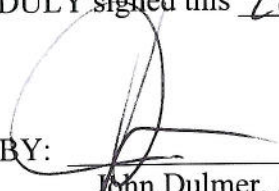
3. At time of Development Order, the applicant shall prepare an operational analysis that identifies the traffic movements along Bonita Beach Road at Miller Road/Hunters Ridge Boulevard and all of the adjacent intersections/median openings, east and west, that are affected by both the traffic to/from the project site and the closing of the median opening. Show turning movements for both the AM and PM peak periods, with and without the project, and before and after the median opening is closed. Analyze all conditions and time periods using accepted traffic engineering methods and evaluate the potential for traffic operational impacts and degradation of safety due to the project and the closing of the median opening. Indicate all improvements that will be needed to mitigate the impacts and maintain the safe flow of traffic. This evaluation should specifically include, but not be limited to, any locations where traffic accessing the site (entering or exiting) must make a U-turn on Bonita Beach Road. Provide a planning level cost estimate for right-of-way,

permitting, utility adjustments and construction. This cost estimate may result in the calculation of proportionate fair share of improvements to the impacted intersections.

4. At time of Development Order, Lee County reserves the right to modify the median opening on Bonita Beach Road in response to operational or safety concerns that may occur when the site is developed.
5. Any change not consistent with this approval will require approval from the City of Bonita Springs Community Development Department.
6. This approval does not supersede any recorded covenants, restrictions, or association documents.
7. All terms and conditions of the original zoning (Zoning Ordinance No. 08-10) remain in full force and effect, unless specifically modified by this approval.

DULY signed this 28 day of AUGUST, A.D., 2015

BY:



John Dulmer, AICP, Community Development Director
Department of Community Development

Exhibit A

**BONITA 75 CPD
LEGAL DESCRIPTION**

A PARCEL OF LAND LOCATED IN THE SOUTHEAST QUARTER OF THE SOUTHWEST QUARTER OF SECTION 31, TOWNSHIP 47 SOUTH, RANGE 26 EAST, LEE COUNTY, FLORIDA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCE AT THE SOUTHEAST CORNER OF THE SOUTHWEST QUARTER OF SECTION 31, TOWNSHIP 47 SOUTH, RANGE 26 EAST, COLLIER COUNTY, FLORIDA; THENCE RUN NORTH 00°44'34" WEST, ALONG THE EAST LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 31, FOR A DISTANCE OF 26.91 FEET TO A POINT ON THE NORTH RIGHT-OF-WAY OF BONITA BEACH (F/K/A CARRELL ROAD), THE SAME SHOWN ON THE STATE OF FLORIDA DEPARTEMENT OF TRANSPORTATION RIGHT-OF-WAY MAP, STATE ROAD No. 93 (I-75), LEE COUNTY, FLORIDA, SECTION 12075-2401, SHEET 5 OF 9, ALSO BEING THE POINT OF BEGINNING OF THE PARCEL OF LAND HEREIN DESCRIBED; THENCE RUN SOUTH 88°57'15" WEST, ALONG SAID NORTH RIGHT-OF-WAY, FOR A DISTANCE OF 34.33 FEET; THENCE RUN NORTH 01°02'45" WEST, FOR A DISTANCE OF 53.78 FEET; THENCE RUN NORTH 83°56'32" WEST, FOR A DISTANCE OF 532.60 FEET; THENCE RUN NORTH 58°29'27" WEST, FOR A DISTANCE OF 217.47 FEET; THENCE RUN NORTH 25°56'09" WEST, FOR A DISTANCE OF 339.32 FEET; THENCE RUN NORTH 23°58'20" WEST, FOR A DISTANCE OF 245.12 FEET; THENCE RUN NORTH 00°47'44" WEST, FOR A DISTANCE OF 7.87 FEET TO A POINT ON THE SOUTH LINE OF THE LANDS DESCRIBED IN OFFICIAL RECORDS BOOK 4549 AT PAGE 4351 OF THE PUBLIC RECORDS OF LEE COUNTY, FLORIDA; THENCE RUN SOUTH 88°57'16" WEST, ALONG SAID SOUTH LINE, FOR A DISTANCE OF 35.94 FEET TO A POINT ON THE NORTHEASTERLY RIGHT-OF-WAY OF STATE ROAD No. 93 (I-75); THENCE RUN ALONG SAID NORTHEASTERLY RIGHT-OF-WAY FOR THE FOLLOWING THREE (3) COURSES:
1) THENCE RUN NORTH 23°58'20" WEST, FOR A DISTANCE OF 12.89 FEET;
2) THENCE RUN NORTH 21°07'25" WEST, FOR A DISTANCE OF 446.23 FEET;
3) THENCE RUN NORTH 15°25'36" WEST, FOR A DISTANCE OF 79.83 FEET TO A POINT ON THE NORTH LINE OF THE SOUTHEAST QUARTER OF THE SOUTHWEST QUARTER OF SAID SECTION 31; THENCE RUN NORTH 88°57'01" EAST, ALONG SAID NORTH LINE, FOR A DISTANCE OF 246.19 FEET TO A POINT ON THE EAST LINE OF A 60 FOOT WIDE RIGHT-OF-WAY, BEING ORR ROAD; THENCE RUN SOUTH 00°47'44" EAST, ALONG SAID EAST RIGHT-OF-WAY, FOR A DISTANCE OF 378.17 FEET; THENCE RUN NORTH 88°57'20" EAST, FOR A DISTANCE OF 299.65 FEET; THENCE RUN NORTH 00°44'39" WEST, FOR A DISTANCE OF 130.00 FEET; THENCE RUN NORTH 88°57'20" EAST, FOR A DISTANCE OF 299.68 FEET TO A POINT ON THE WEST LINE OF A 60 FOOT WIDE EASEMENT, BEING BOONDOCK ROAD; THENCE RUN NORTH 00°44'45" WEST, ALONG SAID WEST LINE, FOR A DISTANCE OF 248.23 FEET TO A POINT ON THE NORTH LINE OF THE SOUTHEAST QUARTER OF THE SOUTHWEST QUARTER OF SAID SECTION 31; THENCE RUN NORTH 88°57'01" EAST, ALONG SAID NORTH LINE, FOR A DISTANCE OF 30.00 FEET; THENCE RUN SOUTH 00°44'45" EAST, FOR A DISTANCE OF 248.38 FEET; THENCE RUN NORTH 88°57'15" EAST, FOR A DISTANCE OF 329.31 FEET TO A POINT ON THE EAST LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 31; THENCE RUN SOUTH 00°44'34" EAST, ALONG SAID EAST LINE, FOR A DISTANCE OF 1,038.09 FEET TO THE POINT OF BEGINNING OF THE PARCEL OF LAND HEREIN DESCRIBED, CONTAINING 19.97 ACRES, MORE OR LESS.

SCHEDULE OF USES

ADMINISTRATIVE OFFICES
 ANIMAL - CLINIC
 ATM (AUTOMATIC TELLER MACHINE)
 AUTO PARTS STORE
 AUTO REPAIR AND SERVICE - GROUP I
 BAIT AND TACKLE SHOP
 BANKS AND FINANCIAL ESTABLISHMENTS - GROUPS I, II
 (LIMITED TO ONE WITHIN THE CPD)
 BOAT PARTS STORES
 BUSINESS SERVICES - GROUPS I, II
 CAR WASH
 CLEANING AND MAINTENANCE SERVICES
 CLOTHING STORES - GENERAL
 CLOTHING STORES - SPECIALTY
 CONVENIENCE AND BUILDERS
 CONVENIENCE FOOD AND BEVERAGE STORES
 CULTURAL FACILITIES
 DAY CARE CENTER - ADULT OR CHILD
 DEPARTMENT STORE
 DRIVE-THROUGH FACILITY FOR ANY PERMITTED USE
 ESSENTIAL SERVICES - GROUP I
 ESSENTIAL SERVICES FACILITIES - GROUP I
 EXCAVATION - WATER RETENTION
 FACTORY OUTLETS (POINT OF MANUFACTURE ONLY)
 FENCES, WALLS
 FOOD STORES - GROUP I
 GIFT AND SOUVENIR SHOP
 HARDWARE STORE
 HEALTH CARE FACILITIES - GROUP III
 HOBBY, TOY AND GAME SHOPS
 HOTEL (LIMITED TO TRACT "A" ONLY, 124 ROOMS MAX.)
 HOUSEHOLD/OFFICE FURNISHINGS-ALL GROUPS
 INSURANCE COMPANIES
 LAUNDRY OR DRY-CLEANING-GROUP I
 LAWN AND GARDEN SUPPLY STORES

LIBRARY
 MEDICAL OFFICE
 PACKAGE STORE
 PAINT, GLASS AND WALLPAPER
 PARKING LOT COMMERCIAL
 PERSONAL SERVICES-GROUPS I, II, III, IV
 PET SERVICES
 PET SHOP
 PHARMACY
 PLACE OF WORSHIP
 POLICE OR SHERIFF'S STATION
 POST OFFICE
 PRINTING AND PUBLISHING
 REAL ESTATE SALES OFFICE
 RECREATION, COMMERCIAL, ALL GROUPS
 RELIGIOUS FACILITIES
 RENTAL OR LEASING ESTABLISHMENT-GROUPS II, III
 REPAIR SHOPS-GROUPS I, II, III, IV, AND V
 RESEARCH AND DEVELOPMENT LABORATORIES-GROUPS II, IV
 RESTAURANT, FAST FOOD (LIMITED TO TWO WITHIN THE CPD)
 RESTAURANTS-GROUPS I, II, III, IV
 RETAIL AND WHOLESALE SALES, WHEN CLEARLY INCIDENTAL
 AND SUBORDINATE TO A PERMITTED PRINCIPAL USE ON
 THE SAME PREMISES
 SCHOOLS-COMMERCIAL
 SELF-SERVICE FUEL PUMPS (LIMITED TO ONE WITHIN THE CPD
 WITH UP TO 16 FUEL PUMPS)
 SIGNS IN ACCORDANCE WITH CHAPTER 30
 SPECIALTY RETAIL SHOPS-GROUPS I, II, III, IV
 STORAGE, INDOOR MINI-STORAGE (MAXIMUM OF 150,000 S.F.,
 LIMITED TO TRACT "A" ONLY)
 STUDIOS
 TEMPORARY USES
 THEATER, INDOOR ONLY
 USED MERCHANDISE STORES-GROUPS I, II, III AND IV
 VARIETY STORE
 VEHICLE AND EQUIPMENT DEALERS-GROUP I (LIMITED TO A
 MAXIMUM OF 3.5 ACRES.

LIBRARY
 MEDICAL OFFICE
 PACKAGE STORE
 PAINT, GLASS AND WALLPAPER
 PARKING LOT COMMERCIAL
 PERSONAL SERVICES-GROUPS I, II, III, IV
 PET SERVICES
 PET SHOP
 PHARMACY
 PLACE OF WORSHIP
 POLICE OR SHERIFF'S STATION
 POST OFFICE
 PRINTING AND PUBLISHING
 REAL ESTATE SALES OFFICE
 RECREATION, COMMERCIAL, ALL GROUPS
 RELIGIOUS FACILITIES
 RENTAL OR LEASING ESTABLISHMENT-GROUPS II, III
 REPAIR SHOPS-GROUPS I, II, III, IV, AND V
 RESEARCH AND DEVELOPMENT LABORATORIES-GROUPS II, IV
 RESTAURANT, FAST FOOD (LIMITED TO TWO WITHIN THE CPD)
 RESTAURANTS-GROUPS I, II, III, IV
 RETAIL AND WHOLESALE SALES, WHEN CLEARLY INCIDENTAL
 AND SUBORDINATE TO A PERMITTED PRINCIPAL USE ON
 THE SAME PREMISES
 SCHOOLS-COMMERCIAL
 SELF-SERVICE FUEL PUMPS (LIMITED TO ONE WITHIN THE CPD
 WITH UP TO 16 FUEL PUMPS)
 SIGNS IN ACCORDANCE WITH CHAPTER 30
 SPECIALTY RETAIL SHOPS-GROUPS I, II, III, IV
 STORAGE, INDOOR MINI-STORAGE (MAXIMUM OF 150,000 S.F.,
 LIMITED TO TRACT "A" ONLY)
 STUDIOS
 TEMPORARY USES
 THEATER, INDOOR ONLY
 USED MERCHANDISE STORES-GROUPS I, II, III AND IV
 VARIETY STORE
 VEHICLE AND EQUIPMENT DEALERS-GROUP I (LIMITED TO A
 MAXIMUM OF 3.5 ACRES.

LEGAL DESCRIPTION
 (METES AND BOUNDS)

A PARCEL OF LAND LOCATED IN THE SOUTHWEST QUARTER OF SECTION 31, TOWNSHIP 47 SOUTH, RANGE 28 EAST, LEE COUNTY, FLORIDA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:
 BEGIN AT THE SOUTHEAST CORNER OF THE SOUTHWEST QUARTER OF SECTION 31, TOWNSHIP 47 SOUTH, RANGE 28 EAST, LEE COUNTY, FLORIDA; THENCE RUN SOUTH 89°57'16" WEST, ALONG THE SOUTH LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 31, FOR A DISTANCE OF 68.51 FEET TO A POINT ON THE EASTERLY RIGHT-OF-WAY LINE OF INTERSTATE 75 (STATE ROAD 83), AS THE SAME IS SHOWN ON THE STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION RIGHT-OF-WAY MAP FOR STATE ROAD No. 83 (17.9) LEE COUNTY, FLORIDA, SECTION 12075-2401, SHEET 5 OF 8, DATED 12/17/78; THENCE RUN NORTH 01°33'33" WEST, ALONG SAID RIGHT-OF-WAY LINE, FOR A DISTANCE OF 84.73 FEET TO A POINT ON THE WESTERLY BOUNDARY OF PARCEL FOUR AS RECORDED IN OFFICIAL RECORDS BOOK 4548 AT PAGES 4351 AND 4352 OF THE PUBLIC RECORDS OF LEE COUNTY, FLORIDA; THENCE RUN NORTH 89°57'16" WEST, ALONG THE NORTHERLY BOUNDARY OF SAID PARCEL OF LAND, FOR A DISTANCE OF 217.22 FEET TO A POINT ON THE WESTERLY BOUNDARY OF THAT PARCEL OF LAND DESCRIBED PARAGRAPH 3 OF THAT PARCEL FOUR AS RECORDED IN OFFICIAL RECORDS BOOK 4548 AT PAGES 4351 AND 4352 OF THE PUBLIC RECORDS OF LEE COUNTY, FLORIDA; THENCE RUN NORTH 00°47'24" EAST, FOR A DISTANCE OF 383.80 FEET TO A POINT ON THE WESTERLY BOUNDARY OF SAID PARCEL OF LAND, FOR A DISTANCE OF 30.00 FEET; THENCE RUN SOUTH 00°47'24" EAST, FOR A DISTANCE OF 383.80 FEET; THENCE RUN NORTH 89°57'20" EAST, FOR A DISTANCE OF 299.65 FEET; THENCE RUN NORTH 88°52'20" EAST, FOR A DISTANCE OF 130.00 FEET; THENCE RUN NORTH 88°52'20" EAST, FOR A DISTANCE OF 299.65 FEET; THENCE RUN NORTH 00°44'45" WEST, FOR A DISTANCE OF 255.87 FEET; THENCE RUN NORTH 88°45'30" EAST, ALONG THE NORTH LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 31, FOR A DISTANCE OF 30.00 FEET; THENCE RUN SOUTH 00°44'45" EAST, FOR A DISTANCE OF 255.12 FEET; THENCE RUN NORTH 89°57'15" EAST, FOR A DISTANCE OF 329.11 FEET TO A POINT ON THE EAST LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 31; THENCE RUN SOUTH 00°45'13" EAST, ALONG THE EAST LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 31, FOR A DISTANCE OF 1,065.00 FEET TO THE POINT OF BEGINNING, CONTAINING 20.986 ACRES, MORE OR LESS.

PROPERTY DEVELOPMENT REGULATIONS

MINIMUM LOT AREA: 10,000 SQUARE FEET
 MINIMUM LOT WIDTH: 100 FEET
 MINIMUM LOT DEPTH: 100 FEET
 17.5' STREET YARD SETBACK:
 20 FEET
 15 FEET
 REAR YARD SETBACK:
 25 FEET
 WATER BODY SETBACK:
 25 FEET (EXCEPT AT BULKHEAD)
 25 FEET
 2.75± ACRES
 6.50± ACRES
 1.34± ACRES
 4.20± ACRES (20%)
 5.97± ACRES (2.1± ACRES INDIGENOUS)
 0.90± ACRES (3.60± ACRES X 25%)
 2.43± ACRES (1.54 ACRES + 0.89 ACRES + 0.07± ACRES)
 0.11± ACRES (2:1 INDIGENOUS CREDIT)
 1.40± ACRES
 1.13± ACRES
 ENHANCED LANDSCAPE AREA
 LAKE/PRESERVE/TRACT "A" BUFFERS:
 ALL OUTPARCELS & TRACT "A":
 (MINIMUM 10%)
 1.40± ACRES
 1.13± ACRES

MAXIMUM BUILDING HEIGHT:
 HOTEL / MOTEL: *
 COMMERCIAL: *
 (* LIMITED TO TRACT "A")
 55 FEET (5 STORIES)
 45 FEET (3 STORIES)
 240,000 SQUARE FEET

APPROVED
 Mayor
 City of Bonita Springs
 Subject to conditions in Ordinance No. 2017-01
 Date: 12/22/2017
 CITY OF BONITA SPRINGS

UTILITIES SERVING THIS SITE

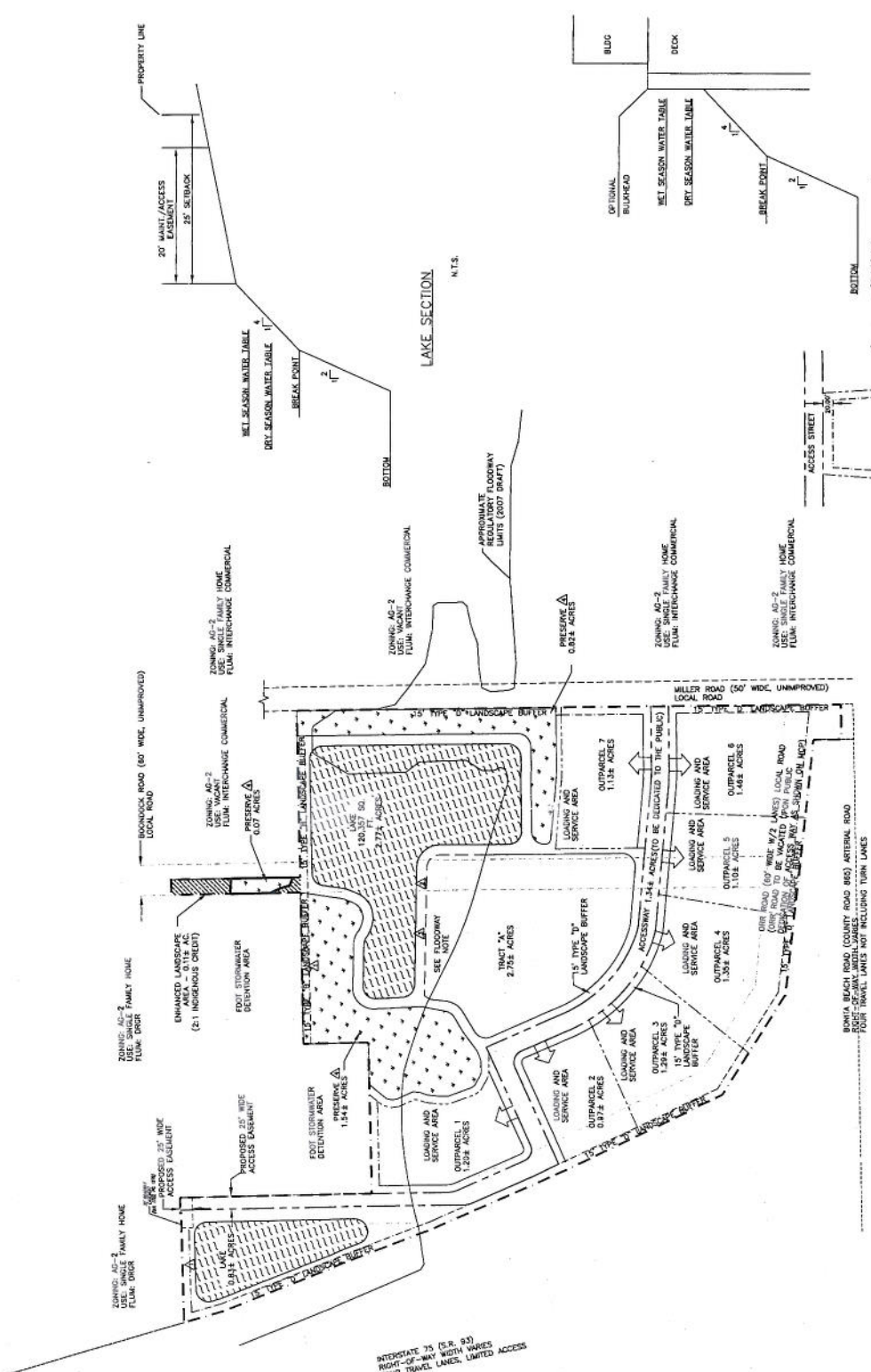
WATER: BONITA SPRINGS UTILITIES, INC.
 P.O. BOX 2388
 BONITA SPRINGS, FLORIDA 34133
 SEWER: BONITA SPRINGS UTILITIES, INC.
 P.O. BOX 2388
 BONITA SPRINGS, FLORIDA 34133
 ELECTRIC: FLORIDA POWER AND LIGHT COMPANY
 4105 15TH AVENUE, S.W.
 NAPLES, FLORIDA 34116
 TELEPHONE: SPRINT / UNITED TELEPHONE OF FLORIDA
 P.O. BOX 2477
 NAPLES, FLORIDA 34106
 REFUSE COLLECTION: ONYX WASTE SERVICES OF FLORIDA, INC.
 1701 PINE RIDGE ROAD
 FORTY MYERS BEACH, FLORIDA 33991
 FIRE AND RESCUE: BONITA SPRINGS FIRE CONTROL AND RESCUE DISTRICT
 P.O. BOX 1989
 BONITA SPRINGS, FLORIDA 34133

DCI2004-00061 ZONING SUBMITTAL
 BONITA 75 CPD
 EXHIBIT IV-E
 MASTER CONCEPT PLAN
 GENERAL NOTES

Q. GRADY MINOR & ASSOCIATES, P.A.
 CIVIL ENGINEERS & LAND SURVEYORS • PLANNERS • LANDSCAPE ARCHITECTS
 1000 W. UNIVERSITY BLVD., SUITE 100
 TAMPA, FLORIDA 33606
 WWW.QGRADYMINOR.COM

Revision	Date	Description
1	5/17/08	REVISE W/P
2	9/4/08	UPDATE PER BOUNDARY CHANGE
3	8/4/08	REVISE PROJECT BOUNDARY
4	5/17/08	REVISE PER BOUNDARY CHANGE
5	5/17/08	REVISE PER BOUNDARY CHANGE
6	5/20/08	REVISE PER BOUNDARY CHANGE
7	5/20/08	REVISE PER BOUNDARY CHANGE
8	5/20/08	REVISE PER BOUNDARY CHANGE
9	5/20/08	REVISE PER BOUNDARY CHANGE
10	5/20/08	REVISE PER BOUNDARY CHANGE

LEGEND
 INTERCHANGE COMMERCIAL
 AG-2, AGRICULTURE, CPD, COMMERCIAL
 PLANNED DEVELOPMENT
 VACANT, RESIDENTIAL, NURSERY
 CPD, COMMERCIAL PLANNED DEVELOPMENT
 20.986 ± ACRES



FLOODWAY NOTE:
 THE APPROXIMATE REGULATORY FLOODWAY LIMITS SHOWN HEREON ARE BASED ON DRAFT MAPS AND GIS INFORMATION AVAILABLE FROM LEE COUNTY. THE FINAL LOCATION OF THE FLOODWAY WILL BE DETERMINED FOLLOWING ISSUANCE OF THE FINAL REGULATORY MAPS.
 NO ABOVE GRADE IMPROVEMENTS ARE ALLOWED WITHIN THE FLOODWAY UNLESS APPROVED BY THE LEE COUNTY ENGINEER. UNLESS OTHERWISE NOTED, ALL IMPROVEMENTS MUST BE DESIGNED AND CONSTRUCTED TO MAINTAIN OR EXCEED ORIGINAL FLOOD ELEVATIONS AND ARE NOT TO INCREASE FLOOD LEVELS.

LEGEND

- PARKING LOT ACCESS
- INDIGENOUS PRESERVE
- ENHANCED LANDSCAPE AREA

REVISIONS

Revision	Date	Description
1	3/10/06	REVISE MOP
2	8/4/08	UPDATE PER BOUNDARY CHANGE
3	1/7/09	ADD ADJACENT TO OUTPARCELS
4	3/7/09	APPROVED
5	5/2/09	REVISE PRESERVE BOUNDARY AND INTERNAL ROW
6	5/2/09	REVISE PRESERVE BOUNDARY AND INTERNAL ROW

SCALE IN FEET

0' 20' 40' 80'

APPROVED
 Master Concept Plan
 City Plan # 20-01-12 Page 3 of 3
 Subject to conditions in Ordinance 20-01-12
 City of Bonita Springs
 MAY 13 2009
 COMMUNITY DEVELOPMENT

RECEIVED
 MAY 13 2009
 COMMUNITY DEVELOPMENT

BONVITA 75 CPD
 EXHIBIT III-A-1
 EXHIBIT IV-A-1
 MASTER CONCEPT PLAN

Q. GRADY MINOR & ASSOCIATES, P.A.
 CIVIL ENGINEERS + LAND SURVEYORS + PLANNERS + LANDSCAPE ARCHITECTS
 BONITA SPRINGS
 10000 W. STATE ROAD 90, SUITE 100
 BONITA SPRINGS, FL 34135
 PHONE: 813.437.1100
 FAX: 813.437.1101
 WWW.GRADYMINOR.COM

CASE DC12004-00061

SHEET 3 OF 5



CASE DCI2004-00061

BONITA 75 CPD

EXHIBIT IV-N
AERIAL

Q. GRADY MINOR & ASSOCIATES, P.A.
CIVIL ENGINEERS • LAND SURVEYORS • PLANNERS • LANDSCAPE ARCHITECTS
1001 W. 12th Street, Suite 100, Bonita, CA 92001
TEL: 619.434.1234 FAX: 619.434.1235
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Revision	Date	Description
6	5/08	REVISE PRESERVE BOUNDARY AND INTERNAL ROW
5	5/07	REVISE SITE PER FLOODWAY
4	4/08	REVISE PRO. LOT BOUNDARY
3	8/4/08	REVISE PRO. LOT BOUNDARY
2	8/4/08	UPDATE PER BOUNDARY CHANGE
1	3/19/06	REVISE MCP

APPROVED
Master Concept Plan
By: [Signature] Date: 3/19/06 Page: 1 of 1
Subject to conditions in Ordinance 2004-011
Case # 2004-011-00061
CITY OF BONITA SERVICES
DATE AERIAL FLOPPY: MARCH 2005

LEGEND

SCALE IN FEET
0 50 100 200

66

ADMINISTRATIVE AMENDMENT (PD) ADD09-BOS00006

CITY OF BONITA SPRINGS ADMINISTRATIVE AMENDMENT
ZONING ORDINANCE NO. 08-10

BONITA SPRINGS, FLORIDA

RECEIVED
MAY 27 2009
Q. Grady Minor
& Associates, P.A.

WHEREAS, Bonita 75 Partners, LLC, c/o Wayne Arnold, filed an application for an administrative amendment to a Commercial Planned Development (CPD) on a project known as Bonita 75 Commercial Planned Development (CPD) to:

1. Amend the Master Concept Plan to revise the right of way width for a portion of the proposed alignment of an internal right of way;
2. Amend the Master Concept Plan to add and modify notations for Orr Road and Boondock Road;
3. Amend the Master Concept Plan to correct the Future Land Use Map designation for properties to the north;
4. Amend the Master Concept Plan to make revisions to Tract A's conceptual access for consistency with other conceptual access points;

described more particularly as:

LEGAL DESCRIPTION: In Section 31, Township 47 South, Range 26, Lee County, Florida.

See legal description attached hereto as "Exhibit A"

WHEREAS, the property was originally rezoned in Zoning Ordinance 08-10 (with subsequent amendments in case numbers Z-96-13 and Z-68-86); and

WHEREAS, the Bonita Springs Land Development Code provides for certain administrative changes to planned development master concept plans and planned unit development final development plans; and

WHEREAS, the proposed administrative amendment is to revise the Bonita 75 CPD Master Concept Plan ("Exhibit B") as stamped received on April 21, 2009; and

WHEREAS, the Bonita 75 CPD Right of Way Vacation (Case #VAC08-BOS00003) depicted an incorrect road width of 25 feet for the northernmost 520 +/- feet of the Orr Road right of way on the Bonita 75 CPD Master Concept Plan approved July 28, 2008 (Zoning Ordinance 08-10) and submitted a revised exhibit for the Bonita 75

CPD Right of Way Vacation to correct that portion of the Orr Road right of way width to sixty (60) feet ("Exhibit C"); and

WHEREAS, the original Bonita 75 CPD Master Concept Plan depicted an incorrect road width of 25 feet for the northernmost 520 +/- feet of the Orr Road right of way and the revised Bonita 75 CPD Master Concept Plan will correct the Orr Road right of way width to sixty (60) feet; and

WHEREAS, the revised Bonita 75 CPD Master Concept Plan, approved under Zoning Ordinance 08-10, has been revised to provide a notation for Boondock Road and label "Boondock Road (To Be Vacated)"; and

WHEREAS, the revised Bonita 75 CPD Master Concept Plan has been revised to amend the notation for Orr Road to add "Upon Public Dedication of Access Way as Shown on MCP"; and

WHEREAS, the original Bonita 75 CPD Master Concept Plan incorrectly depicted the Future Land Use Designation of the property abutting the northern property boundary as Density Reduction Groundwater Recharge; and

WHEREAS, the revised Bonita 75 CPD Master Concept Plan has been revised to depict the Future Land Use Designation of the property abutting the northern property boundary as Interchange Commercial; and

WHEREAS, the revised Bonita 75 CPD Master Concept Plan has been revised to depict an approximate location of a forty foot (40') wide drainage easement adjacent to I-75, which provides an outfall location for the Florida Department of Transportation; and

WHEREAS, the revised Bonita 75 CPD Master Concept Plan has been revised to show conceptual access arrows to Tract A to be consistent with other conceptual Tract access points; and

WHEREAS, the subject application and plans have been reviewed by the City of Bonita Springs Community Development Department in accordance with the applicable regulations for compliance with all terms of the administrative approval procedures; and

WHEREAS, it is found that the proposed amendment does not increase density or intensity within the development; does not decrease buffers or open space required by the LDC; does not underutilize public resources or infrastructure; does not reduce total open space, buffering, landscaping to preservation areas; and does not otherwise adversely impact surrounding land uses.

NOW, THEREFORE, IT IS HEREBY DETERMINED that the application for administrative approval for an amendment to the Bonita 75 CPD Master Concept Plan is **APPROVED, subject to the following conditions:**

1. The Development must be in compliance with the amended Master Concept Plan, stamped received on APRIL 21, 2009 ("Exhibit B"). Master Concept Plan for ADD09-BOS0006 is hereby APPROVED and adopted. A reduced copy is attached hereto.
2. Prior to issuance of the first development order, the applicant must obtain the vacation of Orr Road and Boondock Road, with the provision of the relocation of the accessway to be approved by the City of Bonita Springs Community Development Department, generally consistent with the approved Master Concept Plan, stamped received on APRIL 21, 2009.
3. Any change of use of right of way alignment that requires a different site plan will render this administrative approval null and void.
4. The terms and conditions of the original zoning ordinance will remain in full force and effect.

DULY signed this 15 day of May, A.D., 2009

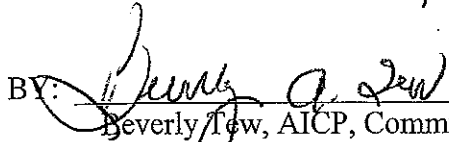
BY: 
Beverly Tew, AICP, Community Development Director
Department of Community Development

Exhibit A

BONITA 75 CPD

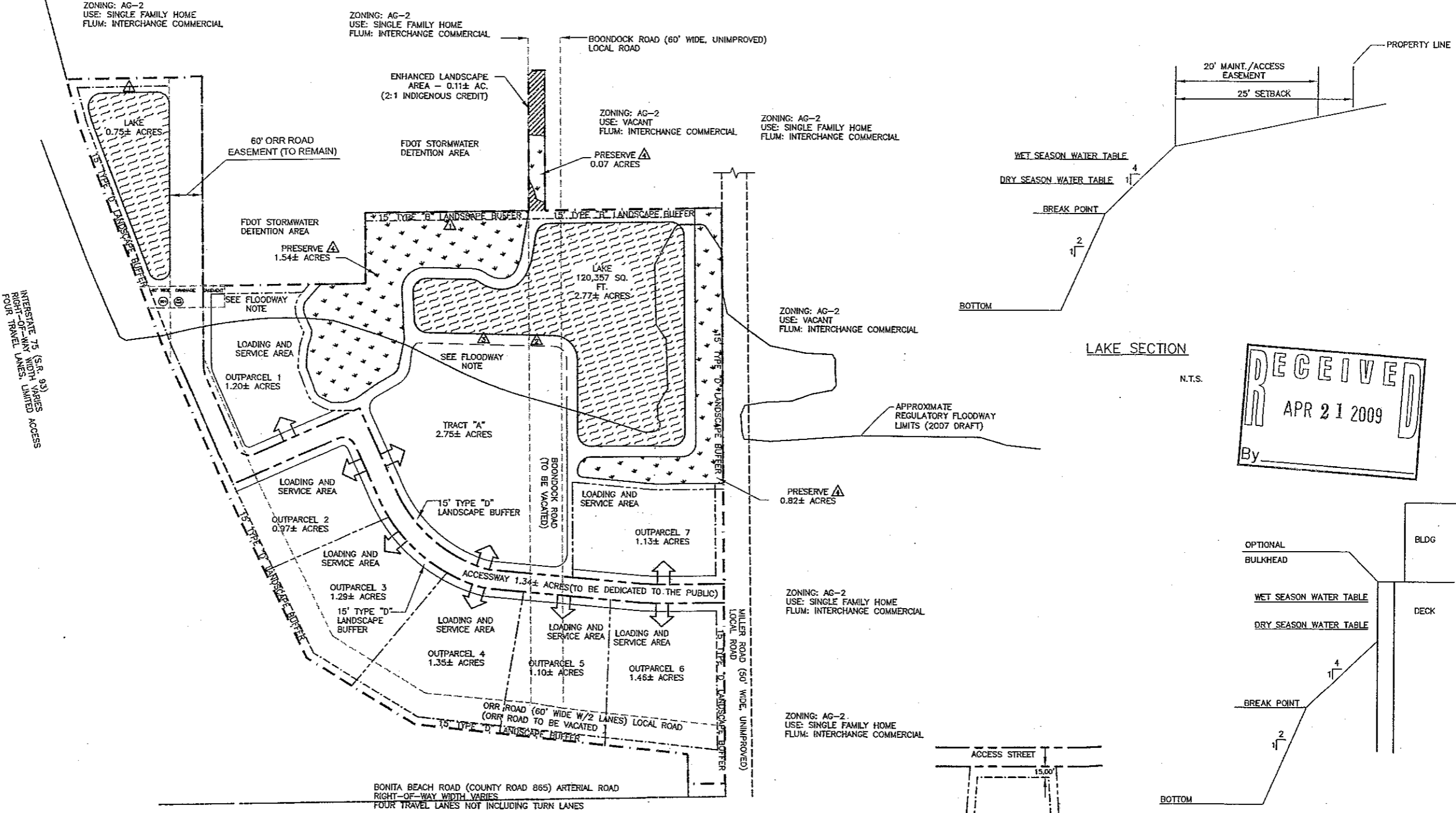
Exhibit II-4 Legal Description

A PARCEL OF LAND LOCATED IN THE SOUTHEAST QUARTER OF THE SOUTHWEST QUARTER OF SECTION 31, TOWNSHIP 47 SOUTH, RANGE 26 EAST, LEE COUNTY, FLORIDA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

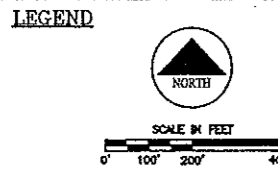
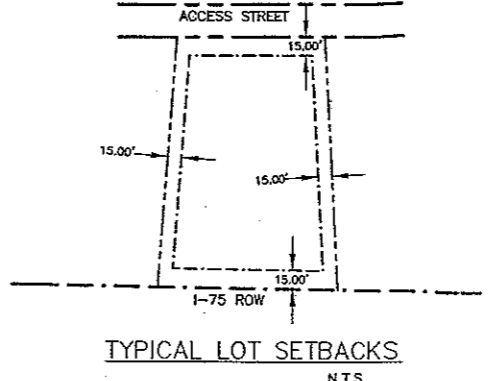
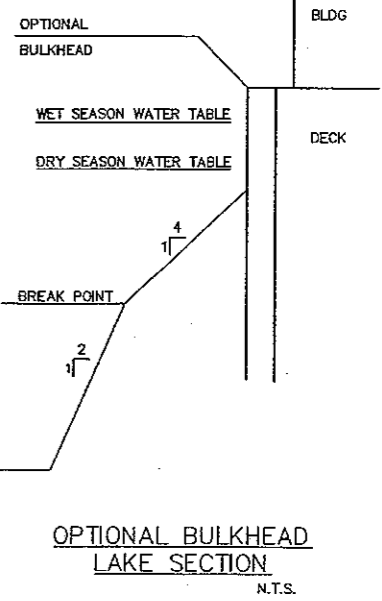
COMMENCE AT THE SOUTHEAST CORNER OF THE SOUTHWEST QUARTER OF SECTION 31, TOWNSHIP 47 SOUTH, RANGE 26 EAST, COLLIER COUNTY, FLORIDA; THENCE RUN NORTH 00°44'34" WEST; ALONG THE EAST LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 31, FOR A DISTANCE OF 26.91 FEET TO A POINT ON THE NORTH RIGHT-OF-WAY OF BONITA BEACH (F/K/A CARRELL ROAD), THE SAME SHOWN ON THE STATE OF FLORIDA DEPARTEMENT OF TRANSPORTATION RIGHT-OF-WAY MAP, STATE ROAD No. 93 (I-75), LEE COUNTY, FLORIDA, SECTION 12075-2401, SHEET 5 OF 9, ALSO BEING THE POINT OF BEGINNING OF THE PARCEL OF LAND HEREIN DESCRIBED; THENCE RUN SOUTH 88°57'15" WEST, ALONG SAID NORTH RIGHT-OF-WAY, FOR A DISTANCE OF 34.33 FEET; THENCE RUN NORTH 01°02'45" WEST, FOR A DISTANCE OF 53.78 FEET; THENCE RUN NORTH 83°56'32" WEST, FOR A DISTANCE OF 532.60 FEET; THENCE RUN NORTH 58°29'27" WEST, FOR A DISTANCE OF 217.47 FEET; THENCE RUN NORTH 25°56'09" WEST, FOR A DISTANCE OF 339.32 FEET; THENCE RUN NORTH 23°58'20" WEST, FOR A DISTANCE OF 245.12 FEET; THENCE RUN NORTH 00°47'44" WEST, FOR A DISTANCE OF 7.87 FEET TO A POINT ON THE SOUTH LINE OF THE LANDS DESCRIBED IN OFFICIAL RECORDS BOOK 4549 AT PAGE 4351 OF THE PUBLIC RECORDS OF LEE COUNTY, FLORIDA; THENCE RUN SOUTH 88°57'16" WEST, ALONG SAID SOUTH LINE, FOR A DISTANCE OF 35.94 FEET TO A POINT ON THE NORTHEASTERLY RIGHT-OF-WAY OF STATE ROAD No. 93 (I-75); THENCE RUN ALONG SAID NORTHEASTERLY RIGHT-OF-WAY FOR THE FOLLOWING THREE (3) COURSES:

- 1) THENCE RUN NORTH 23°58'20" WEST, FOR A DISTANCE OF 12.89 FEET;
- 2) THENCE RUN NORTH 21°07'25" WEST, FOR A DISTANCE OF 446.23 FEET;
- 3) THENCE RUN NORTH 15°25'36" WEST, FOR A DISTANCE OF 79.83 FEET TO A POINT ON THE NORTH LINE OF THE SOUTHEAST QUARTER OF THE SOUTHWEST QUARTER OF SAID SECTION 31; THENCE RUN NORTH 88°57'01" EAST, ALONG SAID NORTH LINE, FOR A DISTANCE OF 246.19 FEET TO A POINT ON THE EAST LINE OF A 60 FOOT WIDE RIGHT-OF-WAY, BEING ORR ROAD; THENCE RUN SOUTH 00°47'44" EAST, ALONG SAID EAST RIGHT-OF-WAY, FOR A DISTANCE OF 378.17 FEET; THENCE RUN NORTH 88°57'20" EAST, FOR A DISTANCE OF 299.65 FEET; THENCE RUN NORTH 00°44'39" WEST, FOR A DISTANCE OF 130.00 FEET; THENCE RUN NORTH 88°57'20" EAST, FOR A DISTANCE OF 299.68 FEET TO A POINT ON THE WEST LINE OF A 60 FOOT WIDE EASEMENT, BEING BOONDOCK ROAD; THENCE RUN NORTH 00°44'45" WEST, ALONG SAID WEST LINE, FOR A DISTANCE OF 248.23 FEET TO A POINT ON THE NORTH LINE OF THE SOUTHEAST QUARTER OF THE SOUTHWEST QUARTER OF SAID SECTION 31; THENCE RUN NORTH 88°57'01" EAST, ALONG SAID NORTH LINE, FOR A DISTANCE OF 30.00 FEET; THENCE RUN SOUTH 00°44'45" EAST, FOR A DISTANCE OF 248.38 FEET; THENCE RUN NORTH 88°57'15" EAST, FOR A DISTANCE OF 329.31 FEET TO A POINT ON THE EAST LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 31; THENCE RUN SOUTH 00°44'34" EAST, ALONG SAID EAST LINE, FOR A DISTANCE OF 1,038.09 FEET TO THE POINT OF BEGINNING OF THE PARCEL OF LAND HEREIN DESCRIBED, CONTAINING 19.97 ACRES, MORE OR LESS.

Exhibit B



RECEIVED
 APR 21 2009
 By _____



Revision	Date	Description

DESIGNED BY: GGMA
DRAWN BY: SU
APPROVED: DWA
JOB CODE: B75AA
SCALE: 24x36 1" = 100'
 11x17 1" = 200'

GradyMinor
 Civil Engineers • Land Surveyors • Planners • Landscape Architects
 CERT. OF AUTH. BB 0005151 CERT. OF AUTH. LB 0005151 BUSINESS LC 26000206
 www.GradyMinor.com O. Grady Minor and Associates, P.A.

Bonita Springs 239.947.1144
 Fort Myers 239.690.4380
 North Port 941.426.5858
 Naples 239.444.2397

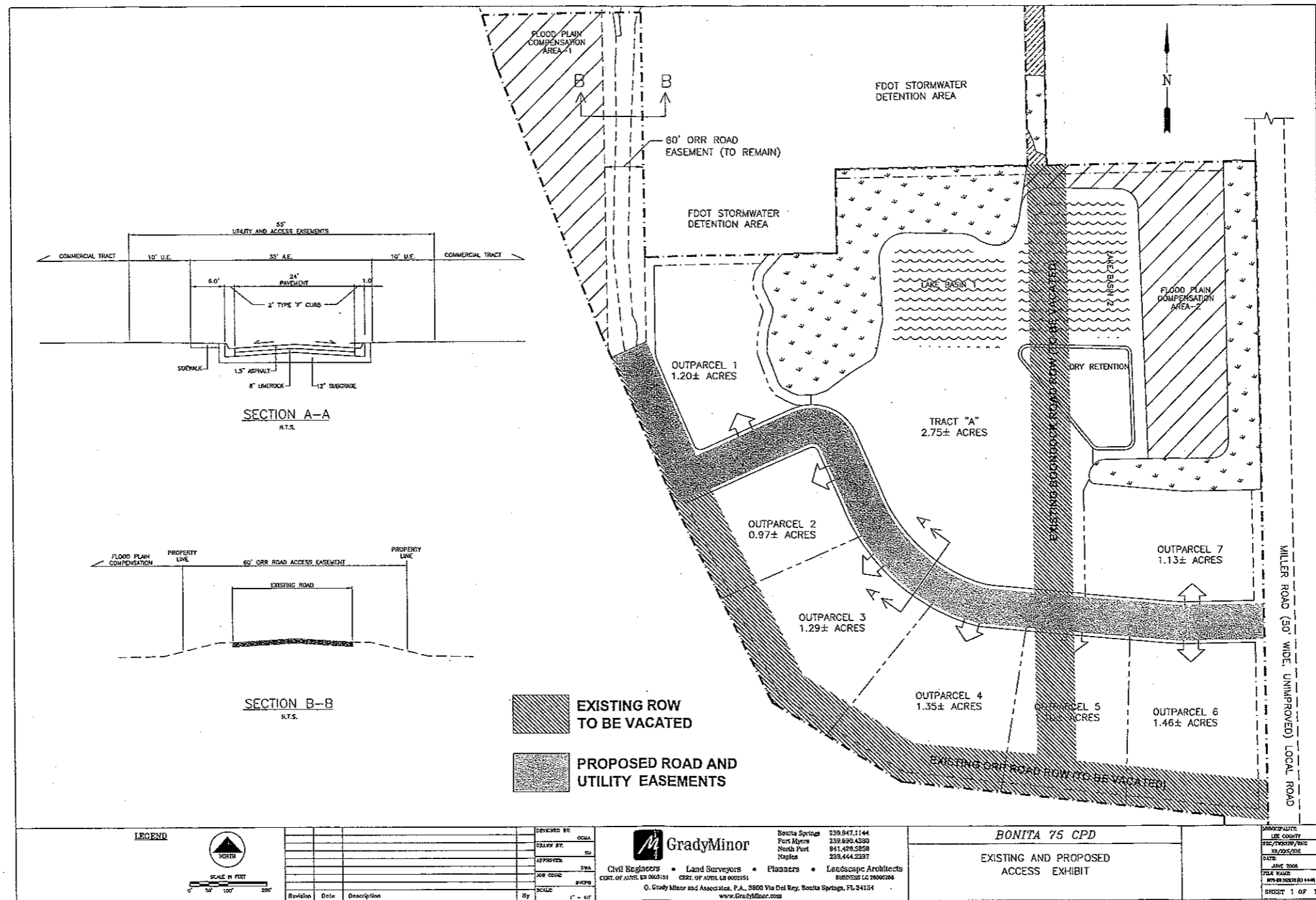
BONITA 75 CPD
MASTER CONCEPT PLAN

ADMINISTRATIVE AMENDMENT

MUNICIPALITY:	CITY OF BONITA SPRINGS
SEC/TWR/BGE:	31/475/26E
DATE:	MARCH 2009
FILE NAME:	B75AA-MCP
SHEET 1 OF 1	

6:\PROJ\11-PLANNING\DWG\B75AA\B75AA-MCP.DWG 3/19/2009 4:39 PM

RECEIVED
 APR 10 2009
 BY *[Signature]*



CITY OF BONITA SPRINGS
ZONING ORDINANCE NO. 08-10

A ZONING ORDINANCE OF THE CITY OF BONITA SPRINGS, FLORIDA; APPROVING A REQUEST TO REZONE BONITA 75 PROJECT FROM COMMERCIAL PLANNED DEVELOPMENT (CPD) AND AGRICULTURAL (AG-2) TO COMMERCIAL PLANNED DEVELOPMENT (CPD); ON LAND LOCATED AT 27952, 27925 AND 27977 BOONDOCK ROAD, BONITA SPRINGS, FLORIDA, ON 20.996 +/- ACRES; PROVIDING FOR AN EFFECTIVE DATE.

WHEREAS, Bonita 75 Partners, LLC. in reference to Bonita 75 CPD has filed an application to rezone the Bonita 75 project from commercial planned development (CPD) and agricultural (AG-2) to commercial planned development (CPD);

WHEREAS, the subject property is located at 27952, 27925 and 27977 Boondock Road, Bonita Springs, Florida, and is described more particularly as:

"See Exhibit A"

WHEREAS, a Public Hearing was advertised and heard on June 6, 2008 by the City of Bonita Springs Board for Land Use Hearings and Adjustments and Zoning Board of Appeals ("Zoning Board") on Case DCI 2004-00061, who gave full consideration to the evidence available and recommended to uphold the staff recommendation of denial due solely to transportation (4-0, with Pescosolido, Norris and Mathes absent); and gave full and complete consideration of the record, consisting of the Staff Recommendation, the recommendation of the Zoning Board, the documents on file with the City and the testimony of all interested parties. The May 21, 2008 staff report prepared by Lee County Development Services Division and evidence submitted at the Zoning Board hearing is on file with the City Clerk.

NOW, THEREFORE, BE IT ORDAINED by the City Council of the City of Bonita Springs, Lee County, Florida:

SECTION ONE: APPROVAL OF REQUEST

City Council of Bonita Springs hereby approves the rezoning to commercial planned development with the following conditions and deviations:

Conditions

1. The development of this project must be consistent with the four-page Master Concept Plan entitled "BONITA 75 CPD" stamped received May 23, 2008 except as modified by the conditions below. This development must comply with all requirements of the Bonita Springs

Land Development Code at time of local Development Order Approval, except as may be granted by deviation as part of this planned development. If changes to the Master Concept Plan are subsequently pursued, appropriate approvals will be necessary.

Development is limited to a maximum of 158,800 square feet of commercial floor area, of which a maximum of 88,000 square feet may be commercial retail (70,000 square feet may be commercial office); plus a maximum of 58,800 square feet of mini-warehouse, plus a convenience store with a maximum of 12 self-service fuel pumps (as defined by the Bonita Springs LDC); and a maximum 5,000 square foot bank with a drive-through. Development of a hotel (maximum 124 units) on Tract A is subject to Condition 10.

The developer must provide a cumulative land development summary table as part of any local development order applications.

2. The following limits apply to the project and uses:

a. Schedule of Uses

Accessory uses and structures *

Animal Clinic

Administrative offices

ATM (automatic teller machine)

Auto parts store

Auto Repair and Service Shop, Group I

Banks and financial establishments, Group I and II

Boats parts store

Business services: Group I, II

Car Wash, limited to 1 for the entire CPD

Cleaning and maintenance services, no outdoor storage of fertilizers or cleaning materials

Clothing stores, general

Consumption on Premises, indoor only, in conjunction with a full-service restaurant or hotel/motel. Outdoor consumption requires a special exception.

Contractors and builders, Group I and II

Convenience food and beverage stores, limited to 1 - maximum 12 self service fuel pumps may be included.

Cultural facilities

Daycare centers, child, adult

Department Store

Drive-through facility for any permitted use, except package stores

Drug store, pharmacy

Essential services

Essential service facilities: Group I

Excavation: Water retention
 Factory outlets, (point of manufacture only)
 Fences, walls
 Food and beverage services, limited
 Food stores, Group I
 Gift and Souvenir shop
 Hardware store - no outdoor storage or display
 Healthcare facilities: Group III
 Hobby, toy and game shop
 Hotel/Motel - See Condition 10.
 Household and office furnishing, all groups
 Insurance companies
 Laundry or dry cleaning: Group I
 Lawn and Garden Supply Stores, no outdoor storage or display.
 Library
 Medical office
 Nonstore retailers: Group I and II
 Package store - maximum 1 stand-alone package store
 Paint, glass and wallpaper - no outdoor storage or display
 Parking lot: Accessory, Temporary, Commercial
 Personal Services, Group I, II, III, IV. Excluding Unlicensed
 Massage Parlors, Steam or Turkish Baths, Escort Services,
 Palm Readers, Fortune Tellers or Card Readers, and Tattoo
 Parlors.
 Pet service
 Pet shop
 Places of worship
 Police or Sheriff's station
 Post office
 Printing and Publishing
 Real estate sales office
 Recreational facilities, Group I, II, and IV.
 Religious facilities
 Rental or leasing establishment: Group II and III
 Repair shops: Group I, II, III, IV
 Restaurant, fast food - maximum 2 stand-alone fast food
 restaurants. No limit on fast food restaurants within a multi-
 occupancy building.
 Restaurants, All groups
 Retail and wholesale sales, when clearly incidental and subordinate to
 a permitted principal use on the same premises.
 Self Service Fuel Pumps - in conjunction with a convenience food and
 beverage store only. See limitations above.
 Schools: Commercial
 Signs in accordance with the City of Bonita Springs' LDC
 Social services: Group I, II and III

approved at the time of development order issuance, unless one of the following two conditions are applied for:

- (1) A Conditional Letter of Map Revision (CLOMR) will be required by contacting FEMA. A CLOMR is FEMA's comment on a proposed project that would affect the hydrologic and/or hydraulic characteristics of a flooding source and thus result in the modification of the existing regulatory floodway or effective Base Flood Elevations, or
- (2) Certification with supporting technical data, made by a registered professional engineer, is needed demonstrating that encroachments shall not result in any increase in flood levels during occurrence of the base flood discharge". Note that the words "supporting technical data" are paraphrased as "hydrologic and hydraulic analyses" in National Flood Insurance Program document (Answers to Questions About the NFIP, F-084 (5/06), FEMA). Therefore, please provide appropriate supporting data or analyses to clarify this potential floodway encroachment issue.

4. Transportation Conditions

- A. Accommodation for cross-access must be provided with properties east of Miller Road.
- B. In the event that a frontage road or reverse frontage road is constructed between the project and Trade Way Drive, then, the developer is responsible for a proportionate share (based on the percentage of left turns attributable to the project) of the cost of signalized improvements at the intersection of Bonita Beach Road and Trade Way Drive.
- C. As a condition for the project's paving, drainage, water and sanitary sewer development order, the Developer, or successor in interests or assigns, is to financially participate in studies to be performed by URS, for Bonita Beach Road from Old 41 East to the eastern terminus of the County maintained portion of Bonita Beach Road. The purpose of the study is to: (i) establish the appropriate level of background traffic for the next ten years, (ii) conduct a corridor wide traffic impact study; (iii) identify needed improvements and a procedure to assess costs to properties; and (iv) identify funding sources, proportionate fair share payments and adjustments for payments made. Any funds expended by the Developer in connection to this condition shall be reimbursed to the Developer via road impact fee credits issued by the City of Bonita Springs.

- D. The median currently located on Bonita Beach Road at Miller/Orr and Hunter's Ridge may be closed by Lee County in the near future. In addition, the applicant understands the applicable jurisdiction reserves the right to modify or close the Miller/Orr Road access onto Bonita Beach Road in the event that the intersection is found to be adverse to the public health, safety and welfare based on insufficient operational circulation and/or roadway capacity. If the Miller/Orr access point is closed then a permanent alternative access for this development will be provided. Administrative variance(s) may be granted for reasonable and adequate signage, or to modify the Master Concept Plan to accommodate the alternative access.
 - E. A local Development Order is required for any construction, improvements, modifications, or changes within the Bonita Beach Road right-of-way. As part of the submittal, a SYNCHRO analysis shall be provided with the Development Order.
 - G. The Developer has shown compliance with traffic generation with development as indicated on the Master Concept Plan, with 1,210 PM peak hour trips generated from the site. Should the developer seek to convert any of the uses to any other allowable land use by amendment to the planned development, the conversion factor would be commensurate with the trip generation rates for commercial land use types per the most recent edition of ITE Trip Generation report.
5. Approval of this zoning request does not address mitigation of the project's vehicular or pedestrian traffic impacts. Additional conditions consistent with the City of Bonita Springs LDC may be required to obtain a local development order.
 6. Approval of this rezoning does not guarantee local development order approval. Future development order approvals must satisfy the requirements of the City of Bonita Springs Comprehensive Growth Management provisions, including but not limited to, concurrency, proportionate fair share and road impact fees. Local development order approval may be subject to access changes and/or median closures.
 7. Prior to the issuance of the first development order, the applicant must obtain the vacation of Orr Road and Boondock Road, with the provision of the relocation of the accessway to be approved by the Division of Development Services, generally consistent with the approved master concept plan.

8. Zoning Resolution #Z-96-13 (O'Donnell Venture) is null and void upon approval of this ordinance.
9. The zoning for the parcel west of Orr Road, as set forth in Zoning Ordinance No. 02-022 (Smith Nursery) is hereby repealed upon the effective date of this ordinance, because this Zoning Ordinance replaces the zoning for that parcel. The remainder of the parcel east of Orr Road retains its original CPD zoning, as approved in Zoning Ordinance No. 02-22 (10,000 s/f of building area). The conditions per Zoning Ordinance No. 02-022 (Smith Nursery) still apply to the east parcel including the schedule of uses, site development regulations, compliance with the Land Development Code of the City of Bonita Springs.
10. On Tract "A" the option of a hotel/motel use may be approved through the administrative amendment process. The developer must demonstrate a hotel/motel use will not have a negative impact on public infrastructure.
11. Applicant will comply with the Carts, Cases, Baskets and Containers Act, Florida Statutes 506.501 - 506.519 and will enforce prosecution of any removal of carts by posting signs to discourage removal of shopping carts from the premises or parking areas (to keep the carts within the planned development perimeter boundary).
12. Fertilizers or hazardous chemicals must be stored within a garden center or retail portion of a building. Fertilizers or hazardous chemicals may not be stored within parking areas.
13. The type D buffer as depicted in the Master Concept Plan along Bonita Beach Road, the I-75 Ramp and the I-75 right-of-ways must be enhanced to provide minimum 14" canopy trees ranging from 4"-5" caliper, measured at four and a half feet above the ground per Land Development Code 3-420.
14. Long leaf pine, slash pine or other suitable pine trees will be incorporated into the restoration planting plans for the indigenous preserve area. The specifics of the planting plan will be reviewed and approved during the development order permitting process.
15. If any archeological/historical sites are uncovered during development activities, all work in the immediate vicinity of such sites will cease. The Developer will immediately contact the Florida Department of State, Division of Historical Resources, Lee County and the City of Bonita Springs and advise them of the discovery. The Developer will have a State-certified archaeologist determine the significance of the findings and recommend appropriate mitigation actions if necessary.

16. The development must comply with the commercial lighting standards found in Land Development Code, Chapter 3. Street, parking lot, and building lighting must be shielded so that light is directed downward to reduce light spillage to off-site parcels.
17. The structures adjacent to Bonita Beach Road and I-75 must meet architectural standards so that it does not appear that the rear of the structure(s) faces the roadways. The development will not have neon banding or gas light enhancements.
18. Any self service fuel pumps located within this development shall be required to install double wall tanks with vapor and liquid sensors between the walls to detect leakage from the inner tank. The tanks shall also be constructed with a liquid proof underlayment that traps all leaks from around the pumps and tank fill areas. Any spills that occur will be contained and designed so it can be removed. This condition shall not be interpreted to permit a less restrictive requirement than those imposed by other regulations, either now or in the future.
19. Prior to local development order approval, the development order plans must depict a minimum of 5.97 acres as overall open space provided to include a minimum of ten percent (10%) open space provided within each tract or outparcel.
20. Prior to local development order approval, the development order plans must delineate a minimum 2.54 acre preserve in substantial compliance with the approved Master Concept Plan to include:
 - ±0.82 acres of pine flatwoods labeled as indigenous preserve; and
 - ±1.61 acres of cabbage palm/live oak and disturbed lands containing large mature and/or clusters of native trees labeled as indigenous preserve restored; and
 - ±0.11 acres of non-indigenous habitat labeled as indigenous preserve created (2:1 mitigation ratio provided).
21. Prior to local development order approval, the applicant must submit for the City of Bonita Springs Environmental Sciences review and approval an indigenous management plan meeting the requirements of Land Development Code Section 3-415 (E) for the entire 2.54 acre preserve.
22. Prior to local development order approval, the applicant must submit for the City of Bonita Springs Environmental Sciences review and approval a restoration plan in conceptual compliance with the preliminary Indigenous Preservation and Restoration Plan stamped Received May 8, 2008, attached as an exhibit to the ES staff report, except to include but not

limited to: Vegetation planted to enhance the ±0.82 acre pine flatwoods preserve must be installed at trees minimum 6-8 feet and shrubs minimum 3-gallon container size.

23. Utilization of one hundred (100) percent native vegetation planting within all required landscaping, restoration, creation, and enhanced preservation areas. The applicant may install non-native plantings for accent above the required plantings in landscaped areas only.
24. Prior to local development order approval, the applicant must meet with the City of Bonita Springs Tree Advisory Board as per Land Development Code Section 3-415(B)(2) to receive recommendations on individual heritage tree and native tree cluster preservation located within the parking and open space areas. Staff may give administrative deviations to parking requirements for tree preservation with adequate space for root structure (drip line).
25. Prior to local development order approval, development regulations must provide a minimum 30 foot building and accessory structure setback from all preserve areas.
26. Prior to local development order approval, landscape plans must depict the:

Indigenous preserve along the east property line with sufficient existing vegetation to meet the intensity of a Type D buffer as required by the Land Development Code Section 3-416(D)4 or propose additional plantings to meet the intensity of a Type D buffer. Vegetation must be installed to avoid impacts to existing nature vegetation and mimic the natural canopy and understory; and

Indigenous preserve restored along the north property line planted to meet the intensity of a Type B buffer as required by the Land Development Code Section 3-416(D)4. Vegetation must be installed to avoid impacts to existing native vegetation and mimic the natural canopy and understory.
27. Prior to local development order approval, a wading bird and alligator management plan meeting the requirements of the Land Development Code Section 3-415((E) must be submitted for the Division of Environmental Sciences staff review and approval. The management and development order plans must include the location and details of signs between the lake area and Tract A that identify the presence of wading birds and alligators and that it is dangerous and illegal to feed or harass.
28. The Developer will use best efforts to incorporate principles for its buildings through the Florida Green Building Coalition program, in site design and construction, by emphasizing use of high energy efficiency

designs and use of high efficiency fixtures and appliances where possible. Dominant landscaping shall be plan species native to Florida. Building materials, both internal and external, will be comprised of durable and recycled materials where possible.

29. The applicant will provide the City of Bonita Springs with cross access for a nature trail within the area of the project located closest to the Imperial River.
30. Outdoor lighting is to be substantially reduced during non-operational hours so as to discourage light pollution or night glow.
31. VEHICLE AND EQUIPMENT DEALER CONDITION.

Vehicle and Equipment Dealers, Group I, are permitted through an administrative amendment to the commercial planned development.

The following conditions are recommended for administrative approval of this use.

1. The applicant must submit a revised master concept plan showing the general location of the proposed use, including outdoor display area, buffers, buildings, and other requirements of the LDC.
2. Outdoor display of vehicles is limited to a maximum of 100,000 s/f.
3. Vehicle and equipment dealers, Group I area allowed in conformance with LDC Sec. 34-1352 with the following enhancements.
4. All buildings and structures must comply with the following setback:
Street setback: 50 feet.
Side yard setback: 40 feet.
Rear yard setback: 40 feet.
5. All items covered by this section which are displayed or offered for sale or rent must be set back a minimum of 20 feet from any property line, unless the LDC sets forth a different setback, in which case the greater setback will apply.
6. All buildings and items covered by this section that are displayed or offered for sale or rent must be set back a minimum of 100 feet from any existing residence or any residentially zoned property. For purposes of this section, "residentially zoned property" does not include property zoned "AG."
7. Landscaping. The following landscape requirements are in addition to the requirements of the LDC.

Required landscaping adjacent to property boundaries:

Right-of-way buffer landscaping. - Landscaping adjacent to rights-of-way external to the development project must be located within a landscape buffer easement that is a minimum of 25 feet in width.

An undulating berm with a maximum slope of 3:1 must be constructed along the entire length of the landscape buffer. The berm must be constructed and maintained at a minimum average height of two feet. The berm must be planted with ground cover (other than grass), shrubs, hedges, trees and palms.

The required number of trees is five canopy trees per 100 linear feet. Three sabal palm trees may be clustered to meet one canopy tree requirement. Palms are limited to a maximum of 50 percent of the right-of-way tree requirement. Palms must be clustered and planted in staggered heights, a minimum of three palms per cluster, spaced at a maximum of four feet on center, with a minimum of a four foot difference in height between each tree.

All of the trees must be a minimum of 14 feet in height at the time of installation. Trees must have a minimum of a three and one-half inch caliper at 12 inches above the ground and a six-foot spread. At installation, shrubs must be a minimum of three gallon, 24 inches in height at time of planting and maintained at a minimum of 36 inches in height within one year of planting. The shrubs must be planted three feet on center.

Landscaping adjacent to all other property lines.

Side property boundaries (other than those adjacent to rights-of-way) must be planted with a single hedge row consistent with the minimum requirements of the LDC. The hedge must be a minimum of 24 inches in height at planting, planted at three feet on center and must be maintained at a height of 36 inches within 12 months of planting.

Rear property boundaries (other than those adjacent to road rights-of-way) must be planted with a single hedge row. The hedge must be a minimum of 24 inches in height at planting, planted at three feet on center and must be maintained at a height of 36 inches within 12 months of planting.

8. Perimeter walls. These sites must be separated from adjacent residentially zoned or residentially developed properties by an architecturally designed eight-foot high solid wall utilizing materials similar in color, module and texture to those utilized for the building. Landscaping must be planted on the residential side of the fence or wall. The wall must be setback 25 feet from the property line and include five trees per 100 linear feet and a double hedge row. The

trees and shrubs must meet the minimum planting standards per the LDC.

9. Outdoor speakers. The use of public address or loudspeaker systems that broadcast outdoors is prohibited.

B. Deviations:

Deviation 1 from LDC Sec. 3-329(e)(1) is approved, granting relief from the requirement of a 50-foot water retention excavation setback to 25 feet from a property line under separate ownership, subject to the following conditions.

1. Approval is limited to lakes as shown on the approved master concept plan.
2. The development order must provide adequate safeguards for the protection of pedestrians and nearby residents such as fences, buffers, plantings, or other means.
3. Prior to local development order approval, development order plans must depict where the setback is reduced and a littoral zone will be provided. The littoral zone must include a minimum 4 horizontal to 1 vertical slope with a 10 foot wide littoral shelf at 2 foot depth below control elevation. The littoral planting requirement must be calculated at 2 herbaceous plants per linear foot of shoreline. The lake's edge within the minimum 25 setback area must be planted with a mix of native wetland trees and shrubs meeting Section 3-416(D)4 Type B density and size requirements, and may be used to meet buffer requirements but may not be used to substitute for required littorals

Deviation 2 is approved, granting relief from the requirement of LDC Sec. 3-329(e)(4) no greater than 4:1 to permit 20% of bank to utilize vertical bulkheads, subject that prior to local development order approval, development order plans must depict the design of the ±2.77 acre lake to include a compensatory littoral zone abutting the north property line and preserve areas equal to the linear footage of the bulkhead. The littoral zone must include a minimum 4 horizontal to 1 vertical slope with a 10 foot wide littoral shelf at 2 foot depth below control elevation. The littoral planting requirement must be calculated at 2 herbaceous plants per linear foot of shoreline.

Deviation 3 is approved, granting relief from the requirement of LDC Sec. 34-2194(b) requiring a 25 foot setback for buildings adjacent to a body of water to allow 0-foot setbacks adjacent to bulkheads, conditioned per Deviation #2.

Deviation 4 is approved, granting relief from the requirement of LDC Sec. 3-415(B)(1)(A) which requires developments with existing native vegetation to provide 50% of their open space percentage requirement of 2.1 acres, through the onsite preservation of existing native vegetation, to allow the indigenous requirement to be met through a combination of 0.82 acres of indigenous

preservation, 1.61 acres of heritage tree cluster preservation and restoration and 0.11 acres of landscape enhancement and restoration. The deviation is approved as conditioned by conditions 20, 21, 22, 23, and 24 of this zoning ordinance.

Findings and Conclusions:

Based upon an analysis of the application and the standards for approval of planned development rezonings, Bonita Springs City Council makes the following findings and conclusions:

1. The applicant has proven entitlement to the rezoning to Commercial Planned Development (CPD) by demonstrating compliance with the Bonita Springs Comprehensive Plan, the Land Development Code, and other applicable codes and regulations.
2. The requested CPD zoning, as conditioned:
 - a) meets or exceeds all performance and locational standards set forth for the potential uses allowed by the request;
 - b) is consistent with the densities, intensities and general uses set forth in the Bonita Springs Comprehensive Plan;
 - c) is compatible with existing or planned uses in the surrounding area; and
 - d) will not adversely affect environmentally critical areas and natural resources.
3. Approval of the request will not place an undue burden upon existing transportation or planned infrastructure facilities, as conditioned.
4. Urban services, as defined in the Bonita Springs Comprehensive Plan, are available and adequate to serve the proposed land use, as conditioned.
5. The proposed mix of uses is appropriate at the subject location, as conditioned.
6. The recommended conditions to the concept plan and other applicable regulations provide sufficient safeguards to the public interest.
7. The recommended conditions are reasonably related to the impacts on the public's interest created by or expected from the proposed development.
8. The deviations granted, as conditioned:

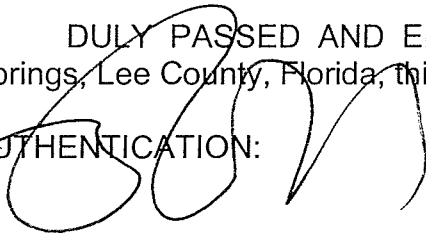
- a) enhance the objectives of the planned development; and
- b) preserve and promote the general intent of the LDC to protect the public health, safety and welfare.

SECTION TWO: EFFECTIVE DATE

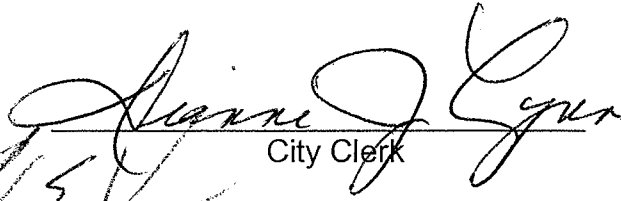
This ordinance shall take effect thirty (30) days from the date of adoption.

DULY PASSED AND ENACTED by the Council of the City of Bonita Springs, Lee County, Florida, this 28th day of July, 2008.

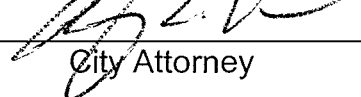
AUTHENTICATION:



 Mayor



 City Clerk

APPROVED AS TO FORM: 

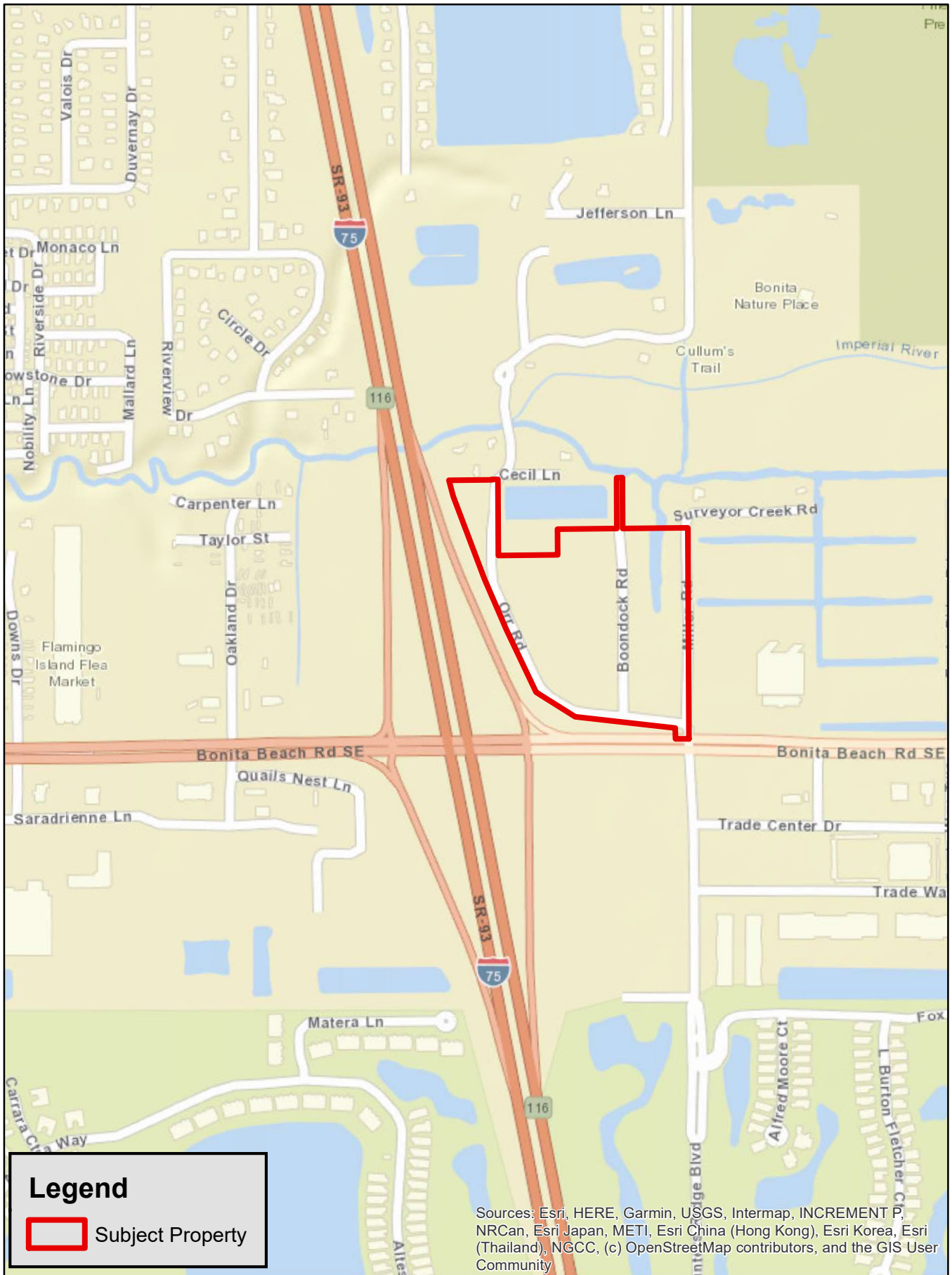
 City Attorney

Vote:

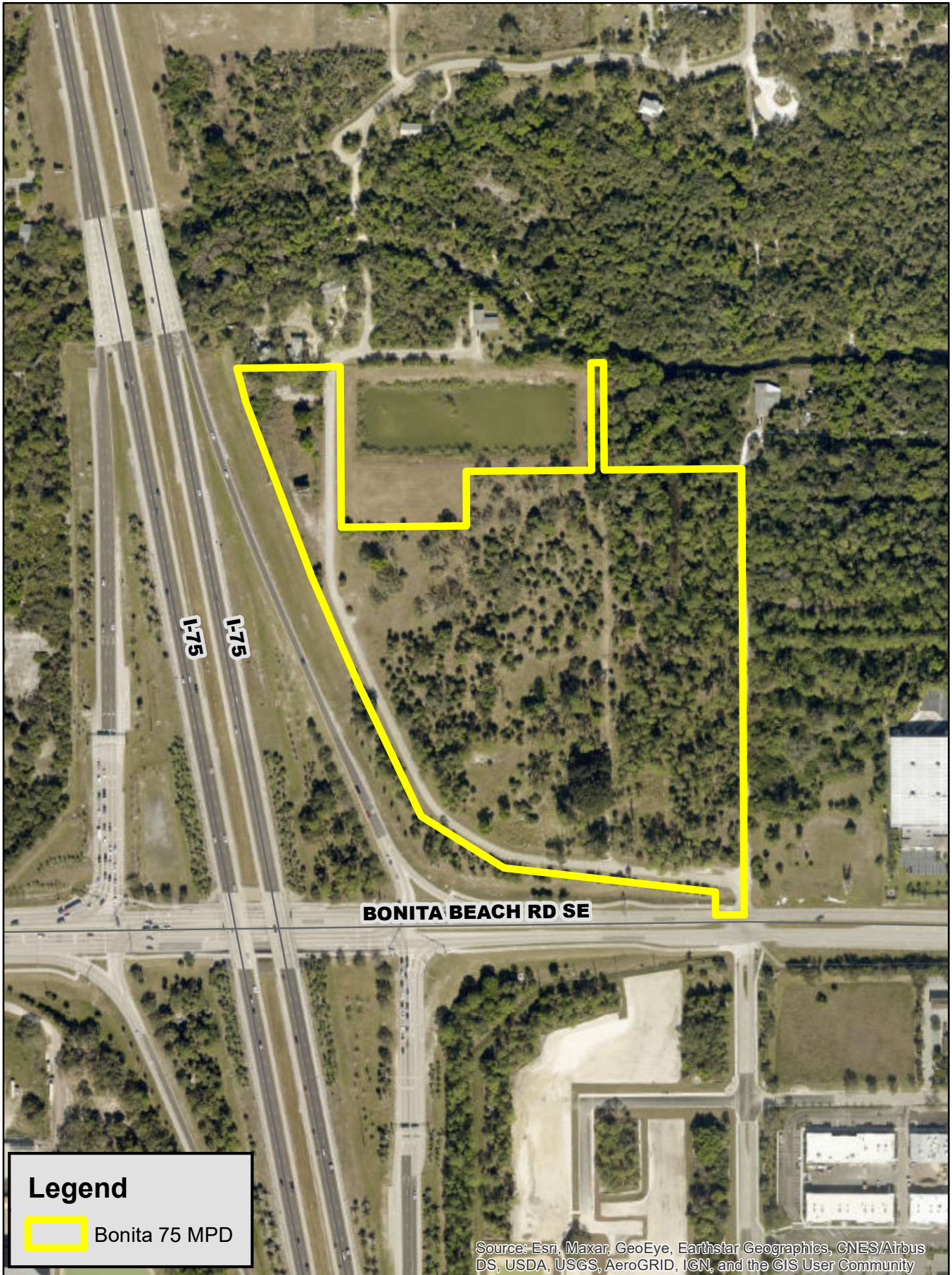
Ferreira	Nay	Nelson	Aye
Lonkart	Nay	Simons	Aye
Martin	Aye	Spear	Aye
McCourt	Nay		

Date filed with City Clerk: 8-6-08


Bonita 75 MPD Exhibit II-D Area Location Map



Bonita 75 MPD Exhibit IV-N Aerial



Legend

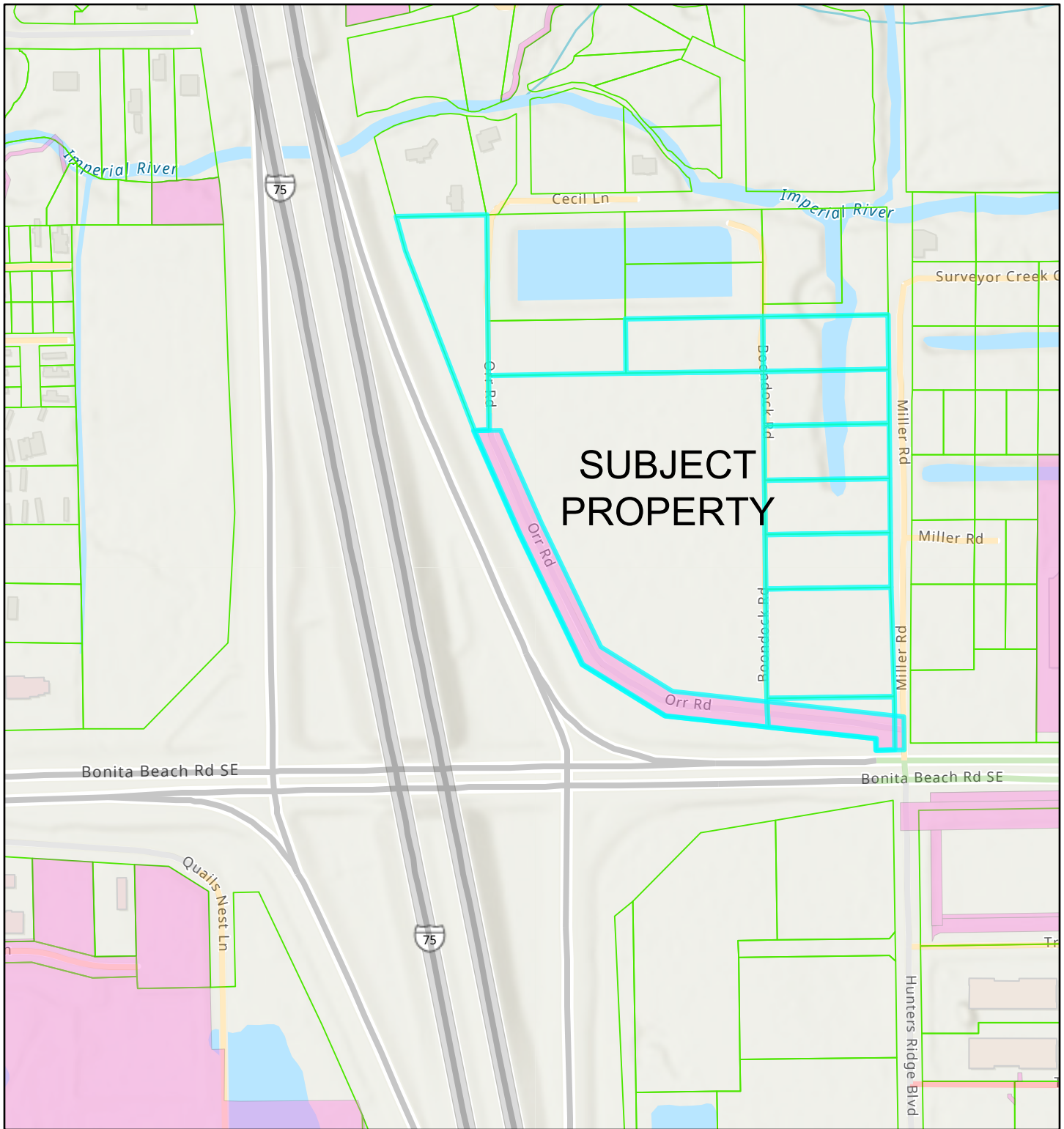
 Bonita 75 MPD

Source: Esri, Maxar, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community










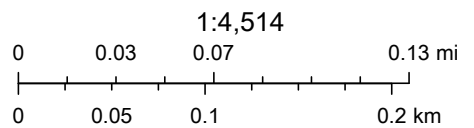
Bonita 75 MPD

Exhibit IVC ROW and Easement Maps



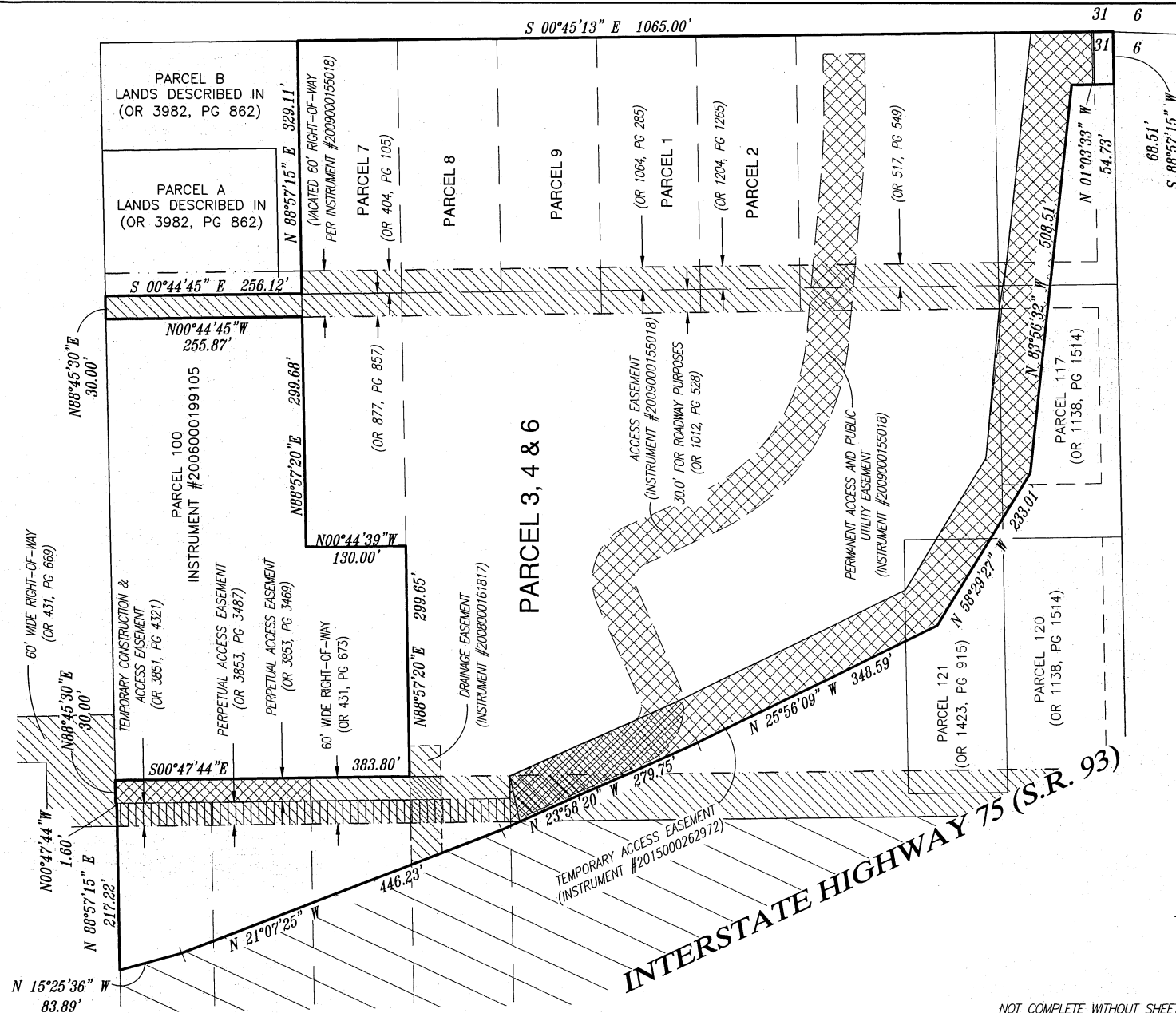
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|  Parcels |  Non-County Maintained |
|  Easements * |  Privately Maintained |
| Roads Maintained By |  City of Bonita Springs |
|  County Maintained |  State Maintained |



Esri Community Maps Contributors, University of South Florida, County of Collier, County of Lee, FL, FDEP, BuildingFootprintUSA, Esri, HERE, Garmin, SafeGraph, INCREMENT P, METI/NASA, USGS, EPA, NPS, US Census Bureau, USDA, Sources: Esri, Airbus DS, USGS, NGA, NASA,

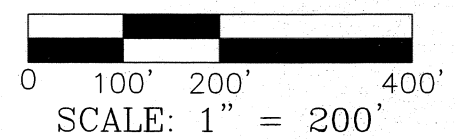
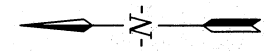
*Also see Sketch and Description and Master Plan



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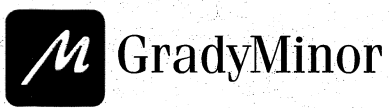
LEGEND

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THIS PLAN MAY HAVE BEEN ENLARGED OR
REDUCED FROM INTENDED DISPLAY SCALE
FOR REPRODUCTION REASONS

NOT COMPLETE WITHOUT SHEETS 1 AND 2 OF 2



Civil Engineers • Land Surveyors • Planners • Landscape Architects
 Cert. of Auth. EB 0005151 Cert. of Auth. LB 0005151 Business LC 26000266
 Bonita Springs: 239.947.1144 www.GradyMinor.com Fort Myers: 239.690.4380

Q. Grady Minor and Associates, P.A.
3800 Via Del Rey
Bonita Springs, Florida 34134

SKETCH AND DESCRIPTION

BONITA I-75

LYING IN
SECTION 31, TOWNSHIP 47 SOUTH, RANGE 26 EAST
LEE COUNTY, FLORIDA

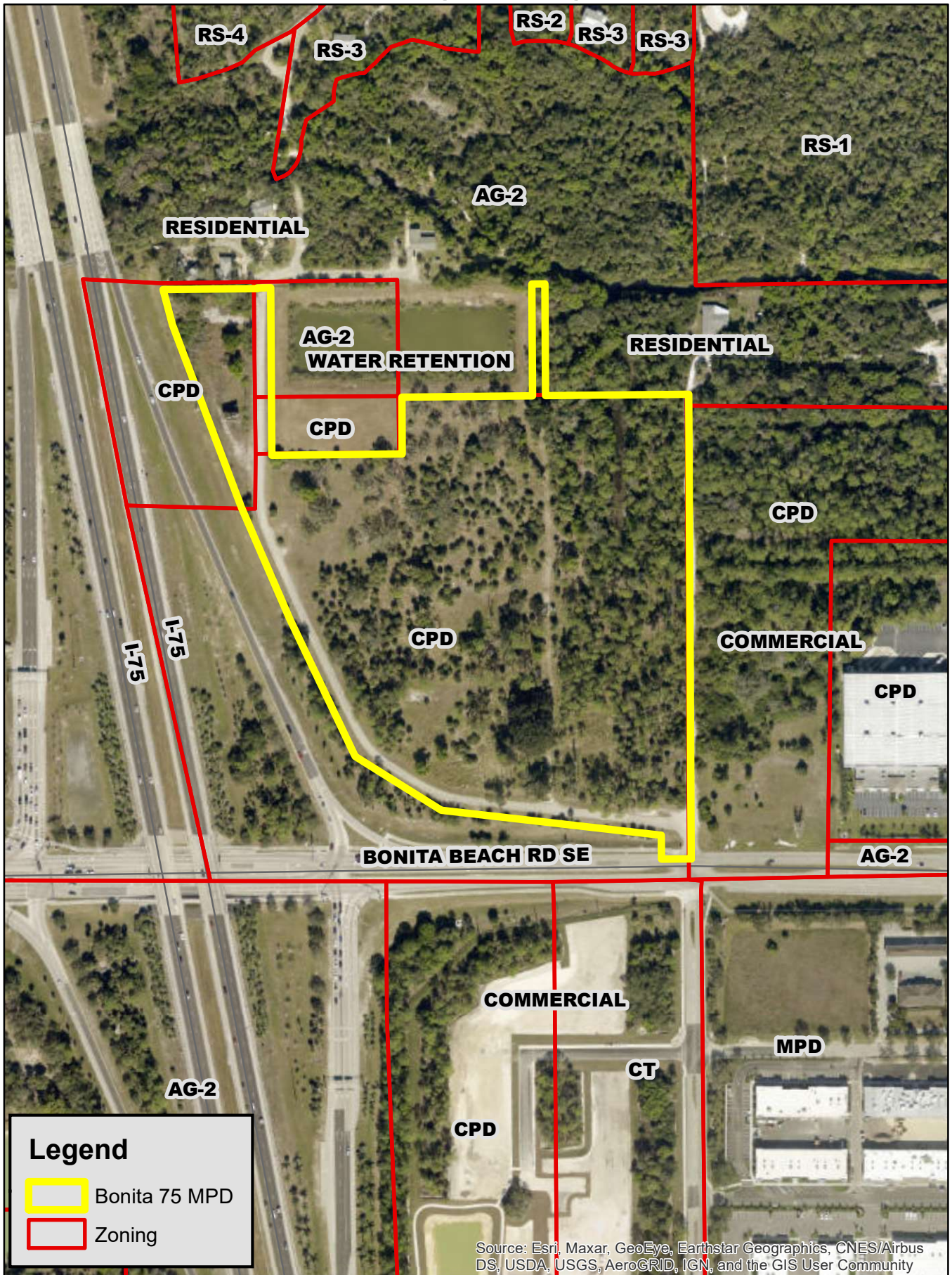
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CHECKED BY:	DLS
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Bonita 75 MPD

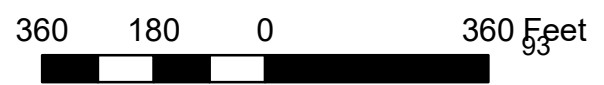
Exhibit II-F-3 Existing Zoning/Land Use Map



Legend

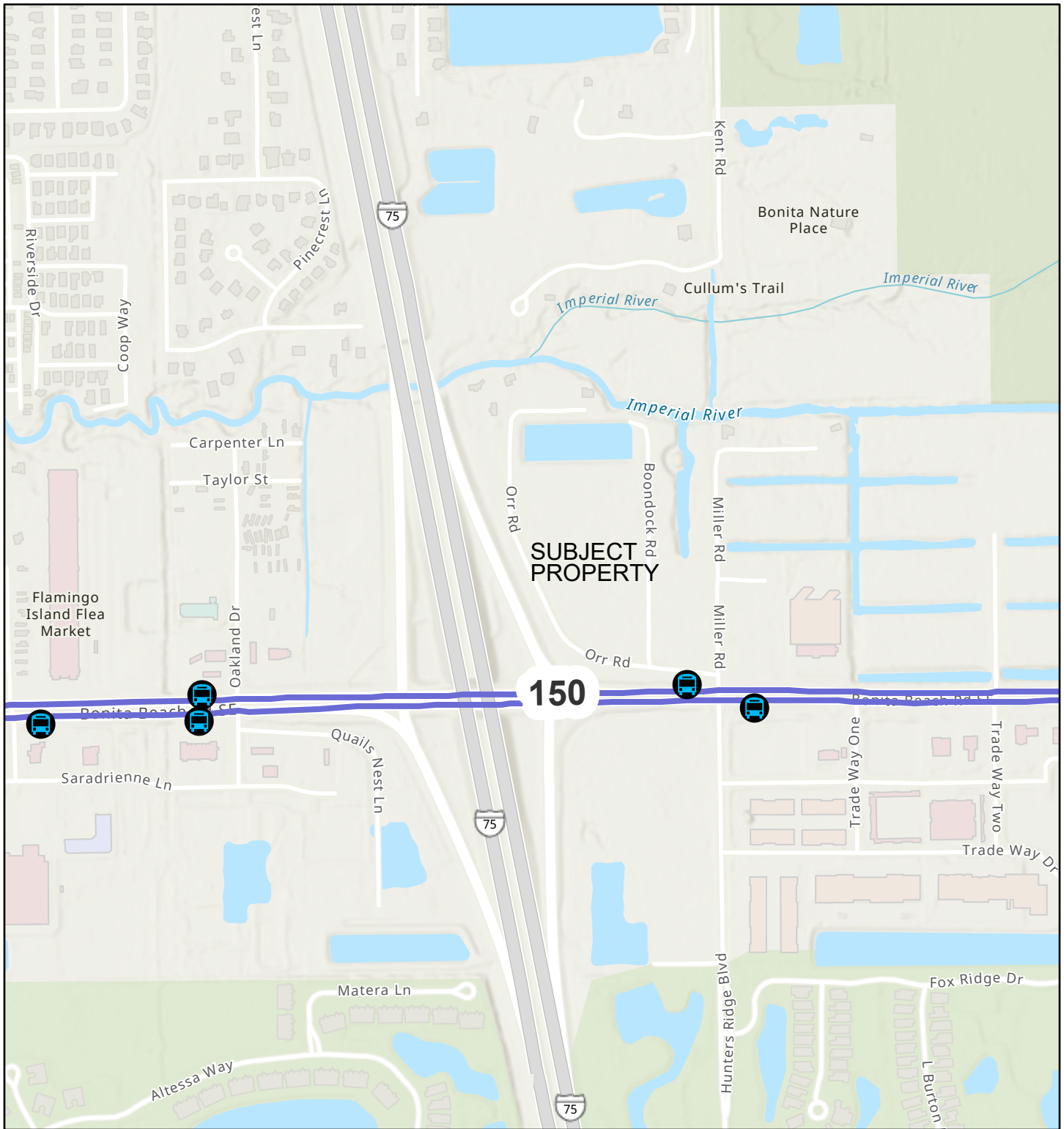
- Bonita 75 MPD
- Zoning

Source: Esri, Maxar, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community




Bonita 75 MPD

Exhibit IV-A Public Transit Routes Map



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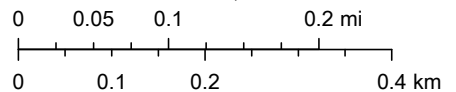
 Bus Stops

Bus Routes

 150

ADA Corridor

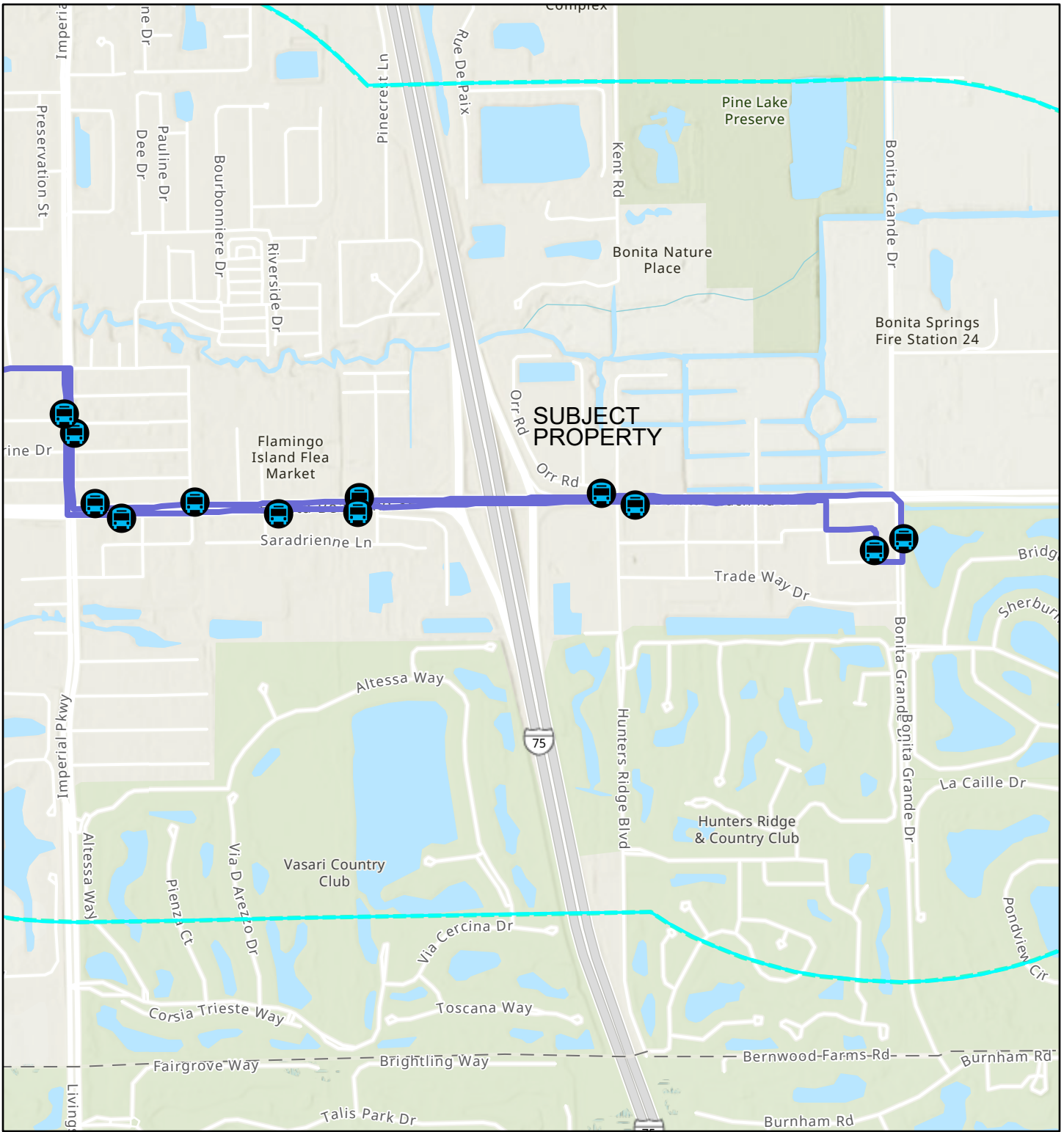
 IN the 3/4 Mile ADA Corridor




Esri Community Maps Contributors, University of South Florida, County of Collier, County of Lee, FL, FDEP, Esri, HERE, Garmin, SafeGraph, INCREMENT P, METI/NASA, USGS, EPA, NPS, US Census Bureau, USDA, Esri, NASA, NGA, USGS, FEMA

Bonita 75 MPD

Exhibit IV-A Public Transit Routes Map



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 Bus Stops

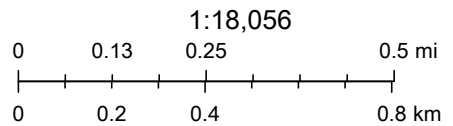
Bus Routes

 150

ADA Corridor

 IN the 3/4 Mile ADA Corridor

NOT in the 3/4 Mile ADA Corridor



Esri Community Maps Contributors, University of South Florida, County of Collier, County of Lee, FL, FDEP, Esri, HERE, Garmin, SafeGraph, INCREMENT P, METI/NASA, USGS, EPA, NPS, US Census Bureau, USDA, Esri, NASA, NGA, USGS, FEMA

Sec. 4-295. Application.

- (a) *Minimum required information for all planned development zoning applications.* Rezoning applications for planned developments must include the following information, supplemented, where necessary, with written material, maps, plans, or diagrams. Wherever this section calls for the exact or specific location of anything on a map or plan, the location must be indicated by dimensions from an acceptable reference point, survey marker or monument.
- (5) *Explanation.* A narrative explanation as to how the proposed development complies with the Bonita Plan, the design standards set forth in section 4-325, and the guidelines for decision-making embodied in section 4-131.

The subject project, which is located in the northeast quadrant of Interstate 75 and Bonita Beach Road, is currently zoned Bonita 75 CPD (ZO-08-10).

The property is designated Interchange Commercial on the City of Bonita Springs Future Land Use Map, which permits a wide range of commercial land uses. Policy 1.1.15, Interchange Commercial, of the Future Land Use Element indicates that this land use category is intended for uses that serve the traveling public, including gas/convenience, restaurants and a range of tourist related commercial uses. Policy 1.1.15 also permits multi-family residential development at a standard maximum density of 10 du/ac and up to 15 du/ac with the use of bonus density. The proposed density is approximately 12 du/ac. The property is also located within the Interchange Zone of the Bonita Beach Road Corridor Overlay. The proposed CPD/RPD includes residential and commercial uses consistent with those permitted in Section 4-898 of the LDC.

The applicant, Madison Capital Group Management, LLC, is proposing to rezone the CPD to CPD/RPD and reduce the overall commercial square footage and add multi-family residential dwelling units. The application will seek 252 multi-family dwelling units, which is approximately 12 du/ac. An affordable housing bonus density agreement will be filed separately for the 47 bonus units. A maximum of 10,000 square feet of commercial uses consistent with those permitted in Section 4-898 of the Bonita Beach Road Corridor Overlay are proposed. The RPD portion of the project will include an amenity area, which will include a clubhouse containing the rental apartment leasing offices, as well as indoor fitness facilities, meeting space and restrooms. The outside of the amenity area will feature a swimming pool with shade structures. The amenity area is designed to be accessible to all residents of the community via internal pedestrian facilities. The amenities are not designed to serve the CPD portion of the project; however, having commercial in close proximity to the residential use is seen as beneficial for the residents. Having the commercial component easily accessible by foot or automobile provides convenient employment or shopping opportunities for the residents of the RPD.

The proposed rental apartments within the RPD will be a maximum of 4 stories in height, although some 2 story buildings may be constructed depending on final site design. The amenity building

will be a single-story building. The CPD has a proposed height limitation of 35 feet which will provide for 2 story maximum heights. These proposed heights are compatible with the limited residential dwellings located at the terminus of Orr Road and Miller Road north of the RPD/CPD due to the strategic location of the proposed open space areas on the site and the presence of lands owned by the State located between the RPD/CPD and these home sites.

The property is located in the Interchange Commercial FLU Category, and it is located at the I-75 northbound entrance ramp. While the project does not have direct frontage on Bonita Beach Road, it does abut Orr Road and Miller Road. The proposed uses are oriented to Bonita Beach Road as close as feasible given the separation from Bonita Beach Road by the I-75 on ramp and Orr Road.

The site consists of approximately 20.99 acres. The entire site has been previously impacted by past usage. Historical aerials from 1995 and 2004 have been included. These aerials show the site containing single family uses. In addition, they show parked tractor trailers, cleared areas and accessory structures. As such, most of the site lacks any native habitat. Most of the site has been altered. The site does have some scattered canopy trees of live oaks, Slash pines and cabbage palms. However, there is little to no ground cover and/ or midstory. A tree survey was conducted. To date, the site has cleared areas with remnants of past structures and debris.

A proposed condition for revegetation within the floodway is anticipated and consistent with the proposed MCP.

Sec. 4-131. Zoning board standard for review.

(d) *Zoning matters.*

- (3) *Findings.* Before preparing their recommendation to the city council on a rezoning, the zoning board must find that:
 - a. The applicant has proved entitlement to the rezoning or special exception by demonstrating compliance with the Bonita Plan, this Land Development Code, and any other applicable code or regulation;

The proposed rezone to add multifamily residential uses is consistent with the Bonita Springs Comprehensive Plan.

Policy 1.1.15, Interchange Commercial provides for a wide variety of commercial land uses and also provides for multifamily residential development. The proposed PD rezone will reduce the amount of commercial square footage. The current zoning permits a maximum of 158,800 square feet of commercial uses and this will be reduced to a maximum of 10,000 square feet of commercial land uses. The schedule of uses has been amended to add 252 multifamily dwelling units. Development standards for the residential dwellings have also been added to the PD. The proposed 12 du/ac density is consistent, with the FLUC density with the use of bonus units, is within the permissible maximum density per Policy 1.1.15.

Future Land Use Policy 1.1.2, Density, Affordable Housing Density Bonus and Wetlands Transfer identifies that property designated Interchange Commercial are eligible for up

to 5 du/ac bonus units. The applicant intends to use the payment in lieu of process to obtain the 47 bonus units. The project at the standard maximum density of 10 du/ac is eligible for 205 dwelling units. This PD application proposes a total of 252 dwelling units and is within the allowable density range. A separate application for the bonus density will be filed for the project.

Future Land Use Policy 1.7.6 supports protection of environmentally sensitive lands. The property is not adjacent to the Imperial River/Keel Canal; however, the northernmost portion of the PD is within a FEMA designated floodway. This area is identified on the MCP and no structures will be placed within this portion of the site. This area will be utilized for floodplain compensation and replanting with native vegetation in accordance with the LDC provisions for open space. The use of this area is consistent with Policy 1.7.6.

Policy 1.11.1 establishes LOS standards. There are no existing or projected LOS deficiencies for public facilities associated with this PD rezone. A Traffic Impact Analysis has been prepared for the project and concludes that the reduction of commercial uses and addition of the proposed 252 multifamily dwelling units reduces traffic over that for the commercial only planned development.

Objective 1.16. and Policies 1.16.1 - 1.16.6 reference the establishment of the Bonita Beach Road Corridor and Bonita Beach Road Corridor Map and implementing regulations in the Land Development Code. The PD is within the Interstate Zone on the Corridor Map. This area is also referred to as the "Gateway Zone".

The proposed commercial and residential uses are consistent with the permitted uses for the corridor as identified in Section 4-898, Table 4-898 of the LDC.

The PD has requested a deviation from the design standards outlined in Section 4-899 to provide development standards as proposed for the CPD/RPD. Most of the Section 4-899 standards are not applicable as the site does not have frontage on Bonita Beach Road.

The residential portion of the site is proposed to be developed as a gated multifamily community that will not have platted streets. There is one commercial tract proposed at the Orr Road intersection with Miller Road. Future interconnections to Miller Road are shown on the MCP. No blocks within the PD will be provided; however, the residential component of the project is walkable and will feature central amenities designed to be easily accessible to all residents including the commercial tract.

Pedestrian access from the project entrance to the pathway located in the Bonita Beach Road ROW will be provided.

Transportation Element

Policy 1.1.3 establishes minimum acceptable Level of Service Standards for roadways. A Traffic Impact Analysis has been prepared by a registered traffic engineer, which finds that the adopted LOS standards will be met based on the intensity of development proposed in the CPD/RPD.

Housing Element

Policy 1.3.2.C and 1.3.2.d acknowledges the use of bonus densities by making a cash contribution dedicated to the provisions of affordable housing. The developer is proposing to make the cash contribution consistent with this policy for 47 bonus density units. The resulting project will not have substantially increased intensities of land use on the perimeter. The project will have open space and indigenous preservation along the northern property boundary nearest the residential neighboring property. The traffic associated with the project will not be required to travel through areas with significantly lower densities. Access to the project will be via Bonita Beach Road/Miller Road. There are adequate public facilities available to service the proposed 252-unit apartment complex. There will be no decrease in open space areas that have any adverse impact to an existing or approved development. The preserve areas are largely located in the areas previously identified as preserves for the commercial-only development scenario. The project is not located in a Category 1 storm surge area; therefore, no storm shelters or other mitigation is required.

Conservation and Coastal Management Element

Policy 7.2.2 provides that large developments will provide 50% of their open space requirement using existing indigenous native vegetation. A scaled credit as identified in LDC Section 3-417 is eligible as an incentive to preserve indigenous habitat. The subject property has been largely cleared in the past and while there are indigenous trees, there is no indigenous habitat. The existing CPD was previously approved with a preservation area that included native trees. The proposed RPD/CPD increases the preservation area required due to the requirement to provide 40% open space for the residential component of the RPD and 20% for the CPD. A condition is proposed to address restoration and heritage tree preservation.

Objective 7.4 requires that the City protect habitats of endangered and threatened species. A listed species survey was conducted on the property as recently as May 2021. No listed species were observed on the property. As discussed in the report the site does not contain indigenous habitat and it is located at the intersection of I-75 and Bonita Beach Road, making the site not conducive to attracting listed species.

- b. The request will meet or exceed all performance and locational standards set forth for the potential uses allowed by the request;

There are no performance or locational standards for the uses. The proposed uses are to be sited in a compatible manner with the surrounding community but taking into account the relationship with the adjacent roads and private access.

- c. The request, including the use of TDR or affordable housing bonus density units, is consistent with the densities, intensities and general uses set forth in the Bonita Plan;

The proposed use of affordable housing bonus density units will be in compliance with the LDC and Comprehensive Plan. Staff has provided direction to the applicant for their use and such application is a companion item to this zoning application. A narrative and draft agreement is provided separately.

- d. The request is compatible with existing or planned uses in the surrounding area;

The surrounding areas are low density residential to the north. A large open space buffer is provided to the adjoining FDOT lake which separates farther the residential areas to this site. The area to the east is an existing commercial PD. Shared access is planned with that use through improvement to Miller Road.

The proposed CPD/RPD is also consistent with the required criteria in LDC 4-299(2)a-c. The proposed mix of uses are appropriate at the subject location. All uses are consistent with the Interchange Commercial Future Land Use Category, and the density is within the density range associated with this land use category. The location is appropriate for higher density rental apartments given the proximity to I-75 and Bonita Beach Road, allowing for easy access to employment and close proximity to local goods and services.

The proposed development standards, buffers, and location of open spaces provides sufficient safeguards to the public interest. The RPD portion of the site will be separated from the few single-family homes located at the terminus of Orr Road by an FDOT water management area, a large flood plain compensation area and re-created preservation area. The staff recommended conditions are related to the impacts on the public interest created by the proposed CPD/RPD. The commercial use will also be accessible to the residents of the CPD/RPD and is designed in a manner to capture their use by pedestrian and vehicular access.

- e. Approval of the request will not place an undue burden upon existing transportation or planned infrastructure facilities and will be served by streets with the capacity to carry traffic generated by the development;

The traffic impact statement (TIS) submitted with this application confirms the proposed uses will not have an adverse impact on capacity.

- f. Where applicable, the request will not adversely affect environmentally critical areas and natural resources;

The site was previously cleared for homesteads in the 1970s and 1980s. No indigenous area remains, however individual indigenous trees do exist. The limits of the floodway have been identified on the MCP. No development will occur within the floodway with the exception of revegetation with ground cover.

- g. In the case of a planned development rezoning, the decision of the zoning board must also be supported by the formal findings required by section 4-299(a)(2) and (4);

Acknowledged.

- h. The zoning board must also find that public facilities are, or will be, available and adequate to serve the proposed land use.

Acknowledged.

Chapter 4-325 Consistency

The CPD/RPD as proposed is in compliance with the General Standards for Planned Developments found in Chapter 4-325.

- a. All planned developments shall be consistent with the provisions of the Bonita Plan.

As discussed in the Chapter 4—295 analysis, the project is consistent with the Bonita Springs Comprehensive Plan.

- b. All planned developments, unless otherwise excepted, shall be designed and constructed in accordance with the provisions of all applicable city development regulations in force at that time.

Except as approved by deviation, the planned development is designed and will be consistent with applicable City regulations in effect at the time of Development Order approval.

- c. The tract or parcel proposed for development under this article must be located so as to minimize the negative effects of the resulting land uses on surrounding properties and the public interest generally, and must be of such size, configuration and dimension as to adequately accommodate the proposed structures, all required open space, including private recreational facilities and parkland, bikeways, pedestrian ways, buffers, parking, access, on-site utilities, including wet or dry runoff retention, and reservations of environmentally sensitive land or water. In large residential or commercial planned developments, the site planner is encouraged to create subunits, neighborhoods or internal communities which promote pedestrian and cyclist activity and community interaction.

The MCP identifies the areas to be developed for MF residential and commercial uses consistent with the Bonita Beach Road Corridor Overlay. The MCP also identifies areas of preservation, open space, and water management. Sidewalks will be provided on Miller Road and the portion of Orr Road that lies between the CPD and RPD tracts.

- d. The tract or parcel shall have access to existing or proposed roads:

- (1) In accordance with chapter 3 and as specified in the Bonita Plan traffic circulation element or the official trafficways map of the county;
- (2) That have either sufficient existing capacity or the potential for expanded capacity to accommodate both the traffic generated by the proposed land use and that traffic expected from the background (through traffic plus that generated by surrounding land uses) at a level of service D or better on an annual average basis and level of service E or better during the peak season, except where higher levels of service on specific roads have been established in the Bonita Plan; and
- (3) That provide ingress and egress without requiring site-related industrial traffic to move through predominantly residential areas.

Primary project access will be via Miller Road, which will be improved from Bonita Beach Road to the proposed residential project entry. Miller Road connects to Bonita Beach Road at an existing location matching that shown on the MCP. The TIS prepared for the proposed RPD/CPD concludes that far few trips are generated from the proposed project that includes MF residential dwellings versus the existing PD which has no residential component. No LOS roadway issues result from the project.

- e. If within the Lee Tran public transit service area, the development shall be designed to facilitate the use of the transit system.

There is a Lee Tran stop just west of the Miller Road connection on Bonita Beach Road. The Bonita 75 CPD/CPD will provide a pedestrian connection to the transit stop.

- f. Development and subsequent use of the planned development shall not create or increase hazards to persons or property, whether on or off the site, by increasing the probability or degree of flood, erosion or other danger, nor shall it impose a nuisance on surrounding land uses or the public's interest generally through emissions of noise, glare, dust, odor, air or water pollutants.

The proposed CPD/CPD will not create hazards to persons or property off-site. The site has previously been issued an ERP through the SFWMD and has obtained a FEMA map amendment which has been noted on the MCP. Glare, dust and odor will not be issues as the site is less intense than originally approved.

- g. Every effort shall be made in the planning, design and execution of a planned development to protect, preserve or to not unnecessarily destroy or alter natural, historical or archaeological features of the site, particularly mature native trees and other threatened or endangered native vegetation. Alteration of the vegetation or topography that unnecessarily disrupts the surface water or groundwater hydrology, increases erosion of the land, or destroys significant wildlife habitat is prohibited. That habitat is significant that is critical for the survival of rare, threatened or endangered species of flora or fauna.

There are no known archeological sites on the property. The site has been previously developed and there are no native vegetation communities remaining on the site. The applicant will be working with staff and the Tree Advisory Board to address the retention and/or relocation of native trees on-site. the Tree Advisory Board meeting was held on May 9, 2022 and they approved the plan as proposed.

- h. A fundamental principle of planned development design is the creative use of the open space requirement to produce an architecturally integrated human environment. This shall be coordinated with the achievement of other goals, e.g., the preservation or conservation of environmentally sensitive land and waters or archaeological sites.

The site has been designed to utilize open spaces for integration of the human environment. The dry detention areas will be designed to provide areas for human and pet use during the dry season.

- i. Site planning and design shall minimize any negative impacts of the planned development on surrounding land and land uses.

The site has been designed to have no impact on surrounding uses. Building heights, setbacks and placement of open space areas are utilized to reduce any negative impacts.

- j. Where a proposed planned development is surrounded by existing development or land use with which it is compatible and of an equivalent intensity of use, the design emphasis shall be on the integration of this development with the existing development, in a manner consistent with current regulation.

The PD has been designed to share the primary access (Miller Road) with the neighboring St. James PD. Orr Road has been realigned in order to provide for convenient access to the small number of properties located north of the site and to increase the throat depth from Bonita Beach Road.

- k. Where the proposed planned development is surrounded by existing development or land use with which it is not compatible or which is of a significantly higher or lower intensity of use (plus or minus ten percent of the gross floor area per acre if a commercial or industrial land use, or plus or minus 20 percent of the residential density), or is surrounded by undeveloped land or water, the design emphasis will be to separate and mutually protect the planned development and its environs.

The uses located within the PD have been designed to lessen any impact to neighboring properties by placing the residential component in closer proximity to surrounding homes and the commercial component has been significantly reduced in square footage and overall intensity. The residential remains separated from homes at the north end of Orr Road by lakes and open space/preservation areas.

- I. In large residential or commercial planned developments, the site planner is encouraged to create subunits, neighborhoods or internal communities which promote pedestrian activity and community interaction.

The project is approximately 20 acres in size and due to the residential use as an apartment complex it is not possible to create subunits. The site will be arranged so that all residents will have good access to project amenities.

- m. In order to enhance the viability and value of the resulting development, the designer shall ensure the internal buffering and separation of potentially conflicting uses within the planned development.

Buffers will be provided as identified on the MCP. The master developer intends to develop an integrated community with the small commercial property being developed in a complementary manner for the residents.

- n. Density or type of use, height and bulk of buildings and other parameters of intensity should vary systematically throughout the planned development. This is intended to permit the location of intense or obnoxious uses away from incompatible land uses at the planned development's perimeter, or, conversely, to permit the concentration of intensity where it is desirable, e.g., on a major road frontage or at an intersection.

The perimeter of the PD is protected by the placement of open space areas including buffers, vegetation preservation and lakes.

- o. Unless otherwise provided for in this article, minimum parking and loading requirements shall be as set forth in article VI, divisions 25 and 26, of this chapter. Where it can be reasonably anticipated that specified land uses are generators of occasional peak demand for parking space, a portion of the required parking may be pervious or semi-pervious surfaces subject to the condition that it be constructed and maintained so as to prevent erosion of soil. In all cases, however, sufficient parking shall be provided to prevent the spilling over of parking demand onto adjacent properties or rights-of-way at times of peak demand.

Parking is intended to be developed at a ratio consistent with that needed for the development. A parking deviation has been requested consistent with other rental apartments they have developed in Florida.

- p. Joint use of parking by various land uses within the planned development may be permitted by special condition where it can be demonstrated or required that the demand for parking by the various uses will not conflict. Joint parking agreements between uses within and uses without the planned development shall be governed by agreement per general regulation (see section 4-1730), without exception.

All areas within the residential development area will share joint use of the parking lots. No parking agreement is necessary as it will be under singular ownership.

- q. Internal consistency through sign control, architectural controls, uniform planting schedules and other similar controls is encouraged.

The Developer will establish the signage, architecture and planning schedule which will insure internal consistency.

Sec. 4-899. - Site design standards.

(a) Property development regulations.

	Special Notes or Regulations	Commercial Zone	Historic Zone	Interstate Zone	Beach Zone
NA Maximum Block Size	Note (1)	375' by 375', Maximum Block perimeter 1500'	375' by 375', Maximum Block perimeter 1500'	375' by 375', Maximum Block perimeter 1500'	375' by 375', Maximum Block perimeter 1500'
Setbacks:					
NA Minimum Street (feet)	Note (2)	20'	15'	20'	15'
JA Maximum Street (feet)	Notes (2), (3), (4) and (5)	25'	25'	25'	25
Side (feet)		15'	0'/15'	0'/15'	0'/15'
Rear (feet)		20'	20'	20'	20
Water body (feet):	<u>4-1892</u> et seq., Note (6)				
NA Gulf of Mexico	In accordance with chapter 6, article III, or 50 feet from mean high water, whichever is the most restrictive.				

NA

Special regulations:					
Animals, reptiles, marine life	<u>4-1045</u> et seq.				
Consumption on premises	<u>4-1020</u> et seq.				
Docks, seawalls, etc.	<u>4-1588</u>				
Essential services	<u>4-1362</u> et seq.				
Essential service facilities (4-408(c) (13))	<u>4-1362</u> et seq., <u>4-1841</u>				
Fences, walls, gatehouses, etc.	<u>4-1841</u> et seq.				
Nonroofed accessory structures	<u>4-1894(c)(3)</u>				
Railroad right-of-way	<u>4-1895</u>				

<p>Maximum height (feet)</p>	<p><u>4-1871</u> et seq. Note: Special height limitations apply to all uses located within Bonita Beach and Bonita Beach Road Corridor. See <u>section 4-1874</u></p>	<p>35</p>	<p>35</p>	<p>35</p>	<p>35</p>
<p>Maximum lot coverage (percent of total lot area)</p>		<p>40%</p>	<p>40%</p>	<p>40%</p>	<p>40%</p>

NA

OK

NA

- (1) Projects exceeding three (3) acres in size shall submit a block layout plan. Blocks located adjacent to arterials are not required to obtain a deviation from LDC Section 3-291, Connection separation subject to review of LDC 3, Article III, Division 2-Transportation, Roadways, Streets and Bridges at local development order review.
 - (2) Accessory structures such as arcades or plazas and courtyards may meet the intent of this requirement. Existing non-conforming developments shall redevelop by incorporating covered sidewalks with arcades and shade trees.
 - (3) Projects located along secondary streets or internal project streets are not subject to a maximum street setback.
 - (4) The maximum street setback for projects located along slip lanes may be measured from the back of sidewalk.
 - (5) The maximum street setback for properties within the Interstate Zone may be increased up to 65' for developments that include one full bay of parking along the street frontage.
 - (6) Projects with proper bulkhead techniques may request a zero (0') foot setback for pedestrian boardwalks and outdoor seating.
- (b) Additional design standards:
- (1) All auto oriented uses shall be designed so that buildings are oriented along the frontage with drive-thru and associated stacking located along the side and rear property lines. Fueling and

charging station pumps shall be oriented behind a building and not visible from the Bonita Beach Road frontage.

OK

(2) Lighting. Building mounted or free standing luminaires (bollards or light poles) shall be provided along frontage lines and/or street rights-of-way. All applicants must submit a photometric plan in accordance with section 3-269.

OK

(3) Parking requirements.

- a. All uses shall provide the minimum required parking as set forth in section 4-1732 except as modified herein. Maximum parking spaces are limited no more than 15 percent more than the minimum required. All uses requesting 16 percent or more spaces than required may only do so through approval of a special exception.
- b. The required spaces may be reduced by the city manager or designee if supported by a parking study submitted by the applicant.
- b. On-street parking along a corresponding frontage(s) of the site shall count 100 percent towards the parking requirements.
- d. Adjacent property owners are encouraged to share parking facilities in accordance with LDC Section 4-1730.
- e. An applicant may provide a valet parking program for commercial and mixed-use projects to offset a maximum of 50 percent of the required parking spaces. Valet parking programs may not be utilized to offset parking requirements for stand-alone residential development.
- f. Bicycle racks shall be provided in accordance with LDC Section 3-438. (Rack provision may be shared by different businesses within each block.). Parking ratios within urban areas may be reduced by using a 3 to 1 ratio (three bicycle parking stalls to one vehicular parking space); however this does not exempt meeting the American Disability Act (ADA) parking standards.

(4) Parking placement.

NA

- a. On-street parking is encouraged along all intersecting streets with the exception on Bonita Beach Road.
- b. Surface parking lots shall not be permitted on a frontage line or within 30 feet of the frontage line within the Commercial, Historic, and Beach Zone. One full bay of parking is permitted along frontage lines within the Interstate Zone. Setbacks for shared parking along side and rear property lines is zero feet.
- c. On-site parking within the Beach, Commercial, and Historic Zone will be restricted to the side or rear yards of those properties fronting Bonita Beach Road. In the case of side yard parking, the parking area shall be a minimum of five feet behind the front build-to line

and a street wall or opaque screen, shall be provided at the right-of-way line or build-to line, whichever is further removed from the roadway. Such street wall or opaque screen shall be no taller than four feet.

d. Buffering adjacent property.

- i. Required right-of-way buffer plantings may be planted in the street right-of-way or roadway easement subject to review and approval of the applicable city, county, state, or special district entities. The applicant shall enter into a maintenance agreement to properly maintain the required buffer.
- ii. If roads, drives, or parking areas are located less than 125 feet from an existing residential dwelling or residential lot, a solid fence, opaque landscape hedge, or combination berm and solid fence or opaque landscape hedge not less than six feet in height must be constructed along the property line. Fence, berms, and hedges must not be placed so they violate the vehicle visibility requirements of LDC.

NA (5) Continuous shaded bike facilities and pedestrian facilities.

- a. All development shall meet the standard set forth in chapter 3 of the Land Development Code
- b. Buffering adjacent property. Required right-of-way buffer plantings may be planted in the street right-of-way or roadway easement subject to review and approval of the applicable city, county, state, or special district entities. The applicant shall enter into a maintenance agreement to properly maintain the required buffer.
- c. When a covered walkway along a building frontage or right-of-way is not provided, tall or medium trees or palms are required to establish continuous shade.

(6) Streetscape amenities. Developments must incorporate sidewalks, benches, street furniture, planters, and other pedestrian amenities in their design. All accessories such as railings, trash receptacles, street furniture and bicycle racks must be complimentary to and consistent with the architectural design of principal building(s) within the development.

OK (7) Dumpsters. Dumpsters shall be screened from off-premises view and not located on frontage lines. Screening materials shall be compatible with the building design. Existing dumpsters that are visible from the right-of-way, adjoining property, or pedestrian walkway must be screened. Wherever possible, dumpsters may be shared between property owners. If a particular property by its use does not require a dumpster or when it can be demonstrated that a dumpster is not required, then the garbage receptacles or containers must be screened from view.

(c) Site specific regulations:

(1) Within the Bonita Beach Road Corridor there are three Gateways or Nodes that announce a place of arrival. They are located at:

- a. The intersection of Bonita Beach Road and Tamiami Trail.
- b. The intersection of Bonita Beach Road and Old U.S. Road 41.
- c. Bonita Beach Road between Imperial Parkway and Interstate 75.

NA (2) All development and redevelopment of these areas shall incorporate the following design standards:

- a. Use of internal and external block patterns.
- b. Building that address the adjacent roadway and embrace pedestrian and other non-automobile access.
- c. Provide for interconnectivity to adjacent parcels thru pedestrian ways and parking areas.
- d. Buildings fronting Bonita Beach Road, Tamiami Trail, Old U.S. Road 41 and Imperial Parkway are shall provide 70 percent glazing, consisting of window and glazed door openings. Upon request by the applicant, the city manager or designee may administratively approve an alternate design in accordance with LDC Section 3-488 that may be substituted in part for a plan that otherwise meets the standards of this section. The alternate plan is subject to architectural review by the city, and must demonstrate compliance with the stated intent of supporting an urbanized development pattern that focuses on human-scale development.

The following figures are provided as an illustrative guide, to assist in the site and architectural design of new development and redevelopment within the Bonita Beach Road Corridor Overlay.

Bonita Beach Road and Tamiami Trail (NODE)

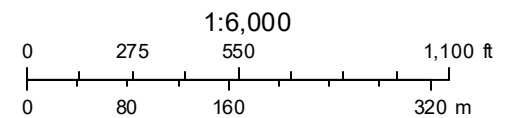
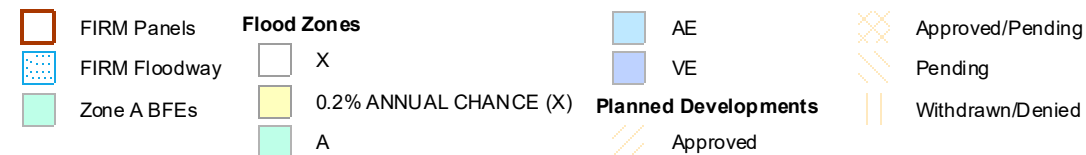
Bonita 75 MPD

Exhibit II-F-1 Special Flood Hazard



May 24, 2021

Air Photos: 2020 Hi-Res (4 inch)



National Flood Hazard Layer FIRMette



81°45'11"W 26°20'11"N



Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS	Without Base Flood Elevation (BFE) Zone A, V, A99	With BFE or Depth Zone AE, AO, AH, VE, AR	Regulatory Floodway

		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes. Zone X
		Area with Flood Risk due to Levee Zone D

OTHER AREAS OF FLOOD HAZARD	NO SCREEN	Area of Minimal Flood Hazard Zone X
		Area of Minimal Flood Hazard Zone X

		Effective LOMRs
		Area of Undetermined Flood Hazard Zone D

GENERAL STRUCTURES	Channel, Culvert, or Storm Sewer	Levee, Dike, or Floodwall

		20.2 Cross Sections with 1% Annual Chance Water Surface Elevation
		17.5 Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
		Jurisdiction Boundary
		Coastal Transect Baseline
		Profile Baseline
		Hydrographic Feature

		Digital Data Available
		No Digital Data Available
		Unmapped

The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on **5/24/2021 at 4:01 PM** and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.

consulted for possible updated or additional flood hazard information.

To obtain more detailed information in areas where **Base Flood Elevations** (BFEs) and/or **floodways** have been determined, users are encouraged to consult the Flood Profiles and Floodway Data and/or Summary of Stillwater Elevations tables contained within the Flood Insurance Study (FIS) report that accompanies this FIRM. Users should be aware that BFEs shown on the FIRM represent rounded whole-foot elevations. These BFEs are intended for flood insurance rating purposes only and should not be used as the sole source of flood elevation information. Accordingly, flood elevation data presented in the FIS report should be utilized in conjunction with the FIRM for purposes of construction and/or floodplain management.

Coastal Base Flood Elevations shown on this map apply only landward of 0.0' North American Vertical Datum of 1988 (NAVD 88). Users of this FIRM should be aware that coastal flood elevations are also provided in the Summary of Stillwater Elevations table in the Flood Insurance Study report for this jurisdiction. Elevations shown in the Summary of Stillwater Elevations table should be used for construction and/or floodplain management purposes when they are higher than the elevations shown on this FIRM.

Boundaries of the **floodways** were computed at cross sections and interpolated between cross sections. The floodways were based on hydraulic considerations with regard to requirements of the National Flood Insurance Program. Floodway widths and other pertinent floodway data are provided in the Flood Insurance Study report for this jurisdiction.

Certain areas not in Special Flood Hazard Areas may be protected by **flood control structures**. Refer to Section 2.4 "Flood Protection Measures" of the Flood Insurance Study report for information on flood control structures for this jurisdiction.

The **projection** used in the preparation of this map was Florida State Plane west zone (FIPSZONE 902). The **horizontal datum** was NAD 83, GRS80 spheroid. Differences in datum, spheroid, projection or State Plane zones used in the production of FIRMs for adjacent jurisdictions may result in slight positional differences in map features across jurisdiction boundaries. These differences do not affect the accuracy of this FIRM.

Flood elevations on this map are referenced to the North American Vertical Datum of 1988. These flood elevations must be compared to structure and ground elevations referenced to the same **vertical datum**. For information regarding conversion between the National Geodetic Vertical Datum of 1929 and the North American Vertical Datum of 1988, visit the National Geodetic Survey website at <http://www.ngs.noaa.gov> or contact the National Geodetic Survey at the following address:

NGS Information Services
NOAA, NNGS12
National Geodetic Survey
SSM-C-3, #9202
1315 East-West Highway
Silver Spring, Maryland 20910-3282
(301) 713-3242

To obtain current elevation, description, and/or location information for **bench marks** shown on this map, please contact the Information Services Branch of the National Geodetic Survey at (301) 713-3242, or visit its website at <http://www.ngs.noaa.gov>.

Base map information shown on this FIRM was provided in digital format by the Lee County GIS Department. The road centerline information was constructed based on orthophotography produced at a scale of 1"=100' from aerial imagery flown in 1998 and updated using orthophotography dated 2002 and 2005. The surface water features were also constructed based on orthophotography produced at a scale of 1"=100' from aerial imagery flown in 1998.

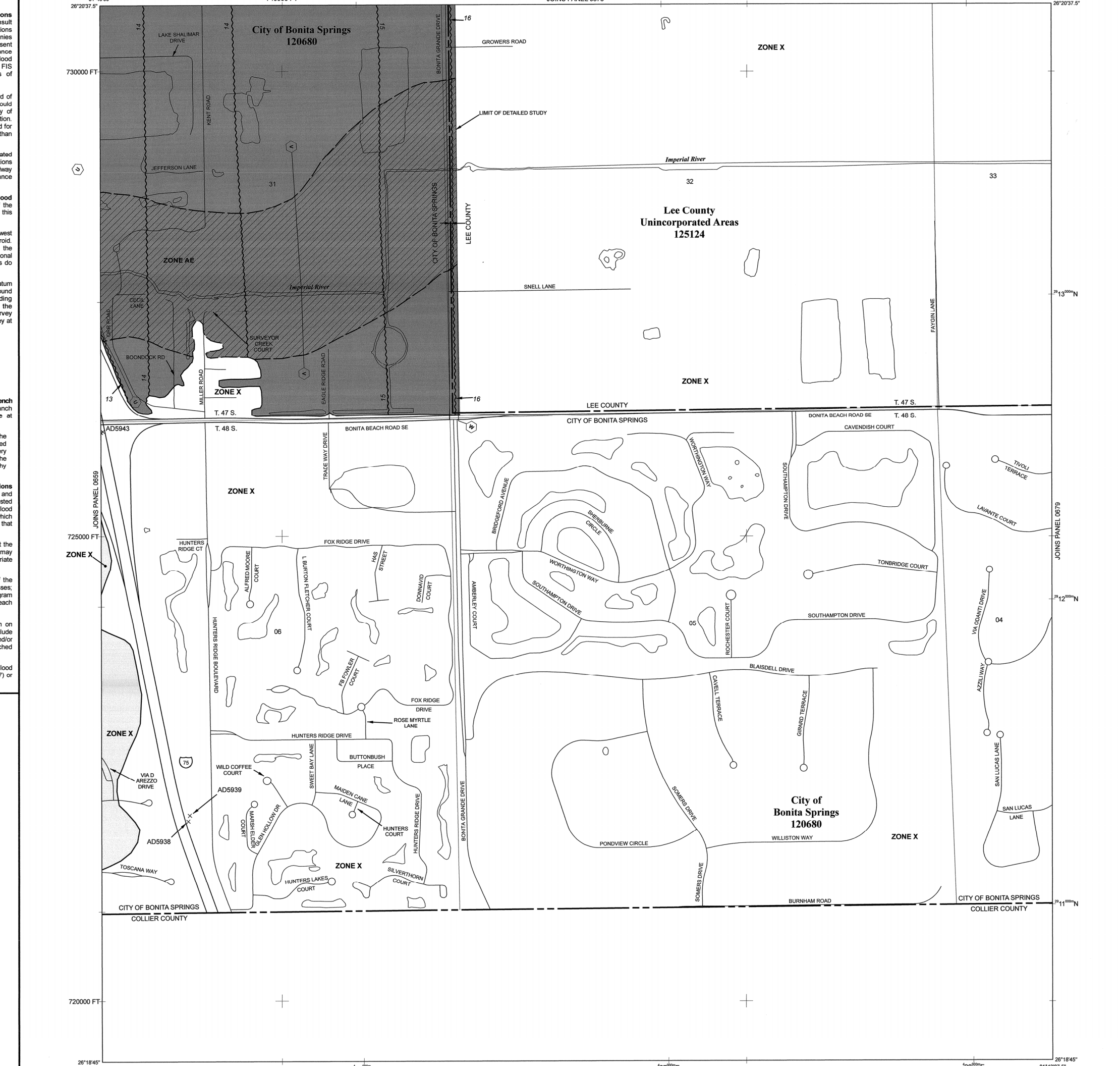
This map reflects more detailed and up-to-date **stream channel configurations** than those shown on the previous FIRM for this jurisdiction. The floodplains and floodways that were transferred from the previous FIRM may have been adjusted to conform to these new stream channel configurations. As a result, the Flood Profiles and Floodway Data tables in the Flood Insurance Study Report (which contains authoritative hydraulic data) may reflect stream channel distances that differ from what is shown on this map.

Corporate limits shown on this map are based on the best data available at the time of publication. Because changes due to annexations or de-annexations may have occurred after this map was published, map users should contact appropriate community officials to verify current corporate limit locations.

Please refer to the separately printed **Map Index** for an overview map of the county showing the layout of map panels; community map repository addresses; and a Listing of Communities table containing National Flood Insurance Program dates for each community as well as a listing of the panels on which each community is located.

Contact the **FEMA Map Service Center** at 1-800-358-9616 for information on available products associated with this FIRM. Available products may include previously issued Letters of Map Change, a Flood Insurance Study report, and/or digital versions of this map. The FEMA Map Service Center may also be reached by Fax at 1-800-358-9620 and its website at <http://www.msc.fema.gov>.

If you have **questions about this map** or questions concerning the National Flood Insurance Program in general, please call 1-877-FEMA MAP (1-877-336-2627) or visit the FEMA website at <http://www.fema.gov>.



The 1% annual flood (100-year flood), also known as the base flood, is the flood that has a 1% chance of being equaled or exceeded in any given year. The Special Flood Hazard Area is the area subject to flooding by the 1% annual chance flood. Areas of Special Flood Hazard include Zones A, AE, AH, AO, AR, A99, V, and VE. The Base Flood Elevation is the water-surface elevation of the 1% annual chance flood.

- ZONE A** No Base Flood Elevations determined.
- ZONE AE** Base Flood Elevations determined.
- ZONE AH** Flood depths of 1 to 3 feet (usually areas of ponding); Base Flood Elevations determined.
- ZONE AO** Flood depths of 1 to 3 feet (usually sheet flow on sloping terrain); average depths determined. For areas of alluvial fan flooding, velocities also determined.
- ZONE AR** Special Flood Hazard Area formerly protected from the 1% annual chance flood by a flood control system that was subsequently deactivated. Zone AR indicates that the former flood control system is being restored to provide protection from the 1% annual chance or greater flood.
- ZONE A99** Area to be protected from 1% annual chance flood by a Federal flood protection system under construction; no Base Flood Elevations determined.
- ZONE V** Coastal flood zone with velocity hazard (wave action); no Base Flood Elevations determined.
- ZONE VE** Coastal flood zone with velocity hazard (wave action); Base Flood Elevations determined.

FLOODWAY AREAS IN ZONE AE
The floodway is the channel of a stream plus any adjacent floodplain areas that must be kept free of encroachment so that the 1% annual chance flood can be carried without substantial increases in flood heights.

OTHER FLOOD AREAS
ZONE X Areas of 0.2% annual chance flood; areas of 1% annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance flood.

OTHER AREAS
ZONE X Areas determined to be outside the 0.2% annual chance floodplain.
ZONE D Areas in which flood hazards are undetermined, but possible.

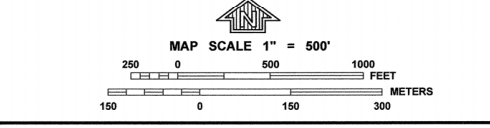
COASTAL BARRIER RESOURCES SYSTEM (CBRS) AREAS
OTHERWISE PROTECTED AREAS (OPAs)
CBRS areas and OPAs are normally located within or adjacent to Special Flood Hazard Areas.

- Floodplain boundary
- Floodway boundary
- Zone D boundary
- CBRS and OPA boundary
- Boundary dividing Special Flood Hazard Area zones and boundary dividing Special Flood Hazard Areas of different Base Flood Elevations, flood depths or flood velocities.
- Base Flood Elevation line and value; elevation in feet*
- Base Flood Elevation value where uniform within zone; elevation in feet*

- * Referenced to the North American Vertical Datum of 1988
- Cross section line
- Transect line
- 8°07'45", 32°22'30" Geographic coordinates referenced to the North American Datum of 1983 (NAD 83), Western Hemisphere
- 76°00'N 1000-meter Universal Transverse Mercator grid values, zone 17
- 600000 FT 5000-foot grid ticks: Florida State Plane coordinate system, West zone (FIPSZONE 0902), Transverse Mercator projection
- DX5510 X Bench mark (see explanation in Notes to Users section of this FIRM panel)
- 1.5 River Mile

MAP REPOSITORY
Refer to listing of Map Repositories on Map Index
EFFECTIVE DATE OF COUNTYWIDE FLOOD INSURANCE RATE MAP
August 28, 2008
EFFECTIVE DATE(S) OF REVISION(S) TO THIS PANEL

For community map revision history prior to countywide mapping, refer to the Community Map History table located in the Flood Insurance Study report for this jurisdiction.
To determine if flood insurance is available in this community, contact your insurance agent or call the National Flood Insurance Program at 1-800-638-6620.



NFIP
PANEL 0678F
FIRM
FLOOD INSURANCE RATE MAP
LEE COUNTY, FLORIDA AND INCORPORATED AREAS
PANEL 678 OF 685
(SEE MAP INDEX FOR FIRM PANEL LAYOUT)
CONTAINS:

COMMUNITY	NUMBER	PANEL	SUFFIX
BONITA SPRINGS, CITY OF	120680	0678	F
LEE COUNTY	125124	0678	F

Notice to User: The Map Number shown below should be used when placing map orders; the Community Number shown above should be used on insurance applications for the subject community.

Development of this CPD/RPD will comply with the following Property Development Regulations:

RPD - Residential Tract:

Minimum Area Dimension:

Minimum Lot Area: N.A.

Minimum Lot Dept: N.A.

Minimum Lot Width: N.A.

Minimum Building Setbacks:

Street/Front (Public Road): 25 feet

Street/Front (Private Ingress/Egress Easement): 20 feet

Internal Driveway: 10 feet

Side Yard: 10 feet

Rear Yard: 10 feet

Northern RPD Boundary: 20 feet

Waterbody:

Principal: 25 feet

Accessory: 0 feet

Preserve: 30 feet

Maximum Building Height: 60 feet (as measured from first finished floor to building eaves), not to exceed 4-stories

Minimum Open Space: 40%

CPD - Commercial Tract:

Minimum Area Dimension:

Minimum Lot Area: 10,000 sq. ft.

Minimum Lot Dept: 100 feet

Minimum Lot Width: 100 feet

Minimum Building Setbacks:

Street/Front (Public Road): 25 feet

Street (Private Road) 10 feet

Drive Aisle: 10 feet

Side Yard: 15 feet

Rear Yard: 25 feet

Maximum Building Height: 35 feet, not to exceed 2-stories

Minimum Open Space: 10%

RPD (not to exceed 252 units): Minimum 40% open space - 6.72± acres

Accessory uses and structures
Administrative offices (leasing office)
Dwelling units, multiple family
Essential service facilities: Group I
Excavation, water retention
Fences, walls
Garage, private
Gatehouse(s)
Recreational facilities, private on-site
Signage
Storage, indoor

CPD (not to exceed 10,000 square feet): Minimum 20% open space – 0.28± acres

Accessory uses and structures
Administrative offices
ATM (automatic teller machine)
Auto parts store
Banks and financial establishments: Group I
Boats parts store
Business services: Group I and II
Clubs, private
Consumption on Premises, indoor only, in conjunction with a full-service restaurant or micro brewery. Outdoor consumption requires a special exception.
Contractors and builders: Group I and II
Daycare centers, child, adult
Drug store, pharmacy
Dwelling unit (ancillary only); Multiple-family building (not to exceed 252 units for the entire PD)
Entrance gates and gatehouse
Essential service facilities: Group I
Excavation: Water retention
Fences, walls
Food stores, Group I
Hardware store - no outdoor storage or display
Healthcare facilities: Group I and II
Household and office furnishing: Group I

Laundry or dry cleaning: Group I
Lawn and Garden Supply Stores, no outdoor storage or display
Micro-breweries
Office – Business
Paint, glass and wallpaper - no outdoor storage or display
Parking lot: Accessory and garage, private
Personal Services, Group I, II and III
Pet service
Rental or leasing establishment: Group I
Residential accessory uses
Restaurant, fast food
Restaurants: Groups I, II, III and IV
Specialty retail: Groups I, II and III
Storage: indoor only
Studios
Temporary uses
Used merchandise stores: Group I
Variety store
Wholesale establishments: Group III

1. Deviation 1 seek relief from LDC Sec. 3-331(d)(1) which provides that no excavations will be allowed within fifty feet of any private property line under separate ownership unless granted an administrative deviation in accordance with section 3-81. The setback for an excavation from a private property line may not be less than 25 feet to allow for 15 ft.

Justification:

The property on the north side of the lake is zoned AG-2 and requires only a Type “B” buffer. In an effort to accommodate the land use to the north, the lake area will function as a buffer to the commercial to the south. The excavation will likely be necessary for floodplain compensation per the anticipated SFWMD consistent with the previously issued permit. Necessary security, such as a fence, will be provided at the discretion of the City Manager or his designee.

2. Deviation 2 seeks relief from LDC Sec. 4-899, site design standards, to allow development within the CPD/RPD to be consistent with the Development Standards proposed for this CPD/RPD.

Justification:

The project physically cannot comply with the maximum building setback and other site design standards in LDC Sec. 4-899 due to the presence of Orr Road. The entrance to the CPD/RPD is at the intersection of Orr Road and Miller Road, which is approximately 85 feet north of the Bonita Beach Road travel lanes which exceeds the maximum setback of 65 feet permitted in the Bonita Beach Road Interstate Zone. The project will also not be developed with internal streets as the residential component will be a gated rental apartment complex. The proposed development standards will permit development of the proposed uses in a manner necessary for this type of mixed-use development and the deviation will have no negative impact on the public.

3. Deviation 3 seeks relief from LDC Sec. 6-112 and 6-113, permanent signs in residential and commercial areas, to allow for up to two shared entrance signs/project signs for the residential and commercial tracts. The signs may be pole or monument signs and if developed as a pole sign the pole must have a pole cover no less than 50% of the width of the sign. the shared sign(s) may be a maximum of 15 feet in height as measured from the finished grade and the maximum area shall not exceed 240 square feet. The sign must be setback a minimum of 5 feet from Orr Road or Miller Road.

Justification:

The Mixed Use Planned Development will have limited visibility due to the physical location of the project which is separated from Bonita Beach Road by Orr Road and the northbound entrance ramp to I-75. The physical separation from the nearest travel lanes on Bonita Beach Road warrant signage that is greater in height than currently allowed for projects located in the Bonita Beach Road Corridor Overlay. Sharing the project signage for the residential and

commercial components will help reduce overall signage and given the small view corridors to the site, sharing the signage at strategic locations makes logical sense.

4. Deviation 4 seeks relief from LDC Sec. 3-418(d)(4), which requires a 15-foot-wide Type 'D' landscape buffer adjacent to a ROW to instead allow a 10-foot-wide Type 'D' buffer adjacent to the Orr Road ROW and ingress/egress/utility easement area.

Justification:

At the request of staff, the Orr Road alignment was revised to accommodate a better intersection spacing from Bonita Beach Road and to alleviate stacking concerns on Miller and Orr Roads. Based on the original easement area for Orr Road and the existing pavement area, approximately 10' in width is available for planting. The applicant is agreeable to plant additional plant material in the reduced buffer width if requested by staff.

5. Deviation 5 seeks relief from LDC Sec. 3-303, Complete Streets Design, for that portion of Orr Road located within the boundary of the CPD/RPD. That portion of Orr Road located between the RPD and CPD areas shall be a 40 foot wide cross-section, with closed drainage and six foot wide sidewalks on each side of the roadway as depicted in the Orr Road Cross-Section, Exhibit A. The remainder of Orr Road shall remain in its current design with open drainage and no sidewalks.

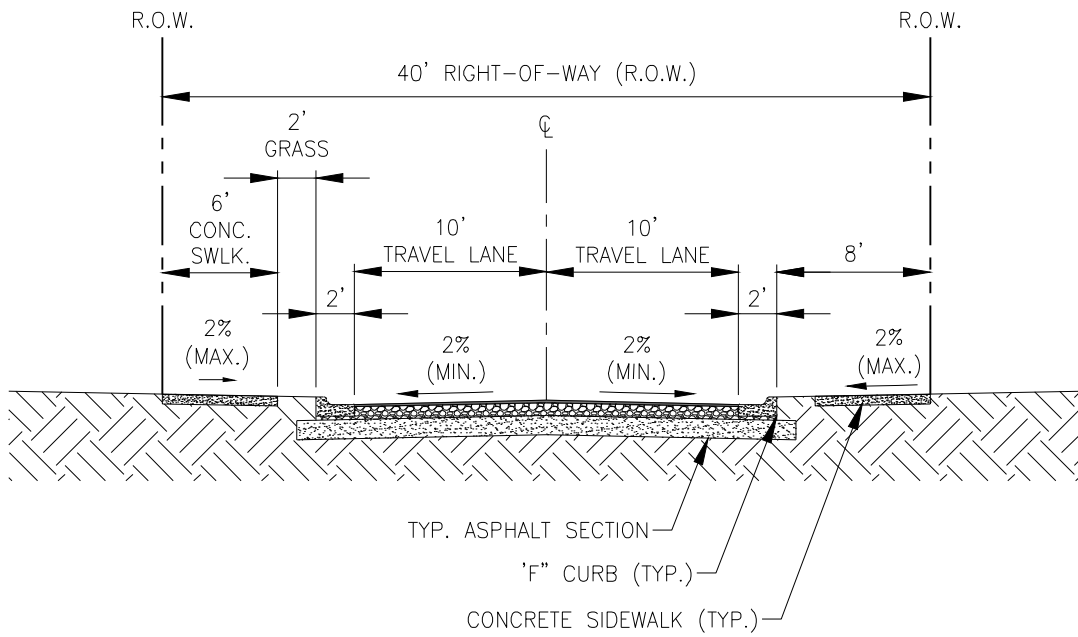
Justification:

The property owner has agreed to re-locate Orr Road in order to increase the intersection spacing from Bonita Beach Road, while also reducing a significant amount of commercial square footage, thereby reducing vehicular traffic associated with the CPD/RPD. Orr Road beyond the CPD tract serves three existing home sites, retention ponds owned by FDOT and vacant lands owned by the State of Florida. The existing Orr Road is in good physical condition and for the very low volume of traffic utilizing this portion of Orr Road it does not make economic sense to modify the existing cross-section. Further, the RPD/CPD will have access from Miller Road. Only an emergency vehicle access will be provided on the older section of Orr Road.

6. Deviation 6 seeks relief from LDC Sec. 4-1732(1)c Required Parking for Multiple-family buildings, which requires parking at a ratio of 1.5 spaces per unit for each 1BR unit, 1.75 spaces per unit for each 2BR unit and 2 spaces per unit for a 3BR unit, with an additional 10% of the total required units to be guest parking, to instead allow a parking ratio of 1.8 spaces per unit as the required parking for the project.

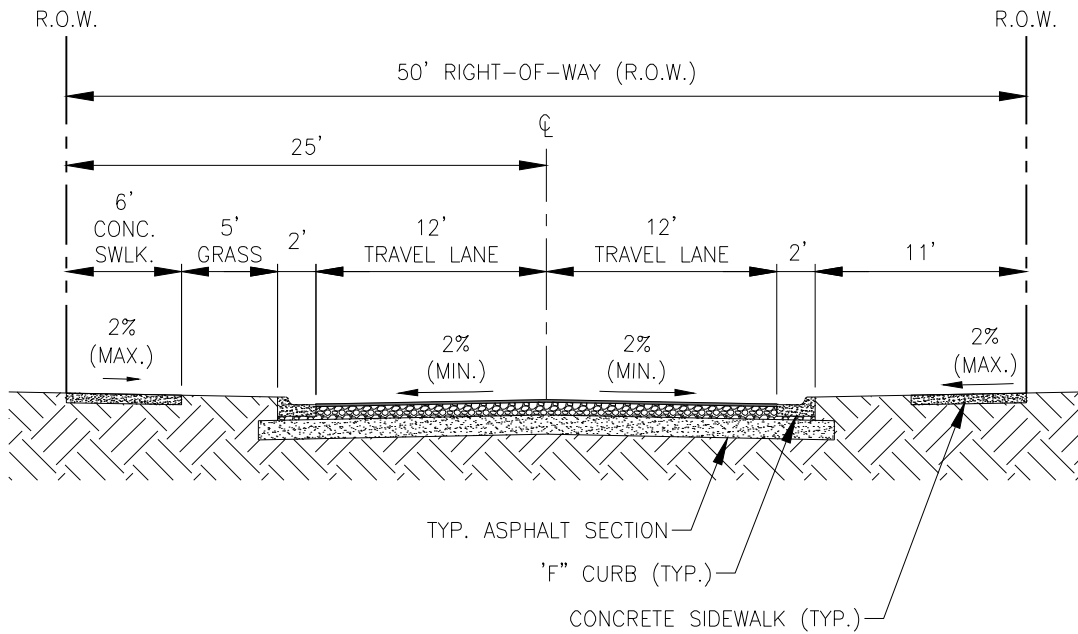
Justification:

Madison Capital Group has developed a number of rental communities throughout the United States and has successfully developed communities having an average parking ratio of 1.8 spaces per unit, with no additional parking for guest spaces. Typical rental communities developed by Madison Capital Group have approximately 50% of the units as 1BR, 40% as 2BR and 10% as 3BR units. The 1.8 parking space per unit average has been utilized in the following three communities in Florida: Tampa, Apopka and Wesley Chapel.



TYPICAL 40' ROW SECTION (INTERNAL TO PROJECT - COMMERCIAL EAST END)

N.T.S.



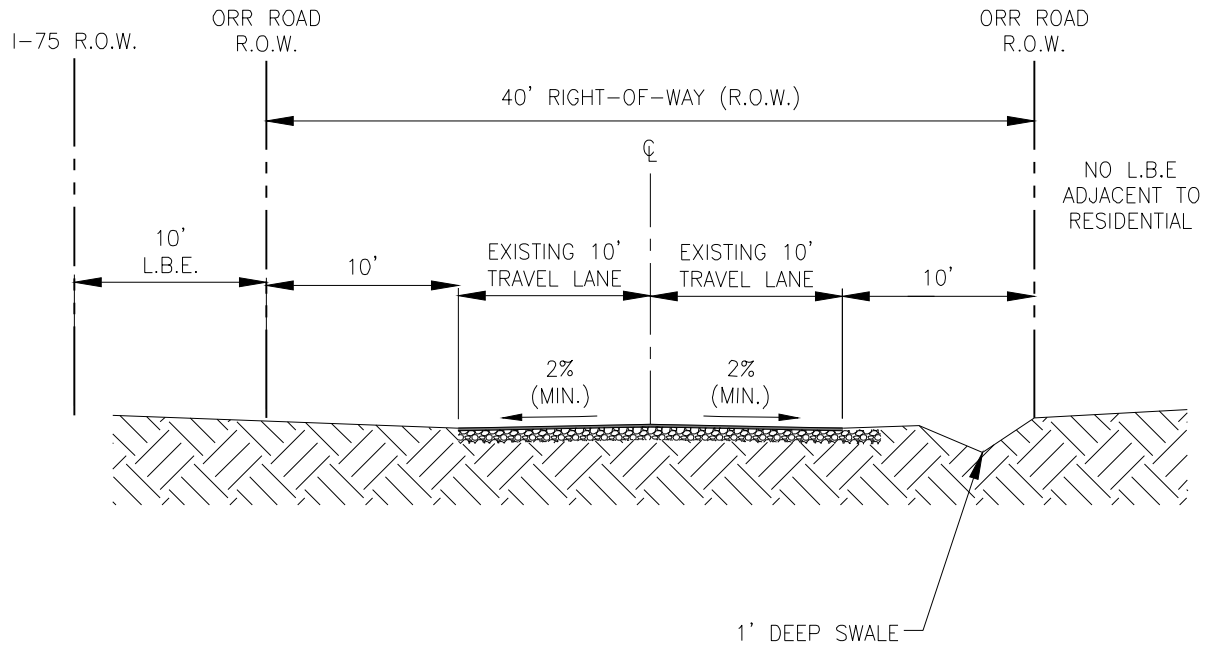
TYPICAL 50' ROW SECTION (MILLER ROAD)

N.T.S.

ROAD CROSS SECTION EXHIBIT (PAGE 1 OF 2)



Civil Engineers • Land Surveyors
Planners • Landscape Architects
www.GradyMinor.com



TYPICAL 40' ROW SECTION (ORR ROAD AT I-75)
N.T.S.





Bonita Springs, Florida
Image capture: Dec 2007 © 2022 Google

Google

Street View - Dec 2007

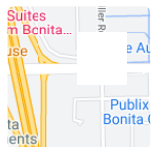




Image capture: Feb 2022 © 2022 Google

Bonita Springs, Florida

Google

Street View - Feb 2022

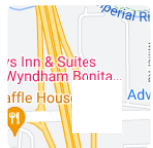




Image capture: Feb 2022 © 2022 Google

Google

Street View - Feb 2022



TRAFFIC IMPACT STATEMENT

FOR

BONITA 75 CPDA

PROJECT NO. F2105.23

**PREPARED BY:
TR Transportation Consultants, Inc.
2726 Oak Ridge Court, Suite 503
Fort Myers, Florida 33901
239-278-3090**

**Revised:
December 2, 2021**

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- IV. TRIP GENERATION
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- VII. PROJECTED LEVEL OF SERVICE AND IMPROVEMENTS
- VIII. CONCLUSION

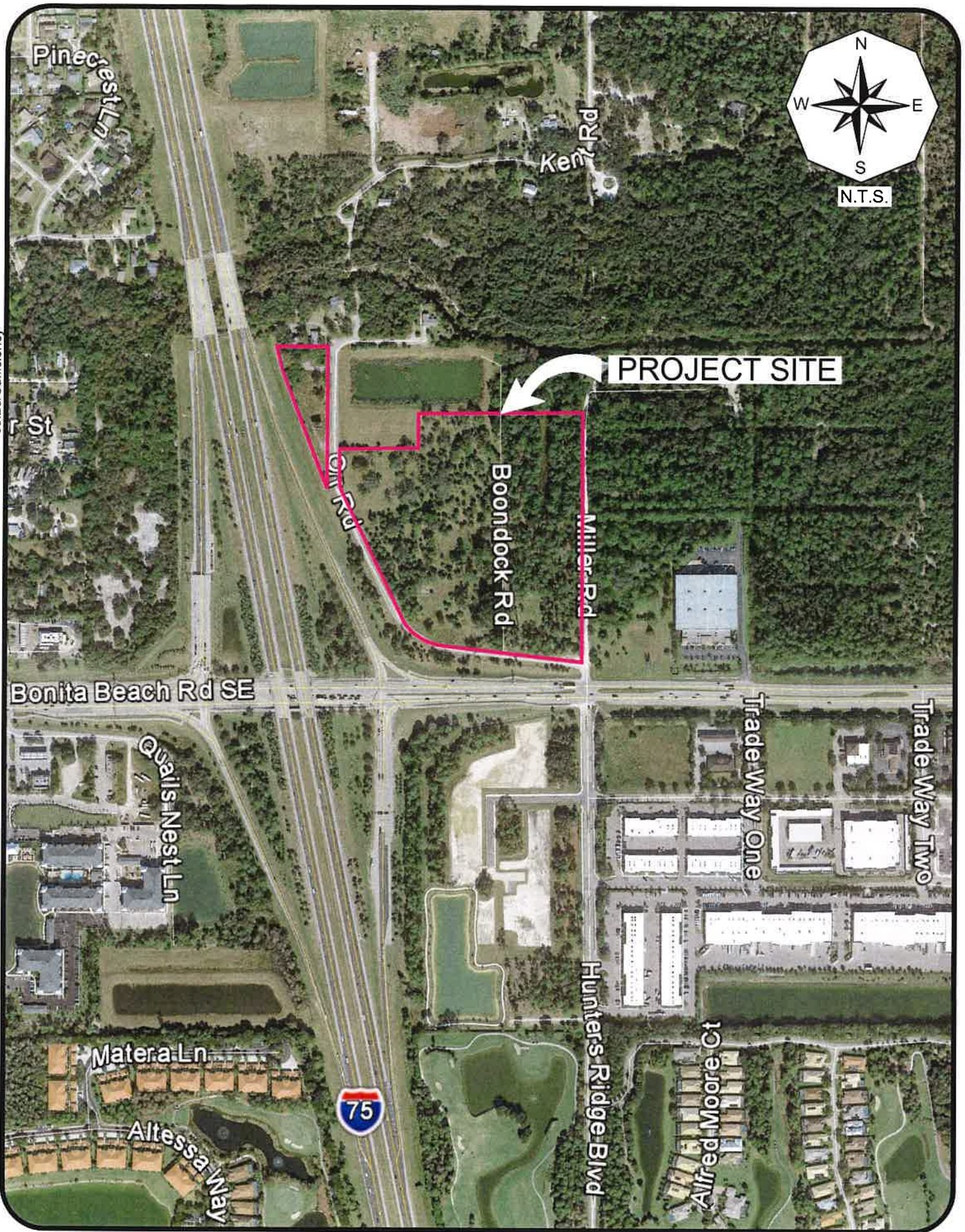
I. INTRODUCTION

TR Transportation Consultants, Inc. has conducted a traffic impact statement to fulfill requirements set forth by the City of Bonita Springs for projects seeking approval for zoning. This report has been completed in compliance with guidelines established in the City of Bonita Springs Traffic Impact Statement Guidelines. The development site is located at the northeast corner of Bonita Beach Road and I-75 in the City of Bonita Springs, Florida. The site location is illustrated on **Figure 1**.

Currently the site is governed by Zoning Ordinance No. 08-10 and ADD15-23355, which permits the site to be developed up to 88,000 square feet of retail uses, 58,800 square feet of mini-warehouse uses, a gas station with up to 12 refueling positions, a 5,000 square foot bank with a drive-through and up to 124 hotel rooms. The applicant is proposing to rezone the subject site to permit a development of up to 252 multi-family residential dwelling units and up to 10,000 square feet of retail uses. Access to the subject site will be provided to Orr Road and Miller Road via multiple connections.

Methodology meeting notes were previously exchanged with the City of Bonita Springs Staff to discuss the requirements for the rezoning traffic study. The initial methodology meeting notes are attached to this Memorandum for reference. This report examines the impact of the development on the surrounding roadways. Trip generation and assignments to the various surrounding intersections will be completed and analysis conducted to determine the impacts of the development on the surrounding streets and intersections.

F2105.23/Sufficiency



II. EXISTING CONDITIONS

The subject site is currently vacant. The subject site is generally bordered by Miller Road to the east, vacant land and a pond to the north, and Orr Road to the south and west.

Miller Road & Orr Road are both two-lane undivided local roadways that border the subject site. Per Lee County's *FindMyRoad* GIS webpage, both roadways have a posted speed limit of 25 mph and are under the jurisdiction of the City of Bonita Springs.

Bonita Beach Road is a four lane divided arterial within the vicinity of the subject site. Bonita Beach Road has a posted speed limit of 45 mph and is under the jurisdiction of the Lee County Department of Transportation.

III. PROPOSED DEVELOPMENT

The site is currently governed by Zoning Ordinance No. 08-10 and ADD15-23355, which permits the site to be developed up to 88,000 square feet of retail uses, 58,800 square feet of mini-warehouse uses, a gas station with up to 12 refueling positions, a 5,000 square foot bank with a drive-through and up to 124 hotel rooms. The applicant is proposing to rezone the subject site to permit a development of up to 252 multi-family residential dwelling units and up to 10,000 square feet of retail uses. **Table 1** summarizes the uses/intensities that would be permitted under the currently approved zoning and the uses/intensities that are requested as part of proposed rezoning request. Also shown in Table 1 is the difference in the intensity for each use between the two scenarios.

**Table 1
Land Uses
Bonita 75 CPDA**

Land Use	Total Approved	Total Proposed	Difference
Retail LUC 821/LUC 822	88,000 sq. ft.	10,000 sq. ft.	-78,000 sq. ft.
Multifamily Housing LUC 220	-	252 Units	+252 Units
Mini-Warehouse LUC 151	58,800 sq. ft.	-	-58,800 sq. ft.
Gas Station LUC 945	12 Refueling Positions	-	-12 Refueling positions
Bank W/ Drive-Through LUC 912	5,000 sq. ft.	-	-5,000 sq. ft.
Hotel LUC 310	124 Rooms	-	-124 Rooms

Access to the subject site will be provided to Orr Road and Miller Road via multiple connections.

IV. TRIP GENERATION

The trip generation for the approved and proposed development was determined by referencing the Institute of Transportation Engineer’s (ITE) report, titled *Trip Generation Manual*, 11th Edition. Land Use Code 821 (Shopping Plaza 40-150k), Land Use Code 822 (Strip Retail Plaza <40k), Land Use Code 220 (Multifamily Housing Low-Rise), Land Use Code 151 (Mini-Warehouse), Land Use Code 945 (Convenience Market/Gas Station), Land Use Code 912 (Drive-in Bank) and Land Use Code 310 (Hotel) were utilized for the trip generation purposes of the approved and proposed zoning request. It is important to note utilizing LUC 220 (Multifamily Housing Low-Rise) results in the most conservative analysis as it generates more weekday peak hour trips than the other land uses under the ITE’s multi-family housing categories (LUC 221 – Multifamily Housing Mid-Rise & LUC 222 (Multifamily Housing High-Rise). The equations utilized from these land uses are contained in the Appendix of this report for reference.

Table 2 and Table 3 reflect the weekday AM and PM peak hour trip generation that is permitted under the currently approved Bonita 75 CPD. Table 3 also summarizes the breakdown between the new trips the development is anticipated to generate and the “pass-by” trips the development is anticipated to attract. The “pass-by” reduction percentages were consistent with the latest ITE’s *Trip Generation Handbook*, 3rd Edition.

**Table 2
Trip Generation – Total Trips
Bonita 75 CPD – Approved**

Land Use	Weekday A.M. Peak Hour			Weekday P.M. Peak Hour			Daily (2-way)
	In	Out	Total	In	Out	Total	
Shopping Plaza (88,000 Sq. Ft.)	193	118	311	381	413	794	8,815
Mini-Warehouse (58,800 Sq. Ft.)	3	2	5	4	5	9	85
Convenience Market/Gas Station (12 Refueling Positions)	162	162	324	136	137	273	3,086
Drive-in Bank (5,000 Sq. Ft.)	29	21	50	52	53	105	502
Hotel (124 Rooms)	31	24	55	33	31	64	921
Total Trips	418	327	745	606	639	1,245	13,409

**Table 3
Trip Generation – New Trips
Bonita 75 CPD – Approved**

Land Use	Weekday A.M. Peak Hour			Weekday P.M. Peak Hour			Daily (2-way)
	In	Out	Total	In	Out	Total	
Total Trips	418	327	745	606	639	1,245	13,409
Less Retail Pass-by (30% AM/PM)	-47	-47	-94	-119	-119	-238	-2,645
Less Gas Station Pass-by (76% AM/ 75% PM)	-123	-123	-246	-102	-102	-204	-2,345
Less Bank Pass-by (29% AM/ 35% PM)	-7	-7	-14	-18	-18	-36	-176
New Trips	241	150	391	367	400	767	8,243

*Consistent with Lee County guidelines, the pass-by rate for retail uses was limited to a maximum of 30%.

Table 4 and Table 5 reflect the weekday AM and PM peak hour trip generation under the proposed zoning request. Table 5 also summarizes the breakdown between the new trips the development is anticipated to generate and the “pass-by” trips the development is anticipated to attract.

**Table 4
Trip Generation – Total Trips
Bonita 75 MPD – Proposed**

Land Use	Weekday A.M. Peak Hour			Weekday P.M. Peak Hour			Daily (2-way)
	In	Out	Total	In	Out	Total	
Multifamily Housing (252 Dwelling Units)	24	77	101	81	48	129	1,691
Strip Retail Plaza (10,000 Sq. Ft.)	17	12	29	39	39	78	652
Total Trips	41	89	130	120	87	207	2,343

**Table 5
Trip Generation – New Trips
Bonita 75 MPD – Proposed**

Land Use	Weekday A.M. Peak Hour			Weekday P.M. Peak Hour			Daily (2-way)
	In	Out	Total	In	Out	Total	
Total Trips	41	89	130	120	87	207	2,343
Less 30% Retail Pass-by	-4	-4	-8	-12	-12	-24	-196
New Trips	37	85	122	108	75	183	2,147

*Consistent with Lee County guidelines, the pass-by rate for retail uses was limited to a maximum of 30%.

Table 6 indicates the trip generation difference between the approved CPD and proposed MPD (Table 3 vs Table 5).

**Table 6
Trip Generation – Resultant Trip Change
Bonita 75 CPDA**

Land Use	A.M. Peak Hour			P.M. Peak Hour			Daily (2-way)
	In	Out	Total	In	Out	Total	
Proposed MPD	37	85	122	108	75	183	2,147
Approved CPD	-241	-150	-391	-367	-400	-767	-8,243
Resultant Trip Change	-204	-65	-269	-259	-325	-584	-6,096

Note: A positive (+) number indicates an increase in trips and a negative number (-) indicates a decrease in trips.

As can be seen in Table 6, the trip generation will be decreased in the AM peak hour by approximately 69% and decreased in the PM peak hour by approximately 76% as a result of the proposed CPD Amendment. Table 6 also shows that the overall daily trip generation will be decreased by approximately 74%. As can be seen from Table 6 the proposed rezoning request will be a significant “down zoning” from what is currently permitted.

V. TRIP DISTRIBUTION

The net new trips generated by the proposed zoning request which are shown in Table 5, were then assigned to the surrounding roadway network. The new trips anticipated to be added to the surrounding roadway network were assigned based upon the routes drivers are anticipated to utilize to approach the subject site. Attached Figure A-1 illustrates the percent project traffic distribution and assignment of the net new project trips. Attached Figure A-2 illustrates the percent project traffic distribution and assignment of pass-by trips. Attached **Figure 2** illustrates the resulting assignment of all project related trips (net new + pass-by).

The City Staff also indicated that the existing full median opening at Bonita Beach Road intersection with Miller Road/Hunters Ridge Boulevard may be closed in the future. Further evaluations regarding the closure of this median opening will be conducted at the time the decision to close the opening is made by the City or Lee County at this location. At this time there is no funding in the County’s Capital Improvement Program regarding this improvement. Therefore, the closure of this median opening cannot be assumed at this time. However, Figure 2A of the Appendix was provided for information purposes only to reflect the reassignment of project trips should the intersection of Miller Road/Hunters Ridge Boulevard with Bonita Beach Road be restricted to a right-in/right-out only turning movements.

In order to determine which roadway segments surrounding the site may be significantly impacted as outlined in the City of Bonita Springs Traffic Impact Statement Guidelines, Table 1A, in the Appendix, was created. This table indicates which roadway links will exceed 2% or 3% of the directional peak hour capacity of the Level of Service Standard.

The Level of Service Thresholds for Bonita Beach Road were obtained from the *Lee County Generalized Peak Hour Directional Service Volume* tables, dated April, 2016. The Level of Service Thresholds for I-75 were obtained from the FDOT's *Generalized Peak Hour Directional Service Volumes, Table 7*. Based on Table 1A, the proposed development is anticipated to significantly impact Bonita Beach Road between I-75 and Miller Road/Hunters Ridge Boulevard.

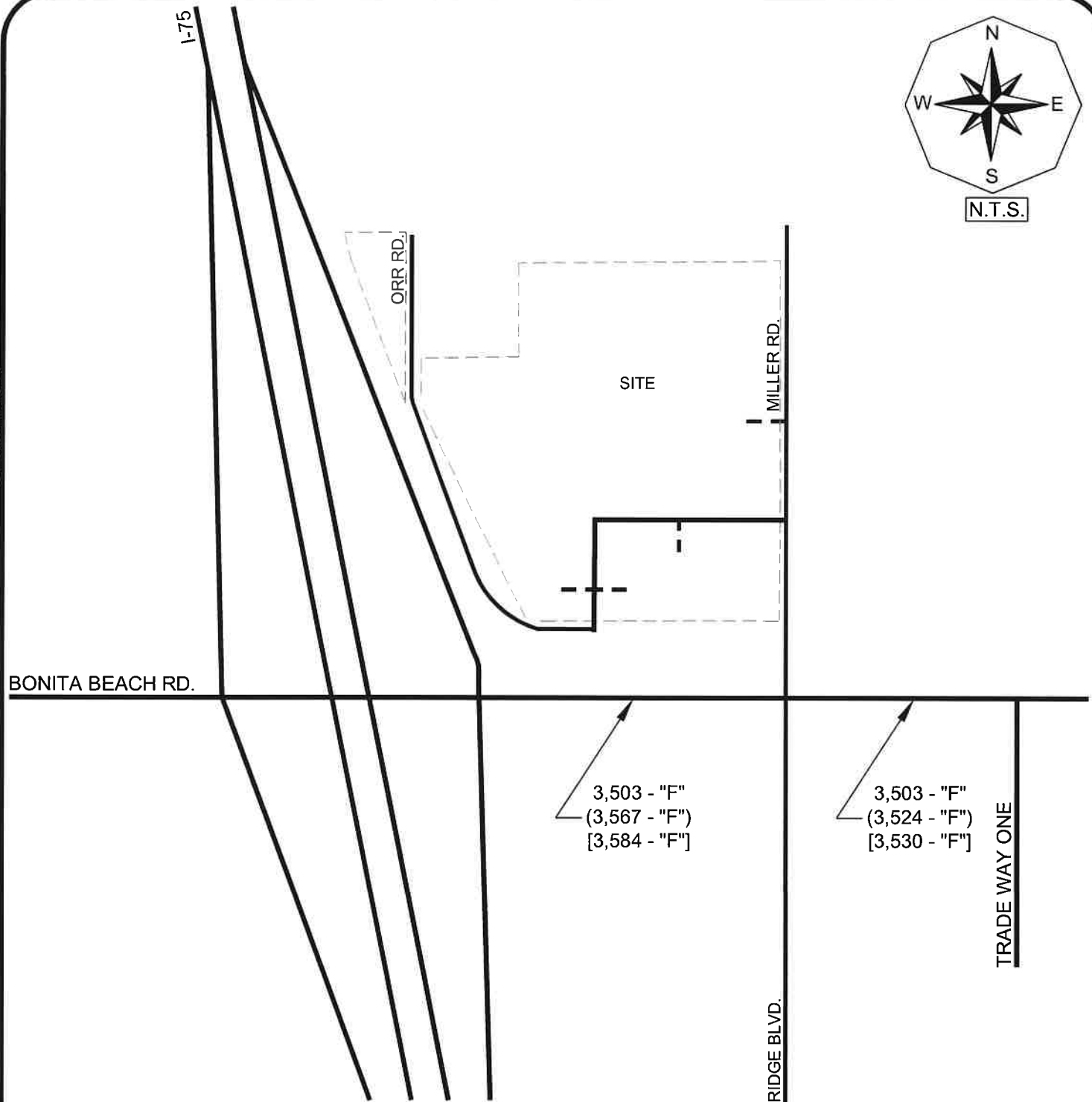
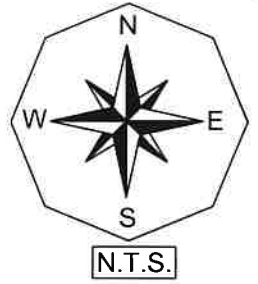
VI. FUTURE TRAFFIC CONDITIONS

A horizon year analysis of 2026 was selected as the analysis year to evaluate the future impacts this project will have on the surrounding roadway network. Based on this horizon year, a growth rate was applied to the existing traffic conditions for all roadway links that could be significantly impacted by this development. For Bonita Beach Road, a growth rate was not applied since all of the background growth on this roadway will be due to the vested traffic provided by the City. The existing roadway link traffic data was obtained from the 2021 *City of Bonita Springs Traffic Count Report*. Based on the project distribution shown in Table 1A, the link data was analyzed for the year 2026 without the development and year 2026 with the development.

Figure 3 indicates the year 2026 peak hour – peak direction traffic volumes and Level of Service for the various roadway links within the study area. Noted on Figure 3 is the peak hour – peak direction volume and Level of Service of each link should no development occur on the subject site and the peak hour – peak direction volume and Level of Service for the weekday A.M. and P.M. peak hour with the development traffic added to the roadways. This figure was derived from **Table 2A** contained in the Appendix. Note, the peak hour - peak direction vested trips were determined by utilizing the vested traffic information provided by the City of Bonita Springs and applying the K100 and D factors obtained from the 2021 *City of Bonita Springs Traffic Count Report*. The vested trips on Bonita Beach Road as determined by the City of Bonita Springs is outlined in the “Bonita Beach Traffic by Link” and is attached to the Appendix of this report for reference.

VII. PROJECTED LEVEL OF SERVICE AND IMPROVEMENTS

In comparing the links’ functional classification and calculated 2026 traffic volumes to the Service Volume Tables, it was determined that the proposed development will not cause any roadway link to fall below the minimum acceptable Level of Service standards. Bonita Beach Road within the vicinity of the site was shown to operate at a poor Level of Service in the 2026 background (without the proposed development) traffic conditions. As previously mentioned, the proposed development is not projected to cause any roadway segments within the study area to operate at a poor Level of Service. Figure 3 and attached Table 2A summarize the results of the Level of Service Analysis.



LEGEND

XXX - "X" 2026 PEAK SEASON PEAK HOUR PEAK DIRECTION
BACKGROUND TRAFFIC AND LEVEL OF SERVICE
DESIGNATION

(XXX -"X") 2026 PEAK SEASON PEAK HOUR PEAK DIRECTION
BACKGROUND TRAFFIC PLUS AM PEAK PROJECT TRAFFIC
AND LEVEL OF SERVICE DESIGNATION

[XXX -"X"] 2026 PEAK SEASON PEAK HOUR PEAK DIRECTION
BACKGROUND TRAFFIC PLUS PM PEAK PROJECT TRAFFIC
AND LEVEL OF SERVICE DESIGNATION

Intersection analysis was conducted utilizing the latest version of the *SYNCHRO*® to determine the operational characteristics of the signalized intersections of Bonita Beach Road and I75 ramps as well as the unsignalized intersections of Bonita Beach Road with Miller Road/Hunters Ridge Boulevard and Trade Way One. The intersection analysis was based on the projected 2026 weekday AM and PM peak hour traffic conditions with the project traffic added to the intersection. A copy of the raw turning movement count data conducted at these intersections is included in the Appendix of this report for reference.

The peak hour turning movements were then adjusted for peak season conditions based on peak season factor data as provided by FDOT in their *Traffic Information Online* resource. The FDOT peak season correction factor is included in the Appendix of this report for reference. The peak hour peak season traffic volumes were then increased by a growth rate factor to determine the projected 2026 background turning movement volumes. **Table 3A** of the Appendix illustrates the methodology utilized to formulate the appropriate annual growth rates for each roadway segment. The turning volumes projected to be added to the intersection as illustrated on Figure 2 were then added to the 2026 background volumes to estimate the future 2026 traffic volumes with the project. These volumes are based on the data from the spreadsheet contained in the Appendix of this report titled *Development of Future Year Background Turning Volumes*.

The *SYNCHRO*® summary sheets, attached to this report for reference, indicate that all of the major approaches at unsignalized intersections of Bonita Beach Road with Hunters Ridge Boulevard and Trade Way One will operate at acceptable LOS in 2026 in the background (without vested trips) plus project traffic conditions. The results of the analysis also indicate that the signalized intersections of Bonita Beach Road with I-75 ramps will operate at acceptable LOS in 2026 in the background (without vested trips) plus project traffic conditions. In the background plus vested trips plus project traffic analysis scenario, all of the major approaches and signalized intersections were shown to operate at a poor LOS in 2026 in the weekday A.M. and P.M. peak hour traffic conditions. It is the result of the vested traffic that causes the intersections on Bonita Beach Road to operate at a poor Level of Service. It is not a responsibility of this

development to mitigate the impacts of other “vested” projects. This project will be responsible for the payment of the road impact fees, which are used to mitigate off-site roadway impacts. Therefore, no intersection improvements will be warranted based on the intersection analysis conducted as part of this report.

Turn lane improvements at the site access drives intersections will be evaluated at the time the project seeks a Local Development Order approval.

VIII. CONCLUSION

The proposed rezoning request will be a significant “down zoning” from what is currently permitted on the property located at the northeast corner of Bonita Beach Road and I-75 in the City of Bonita Springs, Florida. Based on the results of the Level of Service analysis, it was determined that the proposed development will not cause any roadway link to fall below the minimum acceptable Level of Service standards. Bonita Beach Road within the vicinity of the site was shown to operate at a poor Level of Service in the 2026 background (without the proposed development) traffic conditions. Therefore, the proposed development is not projected to cause any roadway segments within the study area to operate at a poor Level of Service.

The results of the intersection analysis indicate that all of the major approaches at the unsignalized intersections of Bonita Beach Road with Hunters Ridge Boulevard and Trade Way One will operate at acceptable LOS in 2026 in the background (without vested trips) plus project traffic conditions. The results of the analysis also indicate that the signalized intersections of Bonita Beach Road with I-75 ramps will operate at acceptable LOS in 2026 in the background (without vested trips) plus project traffic conditions. In the background plus vested trips plus project traffic analysis scenario, all of the major approaches and signalized intersections were shown to operate at a poor LOS in 2026 in the weekday A.M. and P.M. peak hour traffic conditions. It is the result of the vested traffic that causes the intersections on Bonita Beach Road to operate at a poor Level of Service. It is not a responsibility of this development to mitigate the impacts of

other “vested” projects. This project will be responsible for the payment of the road impact fees, which are used to mitigate off-site roadway impacts. Therefore, no intersection improvements will be warranted based on the intersection analysis conducted as part of this report.

Site specific turn lane improvements will be evaluated at the time the project seeks a Local Development Order approval.

APPENDIX

METHODOLOGY MEETING NOTES

MEMORANDUM

TO: Mr. Tom Ross, P.E.
Jacobs

FROM: Yury Bykau
Transportation Consultant

DATE: June 9, 2021

RE: Bonita 75 CPDA
Rezoning Traffic Impact Statement Methodology
City of Bonita Springs

This memorandum summarizes the methodology that will be utilized for the traffic impact analysis to be prepared by TR Transportation Consultants, Inc. for the rezoning application to permit up to 252 multi-family residential dwelling units and up to 10,000 square feet of commercial uses at the northeast corner of Bonita Beach Road and I-75 in the City of Bonita Springs.

Access to the subject site will be provided to Orr Road and Miller Road via multiple connections. Project traffic will utilize the existing median opening at Bonita Beach Road intersection with Miller Road/Hunters Ridge Road to access the site. The TIS will be prepared consistent with the methodology outlined in City of Bonita Springs Resolution No. 17-07.

The site is currently governed by Zoning Ordinance No. 08-10 and ADD15-23355, which permit the site to be developed up to 88,000 square feet of retail uses, 58,800 square feet of mini-warehouse uses, a gas station with up to 12 refueling positions, a 5,000 square foot bank with a drive-through and up to 124 hotel rooms.

TRIP GENERATION

The trip generation will be completed utilizing the Institute of Transportation Engineer's (ITE) report, *Trip Generation Manual*, 10th Edition. **Table 1** summarizes the land uses that could be constructed under the existing CPD and the land uses under the proposed CPD Amendment. As can be seen from Table 1 the proposed rezoning request will be a "down zoning" from what is currently permitted.

Table 1
Land Uses
Bonita 75 CPD

Land Use	Total Approved	Total Proposed	Difference
Shopping Center LUC 820	88,000 sq. ft.	10,000 sq. ft.	-78,000 sq. ft.
Multifamily Housing (Mid-Rise) LUC 221	-	252 Units	+252 Units
Mini-Warehouse LUC 151	58,800 sq. ft.	-	-58,800 sq. ft.
Gas Station LUC 960	12 Refueling Positions	-	-12 Refueling positions
Bank W/ Drive-Through LUC 912	5,000 sq. ft.	-	-5,000 sq. ft.
Hotel LUC 310	124 Rooms	-	-124 Rooms

Table 2 and **Table 3** reflect the weekday AM and PM peak hour trip generation that is permitted under the currently approved Bonita 75 CPD.

Table 2
Trip Generation – Total Trips
Bonita 75 CPD – Approved

Land Use	Weekday A.M. Peak Hour			Weekday P.M. Peak Hour			Daily (2-way)
	In	Out	Total	In	Out	Total	
Shopping Center (88,000 Sq. Ft.)	122	74	196	237	257	494	5,511
Mini-Warehouse (58,800 Sq. Ft.)	4	2	6	5	5	10	89
Super Convenience Market/Gas Station (12 Refueling Positions)	168	169	337	138	138	276	2,766
Drive-in Bank (5,000 Sq. Ft.)	28	20	48	51	51	102	531
Hotel (124 Rooms)	34	23	57	34	33	67	973
Total Trips	356	288	644	465	484	949	9,870

Table 3
Trip Generation – New Trips
Bonita 75 CPD – Approved

Land Use	Weekday A.M. Peak Hour			Weekday P.M. Peak Hour			Daily (2-way)
	In	Out	Total	In	Out	Total	
Total Trips	356	288	644	465	484	949	9,870
Less Retail Pass-by (30% AM/PM)	-49	-49	-98	-74	-74	-148	-1,653
Less Gas Station Pass-by (63% AM/ 66% PM)	-106	-106	-212	-91	-91	-182	-1,826
Less Bank Pass-by (29% AM/ 35% PM)	-7	-7	-14	-18	-18	-36	-186
New Trips	194	126	320	282	301	583	6,205

*Consistent with Lee County guidelines, the pass-by rate for retail uses was limited to a maximum of 30%.
 *Due to lack of pass-by data for LUC 960, the pass-by rates from LUC 853 (Convenience market with Gasoline Pumps) were utilized instead.

Table 4 and Table 5 reflect the weekday AM and PM peak hour trip generation under the proposed zoning request.

Table 4
Trip Generation – Total Trips
Bonita 75 CPD – Proposed

Land Use	Weekday A.M. Peak Hour			Weekday P.M. Peak Hour			Daily (2-way)
	In	Out	Total	In	Out	Total	
Multifamily Housing Mid-Rise (252 Dwelling Units)	22	63	85	66	42	108	1,372
Shopping Center (10,000 Sq. Ft.)	97	60	157	48	51	99	1,256
Total Trips	119	123	242	114	93	207	2,628

Table 5
Trip Generation – New Trips
Bonita 75 CPD – Proposed

Land Use	Weekday A.M. Peak Hour			Weekday P.M. Peak Hour			Daily (2-way)
	In	Out	Total	In	Out	Total	
Total Trips	119	123	242	114	93	207	2,628
Less 30% Retail Pass-by	-23	-23	-46	-15	-15	-30	-378
New Trips	96	100	196	99	78	177	2,250

*Consistent with Lee County guidelines, the pass-by rate for retail uses was limited to a maximum of 30%.

Table 6 indicates the trip generation difference between the approved CPD (Table 3) and proposed CPD Amendment (Table 5).

Table 6
Trip Generation – Resultant Trip Change
Bonita 75 CPDA

Land Use	A.M. Peak Hour			P.M. Peak Hour			Daily (2-way)
	In	Out	Total	In	Out	Total	
Proposed CPD	96	100	196	99	78	177	2,250
Approved CPD	-194	-126	-320	-282	-301	-583	-6,205
Resultant Trip Change	-98	-26	-124	-183	-223	-406	-3,955

Note: A positive (+) number indicates an increase in trips and a negative number (-) indicates a decrease in trips.

As can be seen in Table 6, the trip generation will be decreased in the AM peak hour by approximately 39% and decreased in the PM peak hour by approximately 70% as a result of the proposed CPD Amendment. Table 6 also shows that the overall daily trip generation will be decreased by approximately 64%. As can be seen from Table 6 the proposed rezoning request will be a significant “down zoning” from what is currently permitted.

TRIP DISTRIBUTION

The trip distribution for the project trips was estimated manually. Attached Table 1A illustrates projected traffic distribution of the new project trips.

STUDY AREA

The study area will consist of arterial and collector roads where the project related trips equals or exceeds 2% or 3% of the directional peak hour capacity of the level of service standard for each roadway segment. Based on attached Table 1A the study area will only include Bonita Beach Road links west and east of Miller Road/Hunters Ridge Boulevard.

EXISTING/FUTURE TRAFFIC

Existing traffic conditions will be based on the traffic data as reported in the 2020 City of Bonita Springs Traffic Count Report prepared by FTE.

Analysis scenarios will be as follows:

- Existing traffic within the study area
- Existing traffic plus vested trips (2025 Future Background Traffic Conditions)
- Existing traffic plus vested trips plus AM/PM peak hour project trips added to the links.



Mr. Tom Ross, P.E.
Bonita 75 CPDA
Transportation Methodology
June 9, 2021
Page 5

Intersection analysis will be conducted at the unsignalized intersection of Bonita Beach Road with Miller Road/Hunters Ridge Boulevard.

Attachments

**TABLE 1A
PROJECT'S AREA OF IMPACT
BONITA 75 CPDA**

TOTAL AM PEAK HOUR PROJECT TRAFFIC = 196 VPH IN= 96 OUT= 100
 TOTAL PM PEAK HOUR PROJECT TRAFFIC = 177 VPH IN= 99 OUT= 78

ROADWAY	SEGMENT	ROADWAY						PERCENT			
		CLASS	LOS A VOLUME	LOS B VOLUME	LOS C VOLUME	LOS D VOLUME	LOS E VOLUME	PROJECT TRAFFIC	PROJECT TRAFFIC	2% / 3% IMPACT	
Bonita Beach Road	E. of Site	4LD	0	250	1,840	1,960	1,960	1,960	25%	25	1.3%
	W. of Site	4LD	0	250	1,840	1,960	1,960	1,960	75%	75	3.8%
	W. of I-75	6LD	0	400	2,840	2,940	2,940	2,940	40%	40	1.4%
I-75	N. of Bonita Beach Rd	6LF	0	3,410	4,650	5,780	6,340	6,340	20%	20	0.3%
	S. of Bonita Beach Rd	6LF	0	3,410	4,650	5,780	6,340	6,340	15%	15	0.2%

* Level of Service Thresholds for County/City roadways were obtained from the Lee County Generalized Peak Hour Directional Service Volumes tables (April, 2016)

* Level of Service Thresholds for State Roadways were obtained from the FDOT's Generalized Peak Hour Directional Volumes, Table 7.

TABLES 1A, 2A & 3A

**TABLE 1A
PROJECT'S AREA OF IMPACT
BONITA 75 CPDA**

ROADWAY	SEGMENT	ROADWAY CLASS	TOTAL AM PEAK HOUR PROJECT TRAFFIC =		TOTAL PM PEAK HOUR PROJECT TRAFFIC =		IN=		OUT=		LOS D VOLUME	LOS E VOLUME	PERCENT		PROJECT TRAFFIC	PROJECT IMPACT
			122 VPH	37	183 VPH	108	85	75	LOS A VOLUME	LOS B VOLUME			LOS C VOLUME	LOS D VOLUME		
Bonita Beach Road	E. of Site	4LD	0	250	0	1,840	1,960	1,960	1,960	1,960	1,960	1,960	25%	27	1.4%	
	W. of Site	4LD	0	250	1,840	1,960	1,960	1,960	1,960	1,960	1,960	1,960	75%	81	4.1%	
	W. of I-75	6LD	0	400	2,840	2,940	2,940	2,940	2,940	2,940	2,940	2,940	40%	43	1.5%	
I-75	N. of Bonita Beach Rd	6LF	0	3,410	4,650	5,780	5,780	5,780	5,780	5,780	6,340	6,340	20%	22	0.3%	
	S. of Bonita Beach Rd	6LF	0	3,410	4,650	5,780	5,780	5,780	5,780	6,340	6,340	6,340	15%	16	0.3%	

* Level of Service Thresholds for County/City roadways were obtained from the Lee County Generalized Peak Hour Directional Service Volumes tables (April, 2016)

* Level of Service Thresholds for State Roadways were obtained from the FDOT's Generalized Peak Hour Directional Volumes, Table 7.

**TABLE 3A
ANNUAL GROWTH RATE CALCULATIONS
BASED UPON HISTORICAL AADT DATA**

<u>ROADWAY</u>	<u>SEGMENT</u>	<u>CURRENT ID#</u>	<u>BASE AADT VOLUME</u>	<u>2020 AADT VOLUME</u>	<u>YRS OF GROWTH</u>	<u>ANNUAL GROWTH RATE</u>	<u>ACTUAL GROWTH RATE</u>
Bonita Beach Rd.	E. of Bonita Grande Dr	124903	9,900	12,500	5	4.77%	4.77%

* Historical traffic volumes were obtained from the FDOT's Florida Traffic Online webpage.

** In instances where the historical data indicates a reduction in traffic or insufficient data was available to calculate a growth rate due to construction, a minimum annual growth rate of 2.0% was assumed.

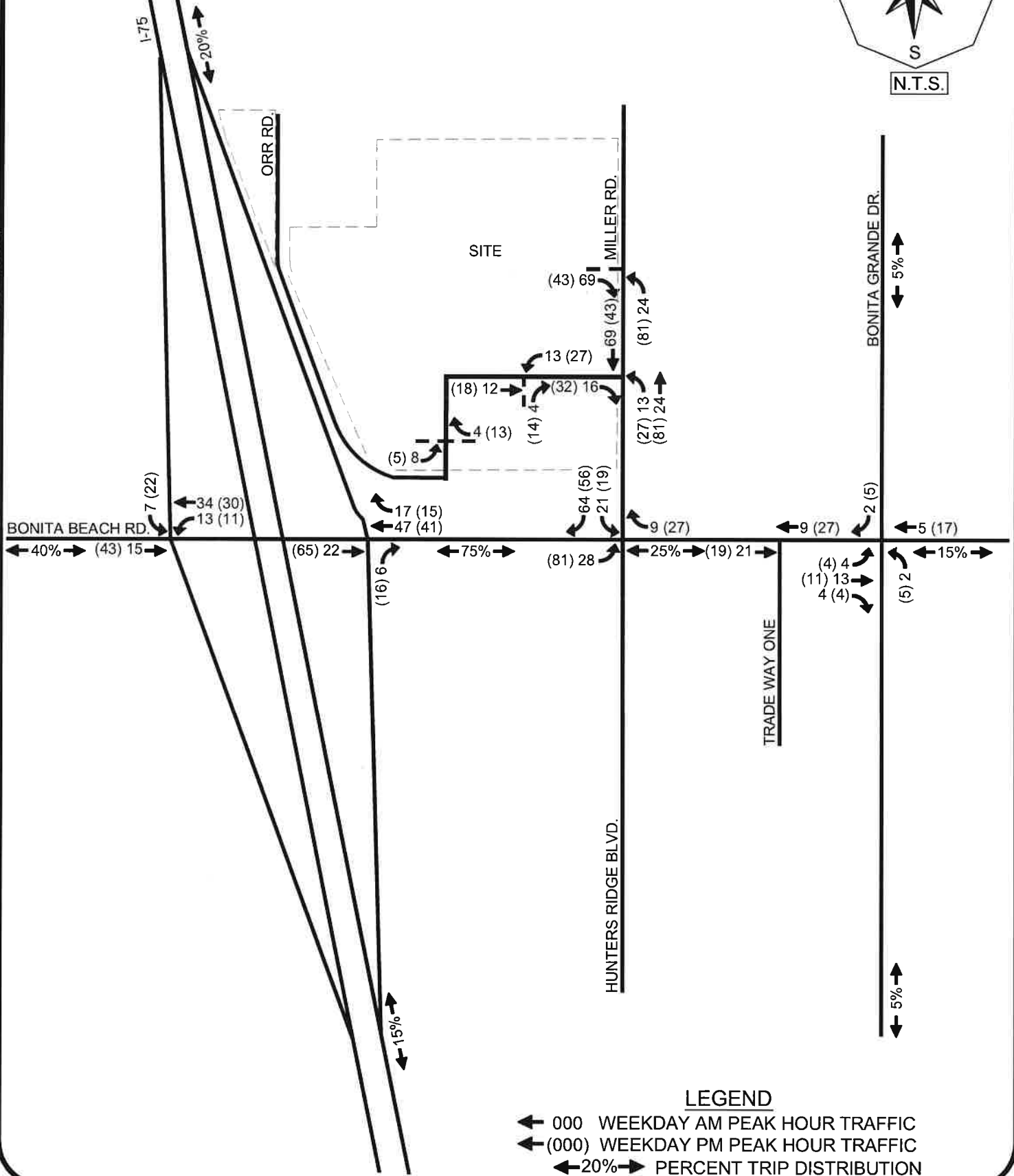
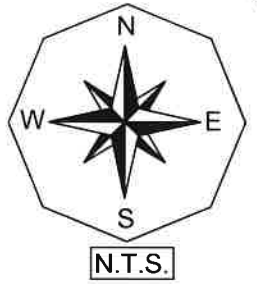
SAMPLE GROWTH RATE CALCULATION

$$\text{Annual Growth Rate (AGR)} = \frac{2020 \text{ AADT}^{\wedge(1/\text{Yrs of Growth})}}{\text{BASE AADT}} - 1$$

$$\text{AGR (BBR)} = \frac{12,500^{\wedge(1/5)}}{9,900} - 1$$

$$\text{AGR (BBR)} = 4.77\%$$

SUPPLEMENTAL FIGURES A-1 & A-2



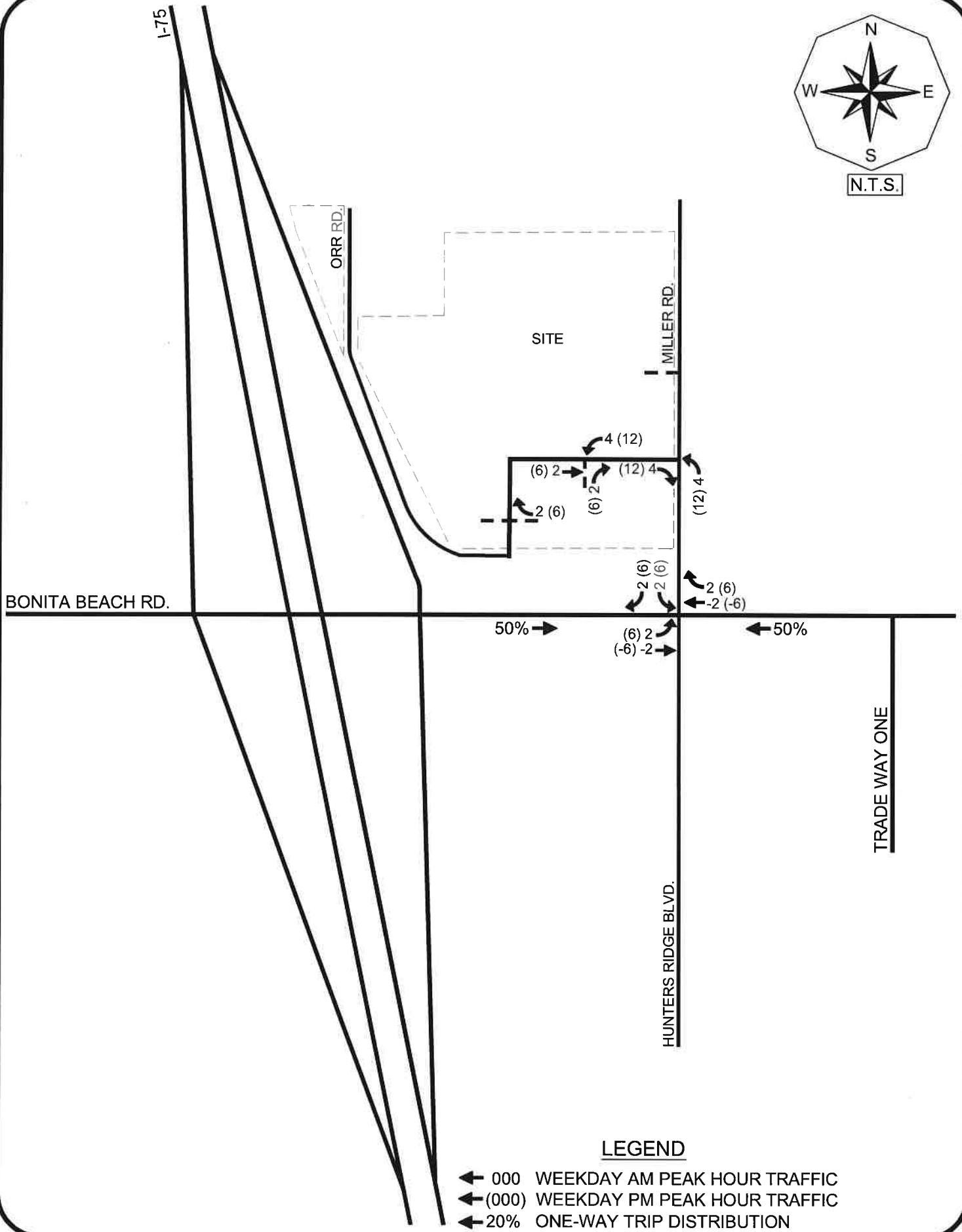
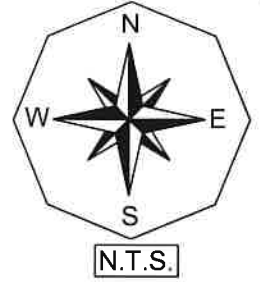
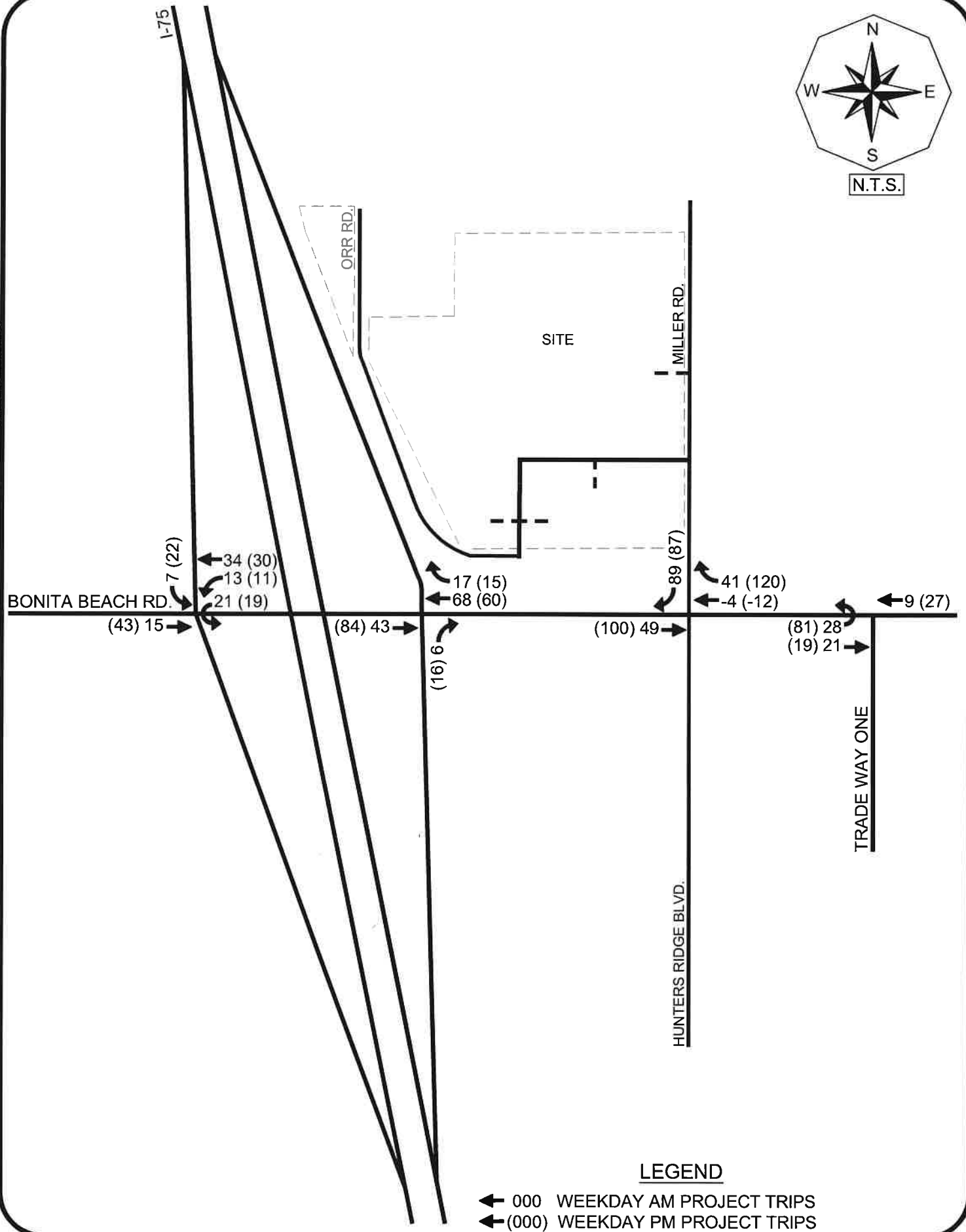
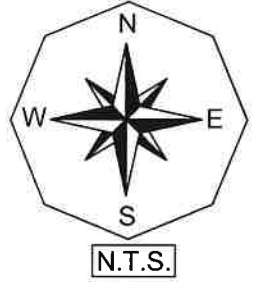


FIGURE 2A



LEGEND

- ← 000 WEEKDAY AM PROJECT TRIPS
- ← (000) WEEKDAY PM PROJECT TRIPS

**FDOT GENERALIZED PEAK HOUR
DIRECTIONAL SERVICE VOLUMES
TABLE 7**

TABLE 7

Generalized **Peak Hour Directional** Volumes for Florida's
Urbanized Areas

January 2020

INTERRUPTED FLOW FACILITIES						UNINTERRUPTED FLOW FACILITIES					
STATE SIGNALIZED ARTERIALS						FREEWAYS					
Class I (40 mph or higher posted speed limit)						Core Urbanized					
Lanes	Median	B	C	D	E	Lanes	B	C	D	E	
1	Undivided	*	830	880	**	2	2,230	3,100	3,740	4,080	
2	Divided	*	1,910	2,000	**	3	3,280	4,570	5,620	6,130	
3	Divided	*	2,940	3,020	**	4	4,310	6,030	7,490	8,170	
4	Divided	*	3,970	4,040	**	5	5,390	7,430	9,370	10,220	
						6	6,380	8,990	11,510	12,760	
Class II (35 mph or slower posted speed limit)						Urbanized					
Lanes	Median	B	C	D	E	Lanes	B	C	D	E	
1	Undivided	*	370	750	800	2	2,270	3,100	3,890	4,230	
2	Divided	*	730	1,630	1,700	3	3,410	4,650	5,780	6,340	
3	Divided	*	1,170	2,520	2,560	4	4,550	6,200	7,680	8,460	
4	Divided	*	1,610	3,390	3,420	5	5,690	7,760	9,520	10,570	
Non-State Signalized Roadway Adjustments						Freeway Adjustments					
(Alter corresponding state volumes by the indicated percent.)						Auxiliary Lane + 1,000 Ramp Metering + 5%					
Non-State Signalized Roadways - 10%											
Median & Turn Lane Adjustments						UNINTERRUPTED FLOW HIGHWAYS					
Lanes	Median	Exclusive Left Lanes	Exclusive Right Lanes	Adjustment Factors		Lanes	Median	B	C	D	E
1	Divided	Yes	No	+5%		1	Undivided	580	890	1,200	1,610
1	Undivided	No	No	-20%		2	Divided	1,800	2,600	3,280	3,730
Multi	Undivided	Yes	No	-5%		3	Divided	2,700	3,900	4,920	5,600
Multi	Undivided	No	No	-25%							
-	-	-	Yes	+5%							
One-Way Facility Adjustment						Uninterrupted Flow Highway Adjustments					
Multiply the corresponding directional volumes in this table by 1.2						Lanes Median Exclusive left lanes Adjustment factors					
						1 Divided Yes +5%					
						Multi Undivided Yes -5%					
						Multi Undivided No -25%					
BICYCLE MODE²						¹ Values shown are presented as peak hour directional volumes for levels of service and are for the automobile/truck modes unless specifically stated. This table does not constitute a standard and should be used only for general planning applications. The computer models from which this table is derived should be used for more specific planning applications. The table and deriving computer models should not be used for corridor or intersection design, where more refined techniques exist. Calculations are based on planning applications of the HCM and the Transit Capacity and Quality of Service Manual.					
(Multiply vehicle volumes shown below by number of directional roadway lanes to determine two-way maximum service volumes.)						² Level of service for the bicycle and pedestrian modes in this table is based on number of vehicles, not number of bicyclists or pedestrians using the facility.					
Paved Shoulder/Bicycle Lane Coverage						³ Buses per hour shown are only for the peak hour in the single direction of the higher traffic flow.					
	B	C	D	E		* Cannot be achieved using table input value defaults.					
0-49%	*	150	390	1,000		** Not applicable for that level of service letter grade. For the automobile mode, volumes greater than level of service D become F because intersection capacities have been reached. For the bicycle mode, the level of service letter grade (including F) is not achievable because there is no maximum vehicle volume threshold using table input value defaults.					
50-84%	110	340	1,000	>1,000		Source: Florida Department of Transportation Systems Implementation Office https://www.fdot.gov/planning/systems/					
85-100%	470	1,000	>1,000	**							
PEDESTRIAN MODE²											
(Multiply vehicle volumes shown below by number of directional roadway lanes to determine two-way maximum service volumes.)											
Sidewalk Coverage	B	C	D	E							
0-49%	*	*	140	480							
50-84%	*	80	440	800							
85-100%	200	540	880	>1,000							
BUS MODE (Scheduled Fixed Route)³											
(Buses in peak hour in peak direction)											
Sidewalk Coverage	B	C	D	E							
0-84%	> 5	≥ 4	≥ 3	≥ 2							
85-100%	> 4	≥ 3	≥ 2	≥ 1							

**LEE COUNTY GENERALIZED
SERVICE VOLUMES**

**Lee County
Generalized Peak Hour Directional Service Volumes
Urbanized Areas**

April 2016

c:\input5

Uninterrupted Flow Highway						
Level of Service						
Lane	Divided	A	B	C	D	E
1	Undivided	130	420	850	1,210	1,640
2	Divided	1,060	1,810	2,560	3,240	3,590
3	Divided	1,600	2,720	3,840	4,860	5,380
Arterials						
Class I (40 mph or higher posted speed limit)						
Level of Service						
Lane	Divided	A	B	C	D	E
1	Undivided	*	140	800	860	860
2	Divided	*	250	1,840	1,960	1,960
3	Divided	*	400	2,840	2,940	2,940
4	Divided	*	540	3,830	3,940	3,940
Class II (35 mph or slower posted speed limit)						
Level of Service						
Lane	Divided	A	B	C	D	E
1	Undivided	*	*	330	710	780
2	Divided	*	*	710	1,590	1,660
3	Divided	*	*	1,150	2,450	2,500
4	Divided	*	*	1,580	3,310	3,340
Controlled Access Facilities						
Level of Service						
Lane	Divided	A	B	C	D	E
1	Undivided	*	160	880	940	940
2	Divided	*	270	1,970	2,100	2,100
3	Divided	*	430	3,050	3,180	3,180
Collectors						
Level of Service						
Lane	Divided	A	B	C	D	E
1	Undivided	*	*	310	660	740
1	Divided	*	*	330	700	780
2	Undivided	*	*	730	1,440	1,520
2	Divided	*	*	770	1,510	1,600
Note: the service volumes for I-75 (freeway), bicycle mode, pedestrian mode, and bus mode should be from FDOT's most current version of LOS Handbook.						

**TRAFFIC DATA FROM CITY OF
BONITA SPRINGS TRAFFIC COUNT
REPORT**

FTE Station Number	Reference Lee County Station Number	Location	Start Date	Direction 1 and 2	3 Day Average Direction 1	3 Day Average Direction 2	ADT Direction 1 and 2	FDOT Seasonal Factor	AAADT Direction 1 and 2	K Factor from Lee County	D Factor from Lee County	Peak Hour Two-way Service Volumes	Level Of Service (LOS)	Lee County PCS
1204	N/A	Morton Ave N of East Terry St	2-Mar-21	N/S	3209	3205	6414	0.93	6000	11%	53%	660	D	42
1223	N/A	Old 41 Rd Between Collier County Line to Bonita Beach Rd	2-Mar-21	N/S	9953	8985	18938	0.93	17600	12%	57%	2112	F	16
1222	N/A	Old 41 Rd N of Bonita Beach Rd	2-Mar-21	N/S	6397	7479	13876	0.93	12900	12%	57%	1548	F	16
1220	N/A	Old 41 Rd N of E/W Terry St	16-Mar-21	N/S	10583	11033	21616	0.93	20100	12%	57%	2412	D	16
1216	N/A	Old 41 Rd S of US 41	2-Mar-21	N/S	8290	6417	14707	0.93	13700	12%	57%	1644	D	16
1228	N/A	Old 41 S/O Berwood Pkwy	2-Mar-21	N/S	8270	8352	16622	0.93	15500	12%	57%	1860	D	16
0002	N/A	Paradise Rd N. of Shangri-La	2-Mar-21	N/S	1891	1897	3788	0.93	3500	17%	57%	595	D	63
0001	N/A	Pennsylvania Ave E. of Los Amigos Lane	16-Mar-21	E/W	1579	1776	3355	0.93	3100	13%	56%	403	C	92
1221	494	Pennsylvania Ave W of Old 41 Rd	2-Mar-21	E/W	2363	2098	4461	0.93	4100	11%	53%	451	C	42
0003	N/A	Tropical Acres Dr N. of Shangri-La	2-Mar-21	N/S	312	282	594	0.93	600	17%	57%	102	C	63
1212	N/A	Shangri-La Rd E of Old US 41	2-Mar-21	E/W	3710	3431	7141	0.93	6600	12%	57%	792	D	16
0010	N/A	US-41, N. of Shopping Center Entrance	2-Mar-21	N/S	26809	26204	53013	0.93	49300	11%	55%	5423	F	93
0009	N/A	US-41, S. of Beaumont Rd	2-Mar-21	N/S	21549	20884	42433	0.93	39500	13%	56%	5135	D	92
0008	N/A	Vanderbilt Dr N. of Woods Edges Pkwy	2-Mar-21	N/S	4881	4730	9611	0.93	8900	13%	58%	1157	D	7
1219	N/A	W Terry St E of US 41	2-Mar-21	E/W	6019	6058	12077	0.93	11200	11%	53%	1232	C	42
1225	N/A	Woods Edge Pkwy W of US 41	2-Mar-21	E/W	3621	2652	6273	0.93	5800	12%	60%	696	C	23
1210	N/A	Longfellow Ln W of Imperial Pkwy	2-Mar-21	E/W	423	298	721	0.93	700	11%	53%	77	C	42
0017	N/A	Bonita Beach Rd between Imperial Parkway and I-75	2-Mar-21	E/W	23672	23177	46849	0.93	43600	11%	53%	4796	C	42
0018	N/A	Bonita Beach Rd between Hunters Ridge Blvd and Bonita Grande Dr	2-Mar-21	E/W	12120	12502	24622	0.93	22900	11%	53%	2519	D	42
0019	N/A	Bonita Beach Rd E. of Bonita Grande Dr	2-Mar-21	E/W	10003	10319	20322	0.93	18900	11%	53%	2079	D	42
0020	N/A	Luke St between Kens Way and Bonita Beach Rd	2-Mar-21	N/S	353	567	920	0.93	900	13%	58%	117	C	7
0021	N/A	Quails Walk E. of Luke St	2-Mar-21	E/W	263	256	519	0.93	500	13%	58%	65	C	7
0022	N/A	Imperial Shores Blvd S. of Vanda Dr	2-Mar-21	N/S	1287	1449	2736	0.93	2500	13%	58%	325	C	7
0023	N/A	Tarpon Avenue E. of Sherry Ln	2-Mar-21	E/W	427	313	740	0.93	700	13%	58%	91	C	7
0024	N/A	Logan Blvd S. of Bonita Beach Rd	2-Mar-21	N/S	2950	2811	5761	0.93	5400	11%	53%	594	D	42
0025	N/A	Bonita Beach Rd E. of Logan Blvd	2-Mar-21	E/W	6553	6545	13098	0.93	12200	11%	53%	1342	C	42

** Collected weekend counts also.



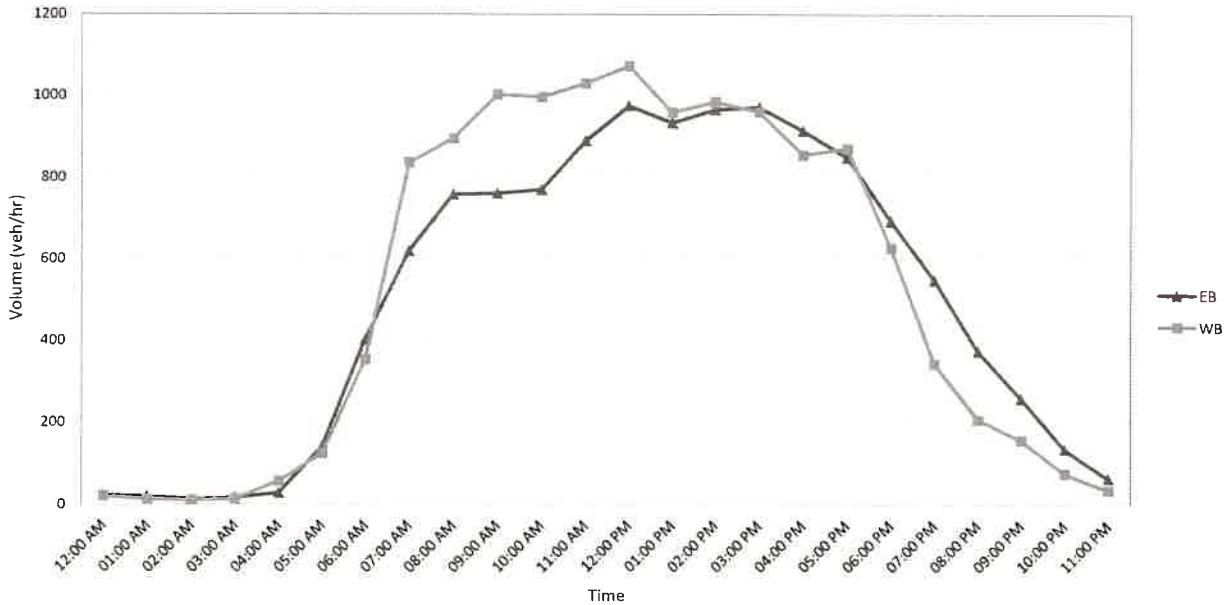
0018-Bonita Beach Rd between
Hunters Ridge Blvd and Bonita Grande Dr
Bonita Springs, FL



8250 Pascal Dr
Punta Gorda, FL
Ph# (941) 639 2818
Fax# (941) 209 5331

Time	Tuesday 3/2/2021		Wednesday 3/3/2021		Thursday 3/4/2021		Threeday Average	
	EB	WB	EB	WB	EB	WB	EB	WB
	12:00 AM	19	21	23	16	25	23	22
01:00 AM	11	12	25	12	25	13	20	12
02:00 AM	12	7	12	8	13	14	12	10
03:00 AM	18	19	13	9	21	10	17	13
04:00 AM	24	49	26	54	34	68	28	57
05:00 AM	149	132	134	122	140	118	141	124
06:00 AM	397	348	421	339	390	372	403	353
07:00 AM	606	809	591	829	660	869	619	836
08:00 AM	740	943	737	848	793	893	757	895
09:00 AM	757	1028	715	980	807	1001	760	1003
10:00 AM	727	994	781	966	798	1032	769	997
11:00 AM	893	1019	901	1015	873	1055	889	1030
12:00 PM	966	1136	969	972	991	1110	975	1073
01:00 PM	932	966	933	929	933	983	933	959
02:00 PM	966	1002	967	967	965	987	966	985
03:00 PM	945	936	983	987	987	958	972	960
04:00 PM	840	788	988	893	913	885	914	855
05:00 PM	865	898	842	851	840	865	849	871
06:00 PM	701	591	638	643	738	647	692	627
07:00 PM	573	359	528	308	544	364	548	344
08:00 PM	325	191	403	215	395	218	374	208
09:00 PM	263	135	242	167	273	170	259	157
10:00 PM	131	65	142	90	136	77	136	77
11:00 PM	46	30	71	33	79	45	65	36
Day Total	11906	12478	12085	12253	12373	12777	12120	12502
Combine Totals	24384		24338		25150		24622	

Threeday Average



**VESTED BONITA BEACH ROAD
TRAFFIC BY LINK
PROVIDED BY THE CITY OF BONITA
SPRINGS**

BONITA BEACH ROAD TRAFFIC BY LINK

LINK	AVERAGE SEASONAL*	AA DT	BACKGROUND	TOTAL
I-75 to East of Bonita Grand	9500		44400	53900
IMPERIAL TO I-75	37500		28300	50330
US 41 TO IMPERIAL	35000		11930	46930
VANDERBILT TO US 41	33585	25205	8350	41935*

Notes:

- Seasonal* is the months of Jan, Feb, Mar.
- Seasonal information is used when available.
- Background is from approved development orders or agreements.
- Link assignment is based on direct access and historic trends.

*FDOT Count Station
 right across street -
 needs to be adjusted*

FDOT PEAK SEASON FACTOR

2020 PEAK SEASON FACTOR CATEGORY REPORT - REPORT TYPE: ALL
 CATEGORY: 1200 LEE COUNTYWIDE

MOCF: 0.90
 PSCF

WEEK	DATES	SF	PSCF
* 1	01/01/2020 - 01/04/2020	0.91	1.01
* 2	01/05/2020 - 01/11/2020	0.89	0.99
* 3	01/12/2020 - 01/18/2020	0.86	0.96
* 4	01/19/2020 - 01/25/2020	0.85	0.94
* 5	01/26/2020 - 02/01/2020	0.84	0.93
* 6	02/02/2020 - 02/08/2020	0.83	0.92
* 7	02/09/2020 - 02/15/2020	0.82	0.91
* 8	02/16/2020 - 02/22/2020	0.85	0.94
* 9	02/23/2020 - 02/29/2020	0.88	0.98
*10	03/01/2020 - 03/07/2020	0.91	1.01
*11	03/08/2020 - 03/14/2020	0.95	1.06
*12	03/15/2020 - 03/21/2020	0.98	1.09
*13	03/22/2020 - 03/28/2020	1.08	1.20
14	03/29/2020 - 04/04/2020	1.18	1.31
15	04/05/2020 - 04/11/2020	1.28	1.42
16	04/12/2020 - 04/18/2020	1.38	1.53
17	04/19/2020 - 04/25/2020	1.31	1.46
18	04/26/2020 - 05/02/2020	1.25	1.39
19	05/03/2020 - 05/09/2020	1.18	1.31
20	05/10/2020 - 05/16/2020	1.12	1.24
21	05/17/2020 - 05/23/2020	1.11	1.23
22	05/24/2020 - 05/30/2020	1.11	1.23
23	05/31/2020 - 06/06/2020	1.10	1.22
24	06/07/2020 - 06/13/2020	1.09	1.21
25	06/14/2020 - 06/20/2020	1.09	1.21
26	06/21/2020 - 06/27/2020	1.09	1.21
27	06/28/2020 - 07/04/2020	1.10	1.22
28	07/05/2020 - 07/11/2020	1.11	1.23
29	07/12/2020 - 07/18/2020	1.11	1.23
30	07/19/2020 - 07/25/2020	1.10	1.22
31	07/26/2020 - 08/01/2020	1.08	1.20
32	08/02/2020 - 08/08/2020	1.06	1.18
33	08/09/2020 - 08/15/2020	1.05	1.17
34	08/16/2020 - 08/22/2020	1.04	1.16
35	08/23/2020 - 08/29/2020	1.04	1.16
36	08/30/2020 - 09/05/2020	1.03	1.14
37	09/06/2020 - 09/12/2020	1.03	1.14
38	09/13/2020 - 09/19/2020	1.02	1.13
39	09/20/2020 - 09/26/2020	1.00	1.11
40	09/27/2020 - 10/03/2020	0.99	1.10
41	10/04/2020 - 10/10/2020	0.97	1.08
42	10/11/2020 - 10/17/2020	0.96	1.07
43	10/18/2020 - 10/24/2020	0.95	1.06
44	10/25/2020 - 10/31/2020	0.95	1.06
45	11/01/2020 - 11/07/2020	0.95	1.06
46	11/08/2020 - 11/14/2020	0.95	1.06
47	11/15/2020 - 11/21/2020	0.95	1.06
48	11/22/2020 - 11/28/2020	0.94	1.04
49	11/29/2020 - 12/05/2020	0.93	1.03
50	12/06/2020 - 12/12/2020	0.92	1.02
51	12/13/2020 - 12/19/2020	0.91	1.01
52	12/20/2020 - 12/26/2020	0.89	0.99
53	12/27/2020 - 12/31/2020	0.86	0.96

* PEAK SEASON

27-FEB-2021 10:29:53

830UPD

1_1200_PKSEASON.TXT

**TRAFFIC DATA FROM FDOT
FLORIDA TRAFFIC ONLINE**

FLORIDA DEPARTMENT OF TRANSPORTATION
 TRANSPORTATION STATISTICS OFFICE
 2020 HISTORICAL AADT REPORT

COUNTY: 12 - LEE

SITE: 4903 - CR 865/BONITA BEACH RD, E OF BONITA GRANDE DR

YEAR	AADT	DIRECTION 1	DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2020	12500	E	0	9.00	59.30	4.20
2019	12500	C	0	9.00	59.60	4.20
2018	12200	C	6000	9.00	53.30	6.70
2017	10700	T		9.00	59.80	3.60
2016	10300	S	5200	9.00	58.80	6.80
2015	9900	F	5000	9.00	57.80	6.80
2014	9500	C	4800	9.00	57.70	6.80
2013	8800	F	4400	9.00	57.10	5.30
2012	8800	S	4400	9.00	56.50	5.30
2011	8800	C	4400	9.00	55.90	5.30
2010	7700	S	3900	12.13	57.28	3.70
2009	7700	F	3900	11.80	57.01	3.70
2008	7700	C	3900	11.72	57.80	3.70

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN
 *K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

TRAFFIC COUNTS

Bonita Beach Rd @ Hunters Ridge Blvd 6-23-2021 AM

File Name: Bonita Beach Rd @ Hunters Ridge Blvd 6-23-2021 AM

Site Code:

Location:

Study Date: 06/23/2021

All Vehicles

Time	Miller Rd Southbound				Bonita Beach Rd Westbound				Hunters Ridge Blvd Northbound				Bonita Beach Rd Eastbound						
	Right	Thru	Left	U-Turn	Right	Thru	Left	U-Turn	Right	Thru	Left	U-Turn	Appr Total	Right	Thru	Left	U-Turn	Appr Total	Int Total
07:00	0	0	0	0	0	132	0	0	0	0	8	0	8	19	148	0	0	167	307
07:15	1	0	0	1	0	162	0	0	1	0	5	0	6	9	163	0	0	172	341
07:30	0	0	0	0	0	221	1	0	222	5	15	0	20	8	148	1	1	158	400
07:45	0	0	0	0	0	167	1	0	168	0	7	0	7	17	184	0	0	201	376
Total	1	0	0	1	0	682	2	0	684	6	35	0	41	53	643	1	1	698	1424
08:00	0	0	0	0	0	181	0	0	181	1	7	0	8	12	148	0	0	160	349
08:15	0	0	0	0	0	200	3	0	203	2	12	0	14	11	161	0	1	173	390
08:30	1	0	0	1	0	203	0	0	203	0	8	0	8	16	198	0	0	214	426
08:45	0	0	0	0	0	188	1	0	189	2	11	0	13	7	193	0	0	200	402
Total	1	0	0	1	0	772	4	0	776	5	38	0	43	46	700	0	1	747	1567
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Appr %	2	0	0	2	0	1454	6	0	1460	11	73	0	84	99	1343	1	2	1445	2991
Total %	100.0	00.0	00.0	00.0	00.0	99.6	00.4	00.0	100.0	13.1	00.0	86.9	00.0	06.9	92.9	00.1	00.1	00.0	00.0
% Trucks	00.1	00.0	00.0	00.0	00.0	48.6	00.2	00.0	00.0	00.4	00.0	02.4	00.0	03.3	44.9	00.0	00.1	00.0	00.0
AM Pk Hr	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00
AM Pk Vol	1	0	0	1	0	772	4	0	776	5	38	0	43	46	700	0	1	747	1567
AM PHF	0.250	NaN	NaN	0.250	NaN	0.951	0.333	NaN	0.956	0.625	NaN	0.792	NaN	0.719	0.884	NaN	0.250	0.873	0.920

Bonita Beach Rd @ Hunters Ridge Blvd 6-23-2021 AM

File Name: Bonita Beach Rd @ Hunters Ridge Blvd 6-23-

Location:

Site Code:

Study Date: 06/23/2021

All Vehicles

Miller Rd				
1	0	0	0	0
Right	Thru	Left	U-Turn	

Bonita Beach Rd				
46	700	0	1	
Right	Thru	Left	U-Turn	

AM Peak Hour Statistics
 AM Peak Hour Begins: 08:00
 AM Peak Hour Volume: 1567
 AM Peak Hour Factor: 0.920

Bonita Beach Rd				
0	772	4	0	
Right	Thru	Left	U-Turn	

Hunters Ridge Blvd				
0	38	0	5	
U-Turn	Left	Thru	Right	

Bonita Beach Rd @ Hunters Ridge Blvd 6-23-2021 PM

File Name: Bonita Beach Rd @ Hunters Ridge Blvd 6-23-

Site Code:

Location:

All Vehicles

Study Date: 06/23/2021

Time	Miller Rd Southbound				Bonita Beach Rd Westbound				Hunters Ridge Blvd Northbound				Bonita Beach Rd Eastbound								
	Right	Thru	Left	U-Turn	Appr Total	Right	Thru	Left	U-Turn	Appr Total	Right	Thru	Left	U-Turn	Appr Total	Right	Thru	Left	U-Turn	Appr Total	Int Total
16:00	0	0	0	0	0	1	214	0	0	215	1	0	13	0	14	15	154	0	0	169	398
16:15	1	0	0	1	148	0	148	0	0	148	1	0	11	0	12	11	191	0	0	202	363
16:30	1	0	0	1	149	0	149	0	0	150	2	0	12	0	14	14	163	0	0	177	342
16:45	0	0	0	0	131	0	131	0	0	131	1	0	15	0	16	19	163	0	0	182	329
Total	2	0	0	2	644	2	642	0	0	644	5	0	51	0	56	59	671	0	0	730	1432
17:00	0	0	0	0	190	0	190	0	0	190	2	0	14	0	16	7	166	0	0	173	379
17:15	0	0	0	0	173	0	173	0	0	173	2	0	10	0	12	9	208	0	0	217	402
17:30	0	0	0	0	158	0	158	0	0	158	0	0	13	0	13	12	174	0	0	186	357
17:45	0	0	0	0	122	0	122	0	0	122	1	0	8	0	9	6	131	0	0	137	268
Total	0	0	0	0	643	0	643	0	0	643	5	0	45	0	50	34	679	0	0	713	1406
Grand Total	2	0	0	2	1287	2	1285	0	0	1287	10	0	96	0	106	93	1350	0	0	1443	2838
Appr %	100.0	00.0	00.0	00.0		00.2	99.8	00.0	00.0		09.4	00.0	90.6	00.0		06.4	93.6	00.0	00.0		
Total %	00.1	00.0	00.0	00.0		00.1	45.3	00.0	00.0		00.4	00.0	03.4	00.0		03.3	47.6	00.0	00.0		
% Trucks	00.0	-	-	00.0		00.0	00.0	-	-		00.0	-	00.0	-		00.0	00.0	-	-		00.0
PM Pk Hr	16:45	16:45	16:45	16:45	16:45	16:45	16:45	16:45	16:45	16:45	16:45	16:45	16:45	16:45	16:45	16:45	16:45	16:45	16:45	16:45	16:45
PM Pk Vol	0	0	0	0	652	0	652	0	0	652	5	0	52	0	57	47	711	0	0	758	1467
PM PHF	NaN	NaN	NaN	NaN	0.858	NaN	0.858	NaN	NaN	0.858	0.625	NaN	0.867	NaN	0.891	0.618	0.855	NaN	NaN	0.873	0.912

Bonita Beach Rd @ Hunters Ridge Blvd 6-23-2021 PM





File Name: Bonita Beach Rd @ Hunters Ridge Blvd 6-23-





Location:

Site Code:





Study Date: 06/23/2021

All Vehicles

Miller Rd				
0	0	0	0	0
Right	Thru	Left	U-Turn	
				

Bonita Beach Rd				
0	0	711	47	
U-Turn	Left	Thru	Right	
				

PM Peak Hour Statistics
 PM Peak Hour Begins: 16:45
 PM Peak Hour Volume: 1467
 PM Peak Hour Factor: 0.912

Bonita Beach Rd				
0	652	0	0	
Right	Thru	Left	U-Turn	
				

Hunters Ridge Blvd				
0	52	0	5	
U-Turn	Left	Thru	Right	
				

Bonita Beach Rd @ Trade Way One

File Name: Bonita Beach Rd @ Trade Way One AM

Site Code:

Location:

Study Date: 11/02/2021

Cars and Peds





Time	Trade Way One Southbound				Bonita Beach Rd Westbound				Trade Way One Northbound				Trade Way One Eastbound										
	Peds	Right	Thru	Left	U-Turn	Appr Total	Peds	Right	Thru	Left	U-Turn	Appr Total	Peds	Right	Thru	Left	U-Turn	Appr Total	Int Total				
06:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1				
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1				
07:00	0	0	0	0	0	143	6	0	7	0	13	20	139	4	0	0	0	163	319				
07:15	2	0	0	0	0	169	2	0	2	0	4	14	124	2	0	0	0	140	315				
07:30	0	0	0	0	0	206	9	0	9	0	18	12	125	2	0	0	0	139	363				
07:45	0	0	0	0	0	186	9	0	11	0	20	11	136	2	0	0	0	149	355				
Total	0	2	0	0	0	704	0	26	29	0	55	0	57	10	0	0	0	591	1352				
08:00	0	0	0	0	0	167	5	0	7	1	13	17	140	0	1	0	0	158	338				
08:15	0	0	0	0	0	179	8	0	13	0	21	11	166	3	1	0	0	181	381				
08:30	0	0	1	0	0	200	5	0	5	0	10	16	178	2	3	0	0	199	410				
08:45	1	0	0	0	0	179	4	0	9	0	13	7	163	2	0	0	0	172	365				
Total	0	1	0	1	0	725	0	22	34	1	57	0	51	7	5	0	0	710	1494				
Grand Total	0	3	0	1	0	4	0	3	1398	28	0	48	0	63	1	112	0	108	1172	17	5	1302	2847
Appr %	75	0	25	0	0	0	0.2	97.8	2	0	0	42.9	0	56.3	0.9	8.3	90	1.3	0.4	0.4	0.2	0.2	0.2
Total %	0.1	0	0	0	0	0	0.1	49.1	1	0	0	1.7	0	2.2	0	3.8	41.2	0.6	0.2	0.6	0.2	0.2	0.2
AM PK Hr	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00
AM PK Vol	1	0	1	0	0	2	2	716	7	0	725	22	0	34	1	57	0	51	647	7	5	710	1494
AM PHF	0.250	NaN	0.250	NaN	NaN	0.906	0.250	0.899	0.438	NaN	0.906	0.688	NaN	0.654	0.250	0.679	0.750	0.909	0.583	0.417	0.892	0.911	0.911





Bonita Beach Rd @ Trade Way One





File Name: Bonita Beach Rd @ Trade Way One AM
 Location:

Site Code:
 Study Date: 11/02/2021




All Vehicles

Trade Way One				
1	0	1	0	
Right	Thru	Left	U-Turn	
				

Trade Way One				
51	647	7	5	
Right	Thru	Left	U-Turn	
				

Bonita Beach Rd				
2	716	7	0	
Right	Thru	Left	U-Turn	
				

AM Peak Hour Statistics
 AM Peak Hour Begins: 08:00
 AM Peak Hour Volume: 1494
 AM Peak Hour Factor: 0.911

Trade Way One				
1	34	0	22	
U-Turn	Left	Thru	Right	
				

Bonita Beach Rd @ Trade Way One

File Name: Bonita Beach Rd @ Trade Way One PM

Site Code:

Location:

Study Date: 11/02/2021

All Vehicles





Time	Trade Way One Southbound				Bonita Beach Rd Westbound				CM Entrance Northbound				Bonita Beach Rd Eastbound										
	Right	Thru	Left	U-Turn	Right	Thru	Left	U-Turn	Right	Thru	Left	U-Turn	Right	Thru	Left	U-Turn	Right	Thru	Left	U-Turn	Appr Total	Int Total	
15:45	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
16:00	8	1	0	0	1	220	4	0	225	5	0	9	0	14	13	168	1	0	0	0	182	430	
16:15	2	0	0	0	0	207	2	0	209	4	0	14	0	18	14	197	0	0	0	0	211	440	
16:30	0	0	0	0	0	202	2	0	204	5	0	14	0	19	6	183	0	3	0	3	192	415	
16:45	2	0	0	0	0	182	1	0	183	1	0	7	0	8	4	185	0	0	0	0	189	382	
Total	12	1	0	0	1	811	9	0	821	15	0	44	0	59	37	733	1	3	0	3	774	1667	
17:00	6	0	0	0	0	249	1	0	250	9	0	20	0	29	7	178	0	0	0	0	185	470	
17:15	4	0	2	0	0	191	4	0	195	6	0	17	0	23	6	204	0	1	0	1	211	435	
17:30	3	0	1	0	0	242	0	0	242	3	0	6	0	9	8	171	0	2	0	2	181	436	
17:45	0	0	0	0	0	197	5	0	202	8	0	8	0	16	10	179	0	0	0	0	189	407	
Total	13	0	3	0	0	879	10	0	889	26	0	51	0	77	31	732	0	3	0	3	766	1748	
Grand Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Appr %	25	1	3	0	29	1691	19	0	1711	41	0	95	0	136	68	1465	1	6	0	6	1540	3416	
Total %	86.2	03.4	10.3	00.0	00.1	98.8	01.1	00.0	30.1	00.0	69.9	00.0	04.4	95.1	00.1	00.4	02.0	42.9	00.0	00.2	00.0	00.0	00.0
% Trucks	00.7	00.0	00.1	00.0	00.0	49.5	00.6	00.0	01.2	00.0	02.8	00.0	00.0	00.0	00.0	00.0	00.0	00.0	00.0	00.0	00.0	00.0	00.0
PM Pk Hr	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00	17:00
PM Pk Vol	13	0	3	0	16	879	10	0	889	26	0	51	0	77	31	732	0	3	0	3	766	1748	
PM PHF	0.542	NaN	0.375	NaN	0.667	0.883	0.500	NaN	0.889	0.722	NaN	0.638	NaN	0.664	0.775	0.897	NaN	0.375	NaN	0.375	0.908	0.930	

Bonita Beach Rd @ Trade Way One

File Name: Bonita Beach Rd @ Trade Way One PM
 Location:





Site Code:
 Study Date: 11/02/2021





All Vehicles

Trade Way One				
13	0	3	0	
Right	Thru	Left	U-Turn	
				

Bonita Beach Rd				
31	0	732	3	
Right	Thru	Left	U-Turn	
				

PM Peak Hour Statistics
 PM Peak Hour Begins: 17:00
 PM Peak Hour Volume: 1748
 PM Peak Hour Factor: 0.930

Bonita Beach Rd				
0	879	10	0	
Right	Thru	Left	U-Turn	
				

CM Entrance				
0	51	0	26	
U-Turn	Left	Thru	Right	
				

Study Name T-8 Bonita Beach Rd & I-75 NB Ramps
Start Date 02-26-2019
Start Time 7:00
Site Code 8
Project
 Bonita Beach Rd & I-75 NB Ramps
 Tuesday TMC

Type Road
Classification Totals

Start Time	Bonita Beach Rd Eastbound			Bonita Beach Rd Westbound			I-75 NB Ramps Northbound			I-75 NB Ramps Southbound					
	U-Turn	Left	Thru	Right	U-Turn	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right
7:00	0	74	123	0	135	57	164	0	51	0	0	0	0	0	0
7:15	0	82	88	0	156	61	192	0	54	0	0	0	0	0	0
7:30	0	116	112	0	243	70	243	0	49	0	0	0	0	0	0
7:45	0	89	137	0	185	39	230	0	62	0	0	0	0	0	0
8:00	0	120	115	0	178	69	194	0	55	0	0	0	0	0	0
8:15	0	116	97	0	204	48	215	1	48	0	0	0	0	0	0
8:30	0	115	147	0	206	48	158	0	61	0	0	0	0	0	0
8:45	0	95	173	0	160	33	161	0	63	0	0	0	0	0	0
16:00	2	164	169	0	197	94	96	0	47	0	0	0	0	0	0
16:15	0	182	152	0	184	66	101	1	43	0	0	0	0	0	0
16:30	0	232	182	0	149	81	112	0	57	0	0	0	0	0	0
16:45	0	205	194	0	153	64	111	0	55	0	0	0	0	0	0
17:00	2	192	144	0	175	82	108	0	44	0	0	0	0	0	0
17:15	0	184	162	0	159	84	109	0	49	0	0	0	0	0	0
17:30	1	218	180	0	159	65	131	0	49	0	0	0	0	0	0
17:45	1	225	172	0	129	54	123	0	57	0	0	0	0	0	0

HOURLY SUMMARY OF INDIVIDUAL MOVEMENTS

Hour Begin	Bonita Beach Rd Eastbound			Bonita Beach Rd Westbound			I-75 NB Ramps Northbound			I-75 NB Ramps Southbound			Intersection Total		
	U-Turn	Left	Thru	Right	U-Turn	Thru	Right	U-Turn	Left	Thru	Right	U-Turn		Left	Thru
7:00	0	361	460	0	641	227	829	0	216	0	0	0	0	0	2,734
7:15	0	407	452	0	684	239	859	0	220	0	0	0	0	0	2,861
7:30	0	441	461	0	732	226	862	1	214	0	0	0	0	0	2,897
7:45	0	440	496	0	773	204	797	1	226	0	0	0	0	0	2,937
8:00	0	446	532	0	748	198	728	1	227	0	0	0	0	0	2,880
16:00	2	783	697	0	683	305	420	1	202	0	0	0	0	0	3,083
16:15	2	811	672	0	661	293	432	1	199	0	0	0	0	0	3,071
16:30	2	813	682	0	636	311	440	0	205	0	0	0	0	0	3,089
16:45	3	799	680	0	646	295	459	0	197	0	0	0	0	0	3,079
17:00	4	819	658	0	622	285	471	0	199	0	0	0	0	0	3,058

AM PEAK HOUR SUMMARY

Hour Begin	Bonita Beach Rd Eastbound			Bonita Beach Rd Westbound			I-75 NB Ramps Northbound			I-75 NB Ramps Southbound			Intersection Total		
	U-Turn	Left	Thru	Right	U-Turn	Thru	Right	U-Turn	Left	Thru	Right	U-Turn		Left	Thru
7:30	0	411	461	0	732	226	862	1	214	0	0	0	0	0	2,957

PM PEAK HOUR SUMMARY

Hour Begin	Bonita Beach Rd Eastbound			Bonita Beach Rd Westbound			I-75 NB Ramps Northbound			I-75 NB Ramps Southbound			Intersection Total		
	U-Turn	Left	Thru	Right	U-Turn	Thru	Right	U-Turn	Left	Thru	Right	U-Turn		Left	Thru
16:00	2	783	697	0	683	305	420	1	202	0	0	0	0	0	3,083

Study Name I-7 Bonita Beach Rd & I-75 SB Ramps
Start Date 02-26-2019
Start Time 7:00
Site Code 7
Project
Bonita Beach Rd & I-75 SB Ramps
Tuesday TMC

Classification Totals

Start Time	Bonita Beach Rd Eastbound			Bonita Beach Rd Westbound			I-75 SB Ramps Northbound			I-75 SB Ramps Southbound		
	U-Turn	Left	Right	U-Turn	Thru	Right	U-Turn	Thru	Right	U-Turn	Thru	Right
7:00	0	104	159	0	51	245	0	0	0	0	0	180
7:15	0	131	174	0	59	292	0	0	0	0	0	185
7:30	0	177	179	0	74	337	0	0	0	0	0	175
7:45	0	147	163	0	64	357	0	0	0	0	0	201
8:00	0	171	175	0	71	301	0	0	0	0	0	197
8:15	0	173	144	0	68	362	0	0	0	0	0	182
8:30	0	200	161	0	73	282	0	0	0	0	0	193
8:45	0	197	132	0	42	278	0	0	0	0	0	192
16:00	0	303	175	0	62	236	0	0	0	0	0	140
16:15	0	348	171	0	79	213	0	0	0	0	0	116
16:30	0	367	171	0	55	213	0	0	0	0	0	131
16:45	1	308	160	0	64	201	0	0	0	0	0	131
17:00	0	283	190	0	76	222	0	0	0	0	0	132
17:15	0	273	212	0	51	205	0	0	0	0	0	148
17:30	0	322	182	0	56	234	0	0	0	0	0	140
17:45	0	310	173	0	45	204	0	0	0	0	0	131

HOURLY SUMMARY OF INDIVIDUAL MOVEMENTS

Hour	Bonita Beach Rd Eastbound			Bonita Beach Rd Westbound			I-75 SB Ramps Northbound			I-75 SB Ramps Southbound			INTERSECTION TOTAL
	U-Turn	Left	Right	U-Turn	Thru	Right	U-Turn	Thru	Right	U-Turn	Thru	Right	
7:00	0	0	559	0	248	1,231	0	0	0	0	0	0	3,746
7:15	0	0	626	0	268	1,267	0	0	0	0	0	0	3,893
7:30	0	0	668	0	277	1,357	0	0	0	0	0	0	3,987
7:45	0	0	691	0	276	1,302	0	0	0	0	0	0	3,959
8:00	0	0	741	0	254	1,223	0	0	0	0	0	0	3,859
16:00	1	0	1,326	0	260	863	0	0	0	0	0	0	3,846
16:15	1	0	1,306	0	276	849	0	0	0	0	0	0	3,839
16:30	1	0	1,231	0	248	841	0	0	0	0	0	0	3,800
16:45	1	0	1,186	0	249	862	0	0	0	0	0	0	3,811
17:00	0	0	1,188	0	230	865	0	0	0	0	0	0	3,799

AM PEAK HOUR SUMMARY

Hour	Bonita Beach Rd Eastbound			Bonita Beach Rd Westbound			I-75 SB Ramps Northbound			I-75 SB Ramps Southbound			INTERSECTION TOTAL
	U-Turn	Left	Right	U-Turn	Thru	Right	U-Turn	Thru	Right	U-Turn	Thru	Right	
7:30	0	0	668	0	277	1,357	0	0	0	0	0	0	3,987

PM PEAK HOUR SUMMARY

Hour	Bonita Beach Rd Eastbound			Bonita Beach Rd Westbound			I-75 SB Ramps Northbound			I-75 SB Ramps Southbound			INTERSECTION TOTAL
	U-Turn	Left	Right	U-Turn	Thru	Right	U-Turn	Thru	Right	U-Turn	Thru	Right	
16:00	1	0	1,326	0	260	863	0	0	0	0	0	0	3,846

**DEVELOPMENT OF FUTURE YEAR
BACKGROUND TURNING VOLUMES**

Development of Future Year Background Turning Volumes

Intersection
Count Date
Build-Out Year

Bonita Beach Rd & Trade Way One
November 2, 2021
2026

	AM Peak Hour											PM Peak Hour												
	NBL	NBT	NBR	SBL	SBT	SBR	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	EBL	EBT	EBR	WBL	WBT	WBR
RAW Turning Movement Counts	35	0	22	1	0	1	12	647	51	7	716	2	51	0	26	3	0	13	3	732	31	10	879	0
Peak Season Correction Factor	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06
Current Peak Season Volumes	37	0	23	1	0	1	13	686	54	7	759	2	54	0	28	3	0	14	3	776	33	11	932	0
Growth Rate	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	4.77%	0.00%	0.00%	4.77%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	4.77%	0.00%	0.00%	4.77%	0.00%	
Years to Build-out	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	
2026 Background Turning Volumes	37	0	23	1	0	1	13	866	54	7	958	2	54	0	28	3	0	14	3	980	33	11	1,177	0
Vested Trips Turning Volumes								1,998			2,442								2,442			1,998		
Project Turning Volumes								21			9								21			9		
2026 Background + Vested	37	0	23	1	0	1	13	2,864	54	7	3,400	2	54	0	23	3	0	14	3	2,864	33	11	3,409	2
2026 Background + Project	37	0	23	1	0	1	13	887	54	7	967	2	54	0	23	3	0	14	3	887	33	11	967	2
2026 Background + Vested + Project	37	0	23	1	0	1	13	2,885	54	7	3,409	2	54	0	23	3	0	14	3	2,885	33	11	3,409	2
RAW Turning Movement Counts	51	0	26	3	0	13	3	732	31	10	879	0	51	0	26	3	0	13	3	732	31	10	879	0
Peak Season Correction Factor	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	1.06	
Current Peak Season Volumes	54	0	28	3	0	14	3	776	33	11	932	0	54	0	28	3	0	14	3	776	33	11	932	0
Growth Rate	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	4.77%	0.00%	0.00%	4.77%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	4.77%	0.00%	0.00%	4.77%	0.00%	
Years to Build-out	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	
2026 Background Turning Volumes	54	0	28	3	0	14	3	980	33	11	1,177	0	54	0	28	3	0	14	3	980	33	11	1,177	0
Vested Trips Turning Volumes								2,442			1,998								2,442			1,998		
Project Turning Volumes								19			27								19			27		
2026 Background + Vested	54	0	28	3	0	14	3	3,422	33	11	3,175	0	54	0	28	3	0	14	3	3,422	33	11	3,175	0
2026 Background + Project	54	0	28	3	0	14	3	999	33	11	1,204	0	54	0	28	3	0	14	3	999	33	11	1,204	0
2026 Background + Vested + Project	54	0	28	3	0	14	3	3,441	33	11	3,202	0	54	0	28	3	0	14	3	3,441	33	11	3,202	0

Development of Future Year Background Turning Volumes

Bonita Beach Rd & Miller Rd/Hunters Ridge Blvd
 June 23, 2021
 2026

Intersection
 Count Date
 Build-Out Year

	AM Peak Hour											
	NBL	NBT	NBR	SBL	SBT	SBR	EBL	EBT	EBR	WBL	WBT	WBR
RAW Turning Movement Counts	38	0	5	0	0	1	1	700	46	4	772	0
Peak Season Correction Factor	1.21	1.21	1.21	1.21	1.21	1.21	1.21	1.21	1.21	1.21	1.21	1.21
Current Peak Season Volumes	46	0	6	0	0	1	1	847	56	5	934	0
Growth Rate	1.00%	1.00%	1.00%	1.00%	1.00%	1.00%	1.00%	4.77%	1.00%	1.00%	4.77%	1.00%
Years to Build-out	5	5	5	5	5	5	5	5	5	5	5	5
2026 Background Turning Volumes	48	0	6	0	0	1	1	1,069	59	5	1,179	0
Vested Trips Turning Volumes								1,998			2,442	
Project Turning Volumes				23		66	30	-2		5	-2	11
2026 Background + Vested	48	0	6	0	0	1	1	3,067	59	5	3,621	0
2026 Background + Project	48	0	6	23	0	67	31	1,067	59	5	1,177	11
2026 Background + Vested + Project	48	0	6	23	0	67	31	3,065	59	5	3,619	11

	PM Peak Hour											
	NBL	NBT	NBR	SBL	SBT	SBR	EBL	EBT	EBR	WBL	WBT	WBR
RAW Turning Movement Counts	52	0	5	0	0	0	0	711	47	0	652	0
Peak Season Correction Factor	1.21	1.21	1.21	1.21	1.21	1.21	1.21	1.21	1.21	1.21	1.21	1.21
Current Peak Season Volumes	63	0	6	0	0	0	0	860	57	0	789	0
Growth Rate	1.00%	1.00%	1.00%	1.00%	1.00%	1.00%	1.00%	4.77%	1.00%	1.00%	4.77%	1.00%
Years to Build-out	5	5	5	5	5	5	5	5	5	5	5	5
2026 Background Turning Volumes	66	0	6	0	0	0	0	1,086	60	0	996	0
Vested Trips Turning Volumes								2,442			1,998	
Project Turning Volumes				25		62	87	-6		0	-6	33
2026 Background + Vested	66	0	6	0	0	0	0	3,528	60	0	2,994	0
2026 Background + Project	66	0	6	25	0	62	87	1,080	60	0	990	33
2026 Background + Vested + Project	66	0	6	25	0	62	87	3,522	60	0	2,988	33

Development of Future Year Background Turning Volumes

Bonita Beach Rd at I-75 NB Ramp
 February 26, 2019
 2026

Intersection
 Count Date
 Build-Out Year

	AM Peak Hour										PM Peak Hour													
	NBL	NBT	NBR	SBL	SBT	SBR	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	EBL	EBT	EBR	WBL	WBT	WBR
RAW Turning Movement Counts	882		214				441	461				732												226
Peak Season Correction Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Current Peak Season Volumes	864	0	210	0	0	0	432	452	0	0	0	717	0	0	0	0	0	432	452	0	0	0	717	221
Growth Rate	2.00%	0.00%	2.00%	0.00%	0.00%	0.00%	2.00%	4.77%	0.00%	0.00%	0.00%	4.77%	0.00%	0.00%	0.00%	0.00%	0.00%	2.00%	4.77%	0.00%	0.00%	0.00%	4.77%	2.00%
Years to Build-out	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7
2026 Background Turning Volumes	992	0	241	0	0	0	496	626	0	0	0	994	0	0	0	0	0	496	626	0	0	0	254	
Vested Trips Turning Volumes			254				1,744					2,044						1,744					398	
Project Turning Volumes			6					22				47						22					17	
2026 Background + Vested	992	0	495	0	0	0	496	2,370	0	0	0	3,038	0	0	0	0	0	496	2,370	0	0	0	652	
2026 Background + Project	992	0	247	0	0	0	496	648	0	0	0	1,041	0	0	0	0	0	496	648	0	0	0	271	
2026 Background + Vested + Project	992	0	501	0	0	0	496	2,392	0	0	0	3,085	0	0	0	0	0	496	2,392	0	0	0	669	
RAW Turning Movement Counts	420		202				785	697				683												305
Peak Season Correction Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Current Peak Season Volumes	412	0	198	0	0	0	769	683	0	0	0	669	0	0	0	0	0	769	683	0	0	0	669	299
Growth Rate	2.00%	0.00%	2.00%	0.00%	0.00%	0.00%	2.00%	4.77%	0.00%	0.00%	0.00%	4.77%	0.00%	0.00%	0.00%	0.00%	0.00%	2.00%	4.77%	0.00%	0.00%	0.00%	4.77%	2.00%
Years to Build-out	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7
2026 Background Turning Volumes	473	0	227	0	0	0	883	946	0	0	0	927	0	0	0	0	0	883	946	0	0	0	343	
Vested Trips Turning Volumes			496				1,946					1,708						1,946					290	
Project Turning Volumes			16					65				41						65					15	
2026 Background + Vested	473	0	723	0	0	0	883	2,892	0	0	0	2,635	0	0	0	0	0	883	2,892	0	0	0	633	
2026 Background + Project	473	0	243	0	0	0	883	1,011	0	0	0	968	0	0	0	0	0	883	1,011	0	0	0	358	
2026 Background + Vested + Project	473	0	739	0	0	0	883	2,957	0	0	0	2,676	0	0	0	0	0	883	2,957	0	0	0	648	

Development of Future Year Background Turning Volumes

Bonita Beach Rd at I-75 SB Ramp
 February 26, 2019
 2026

Intersection
 Count Date
 Build-Out Year

	AM Peak Hour											
	NBL	NBT	NBR	SBL	SBT	SBR	EBL	EBT	EBR	WBL	WBT	WBR
RAW Turning Movement Counts	0.98	0.98	0.98	269	0.98	755	0.98	668	661	277	1,357	0.98
Peak Season Correction Factor	0	0	0	264	0	740	0	655	648	271	1,330	0
Current Peak Season Volumes	0.00%	0.00%	0.00%	2.00%	0.00%	2.00%	0.00%	4.77%	2.00%	2.00%	4.77%	0.00%
Growth Rate	7	7	7	7	7	7	7	7	7	7	7	7
Years to Build-out	0	0	0	303	0	850	0	908	744	311	1,843	0
2026 Background Turning Volumes				471				1273		487	1557	
Vested Trips Turning Volumes												
Project Turning Volumes	0	0	0	7	0	850	0	15	744	13	34	0
2026 Background + Vested	0	0	0	774	0	850	0	2,181	744	798	3,400	0
2026 Background + Project	0	0	0	310	0	850	0	923	744	324	1,877	0
2026 Background + Vested + Project	0	0	0	781	0	850	0	2,196	744	811	3,434	0

	PM Peak Hour											
	NBL	NBT	NBR	SBL	SBT	SBR	EBL	EBT	EBR	WBL	WBT	WBR
RAW Turning Movement Counts	0.98	0.98	0.98	200	0.98	518	0.98	1,326	677	260	863	0.98
Peak Season Correction Factor	0	0	0	196	0	508	0	1,299	663	255	846	0
Current Peak Season Volumes	0.00%	0.00%	0.00%	2.00%	0.00%	2.00%	0.00%	4.77%	2.00%	2.00%	4.77%	0.00%
Growth Rate	7	7	7	7	7	7	7	7	7	7	7	7
Years to Build-out	0	0	0	225	0	584	0	1,800	762	293	1,172	0
2026 Background Turning Volumes				389				1,557		435	1,273	
Vested Trips Turning Volumes												
Project Turning Volumes	0	0	0	22	0	584	0	43	762	11	30	0
2026 Background + Vested	0	0	0	614	0	584	0	3,357	762	728	2,445	0
2026 Background + Project	0	0	0	247	0	584	0	1,843	762	304	1,202	0
2026 Background + Vested + Project	0	0	0	636	0	584	0	3,400	762	739	2,475	0

SYNCHRO SUMMARY SHEETS

**2026 AM PK HR BACKGROUND +
VESTED TRAFFIC CONDITIONS**

Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗	↘	↖	↗	↘		↖	↗		↖	↗
Traffic Vol, veh/h	13	2864	54	7	3400	2	37	0	26	1	0	1
Future Vol, veh/h	13	2864	54	7	3400	2	37	0	26	1	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	300	-	515	410	-	-	-	-	0	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3
Mvmt Flow	13	2864	54	7	3400	2	37	0	26	1	0	1

Major/Minor	Major1		Major2		Minor1		Minor2					
Conflicting Flow All	3402	0	0	2918	0	0	4604	6306	1432	4873	6359	1701
Stage 1	-	-	-	-	-	-	2890	2890	-	3415	3415	-
Stage 2	-	-	-	-	-	-	1714	3416	-	1458	2944	-
Critical Hdwy	4.16	-	-	4.16	-	-	7.56	6.56	6.96	7.56	6.56	6.96
Critical Hdwy Stg 1	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Follow-up Hdwy	2.23	-	-	2.23	-	-	3.53	4.03	3.33	3.53	4.03	3.33
Pot Cap-1 Maneuver	76	-	-	120	-	-	0	0	122	0	0	80
Stage 1	-	-	-	-	-	-	~ 16	35	-	7	18	-
Stage 2	-	-	-	-	-	-	93	18	-	135	32	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	76	-	-	120	-	-	0	0	122	0	0	80
Mov Cap-2 Maneuver	-	-	-	-	-	-	0	0	-	0	0	-
Stage 1	-	-	-	-	-	-	~ 13	29	-	6	17	-
Stage 2	-	-	-	-	-	-	86	17	-	88	27	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.3	0.1		51.2
HCM LOS				F

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	122	76	-	-	120	-	-	80
HCM Lane V/C Ratio	-	0.213	0.171	-	-	0.058	-	-	0.025
HCM Control Delay (s)	-	42.3	61.9	-	-	36.9	-	-	51.2
HCM Lane LOS	-	E	F	-	-	E	-	-	F
HCM 95th %tile Q(veh)	-	0.8	0.6	-	-	0.2	-	-	0.1

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	0.6											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕	↗		↕↕			↕			↕	
Traffic Vol, veh/h	1	3067	59	5	3621	0	48	0	6	0	0	1
Future Vol, veh/h	1	3067	59	5	3621	0	48	0	6	0	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	205	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3
Mvmt Flow	1	3067	59	5	3621	0	48	0	6	0	0	1

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	3621	0	0	3126	0	0	4890	6700	1534	5167	6759	1811
Stage 1	-	-	-	-	-	-	3069	3069	-	3631	3631	-
Stage 2	-	-	-	-	-	-	1821	3631	-	1536	3128	-
Critical Hdwy	4.16	-	-	4.16	-	-	7.56	6.56	6.96	7.56	6.56	6.96
Critical Hdwy Stg 1	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Follow-up Hdwy	2.23	-	-	2.23	-	-	3.53	4.03	3.33	3.53	4.03	3.33
Pot Cap-1 Maneuver	62	-	-	99	-	-	0	0	104	0	0	67
Stage 1	-	-	-	-	-	-	~12	28	-	5	14	-
Stage 2	-	-	-	-	-	-	79	14	-	120	26	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	62	-	-	99	-	-	0	0	104	0	0	67
Mov Cap-2 Maneuver	-	-	-	-	-	-	0	0	-	0	0	-
Stage 1	-	-	-	-	-	-	~12	28	-	5	14	-
Stage 2	-	-	-	-	-	-	78	14	-	113	26	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0	0.1	72.1	59.5
HCM LOS			F	F



















Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	104	62	-	-	99	-	-	67
HCM Lane V/C Ratio	0.519	0.016	-	-	0.051	-	-	0.015
HCM Control Delay (s)	72.1	64	0	-	43.3	0	-	59.5
HCM Lane LOS	F	F	A	-	E	A	-	F
HCM 95th %tile Q(veh)	2.4	0	-	-	0.2	-	-	0

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Lanes, Volumes, Timings
5405: I75 NB Ent & Bonita Bch

2026 AM Pk Hr Background + Vested

11/08/2021

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	496	2370	0	0	3038	652	992	0	495	0	0	0
Future Volume (vph)	496	2370	0	0	3038	652	992	0	495	0	0	0
Ideal Flow (vphpl)	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750
Storage Length (ft)	525		0	650		375	830		700	0		0
Storage Lanes	0		0	1		1	2		1	0		0
Taper Length (ft)	25			25			100			25		
Lane Util. Factor	0.97	0.95	1.00	1.00	0.91	1.00	0.97	1.00	0.88	1.00	1.00	1.00
Frnt						0.850			0.850			
Flt Protected	0.950						0.950					
Satd. Flow (prot)	3131	3228	0	0	4638	1444	3131	0	2542	0	0	0
Flt Permitted	0.950						0.950					
Satd. Flow (perm)	3131	3228	0	0	4638	1444	3131	0	2542	0	0	0
Right Turn on Red			Yes			Yes			No			Yes
Satd. Flow (RTOR)						297						
Link Speed (mph)		45			45			45			45	
Link Distance (ft)		724			797			860			629	
Travel Time (s)		11.0			12.1			13.0			9.5	
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj. Flow (vph)	496	2370	0	0	3038	652	992	0	495	0	0	0
Shared Lane Traffic (%)												
Lane Group Flow (vph)	496	2370	0	0	3038	652	992	0	495	0	0	0
Turn Type	Prot	NA			NA	Perm	Prot		Prot			
Protected Phases	1	6			2		3		3			
Permitted Phases						2	3					
Detector Phase	1	6			2	2	3		3			
Switch Phase												
Minimum Initial (s)	7.0	15.0			15.0	15.0	7.0		7.0			
Minimum Split (s)	14.2	21.7			21.7	21.7	13.5		13.5			
Total Split (s)	56.0	98.0			42.0	42.0	52.0		52.0			
Total Split (%)	37.3%	65.3%			28.0%	28.0%	34.7%		34.7%			
Maximum Green (s)	48.8	91.3			35.3	35.3	45.5		45.5			
Yellow Time (s)	4.7	4.7			4.7	4.7	4.0		4.0			
All-Red Time (s)	2.5	2.0			2.0	2.0	2.5		2.5			
Lost Time Adjust (s)	-3.5	-3.5			-3.5	-1.0	-3.0		-1.0			
Total Lost Time (s)	3.7	3.2			3.2	5.7	3.5		5.5			
Lead/Lag	Lag				Lead	Lead						
Lead-Lag Optimize?	Yes				Yes	Yes						
Vehicle Extension (s)	3.0	3.0			3.0	3.0	5.0		5.0			
Recall Mode	C-Max	C-Max			Max	Max	None		None			
Act Effct Green (s)	52.3	94.8			38.8	36.3	48.5		46.5			
Actuated g/C Ratio	0.35	0.63			0.26	0.24	0.32		0.31			
v/c Ratio	0.45	1.16			2.53	1.14	0.98		0.63			
Control Delay	26.4	94.2			715.7	108.3	73.8		48.6			
Queue Delay	0.0	0.2			1.4	0.0	663.5		0.0			
Total Delay	26.4	94.4			717.1	108.3	737.3		48.6			
LOS	C	F			F	F	F		D			
Approach Delay		82.6			609.5		508.0					
Approach LOS		F			F		F					

Lanes, Volumes, Timings
5405: I75 NB Ent & Bonita Bch

2026 AM Pk Hr Background + Vested
11/08/2021



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Length 50th (ft)	188	~1441			~1811	~507	496		236			
Queue Length 95th (ft)	m193	m#1487			#1883	#754	#642		306			
Internal Link Dist (ft)		644			717			780			549	
Turn Bay Length (ft)	525					375	830		700			
Base Capacity (vph)	1091	2040			1199	574	1012		788			
Starvation Cap Reductn	0	146			0	0	0		0			
Spillback Cap Reductn	0	0			315	0	1012		0			
Storage Cap Reductn	0	0			0	0	0		0			
Reduced v/c Ratio	0.45	1.25			3.44	1.14	992.00		0.63			

Intersection Summary

Area Type: Other

Cycle Length: 150

Actuated Cycle Length: 150

Offset: 56 (37%), Referenced to phase 1:EBL and 6:EBT, Start of Green

Natural Cycle: 150

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 2.53

Intersection Signal Delay: 403.0

Intersection LOS: F

Intersection Capacity Utilization 132.7%

ICU Level of Service H

Analysis Period (min) 15

~ Volume exceeds capacity, queue is theoretically infinite.

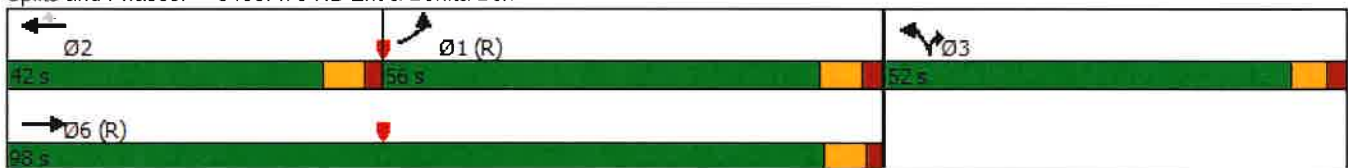
Queue shown is maximum after two cycles.

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 5405: I75 NB Ent & Bonita Bch



Lanes, Volumes, Timings
5404: I75 SB Ent/I-75 SB Ent & Bonita Bch

2026 AM Pk Hr Background + Vested
11/08/2021

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑↑↑↑	↗↗	↘↘	↑↑					↗↗		↘↘
Traffic Volume (vph)	0	2181	744	798	3400	0	0	0	0	774	0	850
Future Volume (vph)	0	2181	744	798	3400	0	0	0	0	774	0	850
Ideal Flow (vphpl)	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750
Storage Length (ft)	0		590	525		0	0		0	600		375
Storage Lanes	0		2	2		0	0		0	2		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	0.86	0.88	0.97	0.95	1.00	1.00	1.00	1.00	0.97	1.00	0.76
Frt			0.850									0.850
Flt Protected				0.950						0.950		
Satd. Flow (prot)	0	5845	2542	3131	3228	0	0	0	0	3131	0	3293
Flt Permitted				0.950						0.950		
Satd. Flow (perm)	0	5845	2542	3131	3228	0	0	0	0	3131	0	3293
Right Turn on Red			No			Yes			Yes			No
Satd. Flow (RTOR)												
Link Speed (mph)		45			45			45			45	
Link Distance (ft)		785			724			1234			1189	
Travel Time (s)		11.9			11.0			18.7			18.0	
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj. Flow (vph)	0	2181	744	798	3400	0	0	0	0	774	0	850
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	2181	744	798	3400	0	0	0	0	774	0	850
Turn Type		NA	Perm	Prot	NA					Prot		Prot
Protected Phases		6		5	2					3		8
Permitted Phases			6							3		
Detector Phase		6	6	5	2					3		8
Switch Phase												
Minimum Initial (s)		20.0	20.0	7.0	20.0					7.0		7.0
Minimum Split (s)		26.7	26.7	13.5	26.7					13.5		13.5
Total Split (s)		70.0	70.0	42.0	112.0					38.0		38.0
Total Split (%)		46.7%	46.7%	28.0%	74.7%					25.3%		25.3%
Maximum Green (s)		63.3	63.3	35.5	105.3					31.5		31.5
Yellow Time (s)		4.7	4.7	4.0	4.7					4.0		4.0
All-Red Time (s)		2.0	2.0	2.5	2.0					2.5		2.5
Lost Time Adjust (s)		-3.0	-1.0	-1.5	-3.0					-3.0		-3.0
Total Lost Time (s)		3.7	5.7	5.0	3.7					3.5		3.5
Lead/Lag		Lag	Lag	Lead								
Lead-Lag Optimize?		Yes	Yes	Yes								
Vehicle Extension (s)		3.0	3.0	3.0	3.0					3.0		3.0
Recall Mode		Max	Max	C-Max	C-Max					None		None
Walk Time (s)		5.0	5.0		5.0							
Flash Dont Walk (s)		11.0	11.0		11.0							
Pedestrian Calls (#/hr)		0	0		0							
Act Effct Green (s)		66.3	64.3	37.0	108.3					34.5		34.5
Actuated g/C Ratio		0.44	0.43	0.25	0.72					0.23		0.23
v/c Ratio		0.84	0.68	1.03	1.46					1.07		1.12
Control Delay		24.5	23.0	93.6	233.2					109.0		123.4
Queue Delay		5.6	0.0	0.0	0.4					0.0		0.0
Total Delay		30.1	23.0	93.6	233.7					109.0		123.4



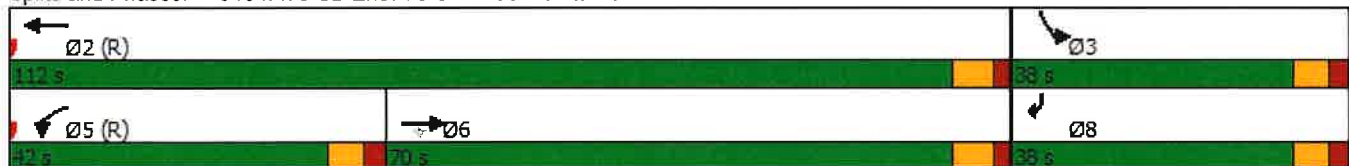
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
LOS		C	C	F	F					F		F
Approach Delay		28.3			207.0						116.5	
Approach LOS		C			F						F	
Queue Length 50th (ft)		365	170	~434	~2451					~432		~419
Queue Length 95th (ft)		358	231	m215	m325					#562		#532
Internal Link Dist (ft)		705			644			1154			1109	
Turn Bay Length (ft)			590	525						600		375
Base Capacity (vph)		2583	1089	772	2330					720		757
Starvation Cap Reductn		0	0	0	359					0		0
Spillback Cap Reductn		357	0	0	0					0		0
Storage Cap Reductn		0	0	0	0					0		0
Reduced v/c Ratio		0.98	0.68	1.03	1.73					1.07		1.12

Intersection Summary

Area Type: Other
 Cycle Length: 150
 Actuated Cycle Length: 150
 Offset: 123 (82%), Referenced to phase 2:WBT and 5:WBL, Start of Green
 Natural Cycle: 130
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.46
 Intersection Signal Delay: 130.5
 Intersection Capacity Utilization 132.7%
 Analysis Period (min) 15
 Intersection LOS: F
 ICU Level of Service H

~ Volume exceeds capacity, queue is theoretically infinite.
 Queue shown is maximum after two cycles.
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.
 m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 5404: I75 SB Ent/I-75 SB Ent & Bonita Bch



**2026 PM PK HR BACKGROUND +
VESTED TRAFFIC CONDITIONS**

Intersection

Int Delay, s/veh 0.2

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↗↗	↗	↘	↗↗			↗	↗		↕	
Traffic Vol, veh/h	3	3422	33	11	3175	0	54	0	28	3	0	14
Future Vol, veh/h	3	3422	33	11	3175	0	54	0	28	3	0	14
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	300	-	515	410	-	-	-	-	0	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3
Mvmt Flow	3	3422	33	11	3175	0	54	0	28	3	0	14

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	3175	0	0	3455
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	4.16	-	-	4.16
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	2.23	-	-	2.23
Pot Cap-1 Maneuver	94	-	-	72
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	94	-	-	72
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0	0.2		50.4
HCM LOS			-	F

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	79	94	-	-	72	-	-	96
HCM Lane V/C Ratio	-	0.354	0.032	-	-	0.153	-	-	0.177
HCM Control Delay (s)	-	73.7	44.6	-	-	63.8	-	-	50.4
HCM Lane LOS	-	F	E	-	-	F	-	-	F
HCM 95th %tile Q(veh)	-	1.4	0.1	-	-	0.5	-	-	0.6

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	2.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕	↗		↕↕			↕↕			↕↕	
Traffic Vol, veh/h	0	3528	60	0	2994	0	66	0	6	0	0	0
Future Vol, veh/h	0	3528	60	0	2994	0	66	0	6	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	205	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3
Mvmt Flow	0	3528	60	0	2994	0	66	0	6	0	0	0

Major/Minor	Major1		Major2		Minor1		Minor2					
Conflicting Flow All	2994	0	0	3588	0	0	5025	6522	1764	4758	6582	1497
Stage 1	-	-	-	-	-	-	3528	3528	-	2994	2994	-
Stage 2	-	-	-	-	-	-	1497	2994	-	1764	3588	-
Critical Hdwy	4.16	-	-	4.16	-	-	7.56	6.56	6.96	7.56	6.56	6.96
Critical Hdwy Stg 1	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Follow-up Hdwy	2.23	-	-	2.23	-	-	3.53	4.03	3.33	3.53	4.03	3.33
Pot Cap-1 Maneuver	112	-	-	64	-	-	0	0	72	0	0	111
Stage 1	-	-	-	-	-	-	~6	15	-	14	30	-
Stage 2	-	-	-	-	-	-	127	30	-	86	14	-
Platoon blocked, %		-	-	-	-	-						
Mov Cap-1 Maneuver	112	-	-	64	-	-	0	0	72	0	0	111
Mov Cap-2 Maneuver	-	-	-	-	-	-	0	0	-	0	0	-
Stage 1	-	-	-	-	-	-	~6	15	-	14	30	-
Stage 2	-	-	-	-	-	-	127	30	-	79	14	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0	0	205	0
HCM LOS			F	A

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	72	112	-	-	64	-	-	-
HCM Lane V/C Ratio	1	-	-	-	-	-	-	-
HCM Control Delay (s)	205	0	-	-	0	-	-	0
HCM Lane LOS	F	A	-	-	A	-	-	A
HCM 95th %tile Q(veh)	5.2	0	-	-	0	-	-	-

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Lanes, Volumes, Timings
5405: I75 NB Ent & Bonita Bch

2026 PM Pk Hr Background + Vested
11/08/2021

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	883	2892	0	0	2635	633	473	0	723	0	0	0
Future Volume (vph)	883	2892	0	0	2635	633	473	0	723	0	0	0
Ideal Flow (vphpl)	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750
Storage Length (ft)	525		0	650		375	830		700	0		0
Storage Lanes	0		0	1		1	2		1	0		0
Taper Length (ft)	25			25			100			25		
Lane Util. Factor	0.97	0.95	1.00	1.00	0.91	1.00	0.97	1.00	0.88	1.00	1.00	1.00
Frnt						0.850			0.850			
Flt Protected	0.950						0.950					
Satd. Flow (prot)	3131	3228	0	0	4638	1444	3131	0	2542	0	0	0
Flt Permitted	0.950						0.950					
Satd. Flow (perm)	3131	3228	0	0	4638	1444	3131	0	2542	0	0	0
Right Turn on Red			Yes			Yes			No			Yes
Satd. Flow (RTOR)						332						
Link Speed (mph)		45			45			45			45	
Link Distance (ft)		724			797			860			629	
Travel Time (s)		11.0			12.1			13.0			9.5	
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj. Flow (vph)	883	2892	0	0	2635	633	473	0	723	0	0	0
Shared Lane Traffic (%)												
Lane Group Flow (vph)	883	2892	0	0	2635	633	473	0	723	0	0	0
Turn Type	Prot	NA			NA	Perm	Prot		Prot			
Protected Phases	1	6			2		3		3			
Permitted Phases						2	3					
Detector Phase	1	6			2	2	3		3			
Switch Phase												
Minimum Initial (s)	7.0	15.0			15.0	15.0	7.0		7.0			
Minimum Split (s)	14.2	21.7			21.7	21.7	13.5		13.5			
Total Split (s)	56.0	98.0			42.0	42.0	52.0		52.0			
Total Split (%)	37.3%	65.3%			28.0%	28.0%	34.7%		34.7%			
Maximum Green (s)	48.8	91.3			35.3	35.3	45.5		45.5			
Yellow Time (s)	4.7	4.7			4.7	4.7	4.0		4.0			
All-Red Time (s)	2.5	2.0			2.0	2.0	2.5		2.5			
Lost Time Adjust (s)	-3.5	-3.5			-3.5	-1.0	-3.0		-1.0			
Total Lost Time (s)	3.7	3.2			3.2	5.7	3.5		5.5			
Lead/Lag	Lag				Lead	Lead						
Lead-Lag Optimize?	Yes				Yes	Yes						
Vehicle Extension (s)	3.0	3.0			3.0	3.0	5.0		5.0			
Recall Mode	C-Max	C-Max			Max	Max	None		None			
Act Effct Green (s)	52.3	95.0			39.0	36.5	48.3		46.3			
Actuated g/C Ratio	0.35	0.63			0.26	0.24	0.32		0.31			
v/c Ratio	0.81	1.41			2.18	1.05	0.47		0.92			
Control Delay	24.9	206.0			562.6	76.1	42.5		68.1			
Queue Delay	0.0	0.2			0.4	0.0	5.5		0.0			
Total Delay	24.9	206.2			563.0	76.1	47.9		68.1			
LOS	C	F			F	E	D		E			
Approach Delay		163.8			468.7			60.1				
Approach LOS		F			F			E				

Lanes, Volumes, Timings
5405: I75 NB Ent & Bonita Bch

2026 PM Pk Hr Background + Vested
11/08/2021



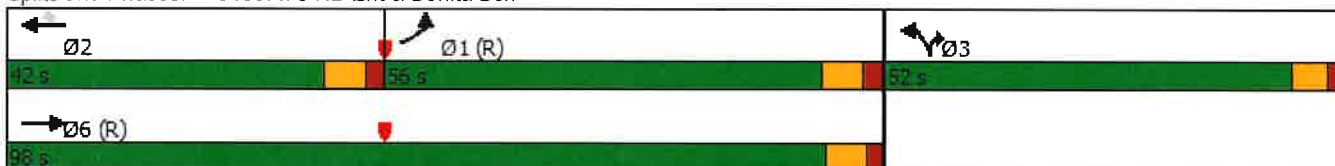
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Length 50th (ft)	408	~2021			~1512	~413	190		388			
Queue Length 95th (ft)	m287	m#1429			#1593	#658	244		#521			
Internal Link Dist (ft)		644			717			780			549	
Turn Bay Length (ft)	525					375	830		700			
Base Capacity (vph)	1091	2045			1206	603	1012		788			
Starvation Cap Reductn	0	168			0	0	0		0			
Spillback Cap Reductn	0	0			111	0	467		0			
Storage Cap Reductn	0	0			0	0	0		0			
Reduced v/c Ratio	0.81	1.54			2.41	1.05	0.87		0.92			

Intersection Summary

Area Type: Other
 Cycle Length: 150
 Actuated Cycle Length: 150
 Offset: 56 (37%), Referenced to phase 1:EBL and 6:EBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 2.18
 Intersection Signal Delay: 269.7
 Intersection Capacity Utilization 122.2%
 Analysis Period (min) 15
 Intersection LOS: F
 ICU Level of Service H

~ Volume exceeds capacity, queue is theoretically infinite.
 Queue shown is maximum after two cycles.
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.
 m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 5405: I75 NB Ent & Bonita Bch



Lanes, Volumes, Timings
5404: I75 SB Ent/I-75 SB Ent & Bonita Bch

2026 PM Pk Hr Background + Vested
11/08/2021

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑↑↑↑	↗↗	↘↘	↑↑					↖↖		↗↗
Traffic Volume (vph)	0	3357	762	728	2445	0	0	0	0	614	0	584
Future Volume (vph)	0	3357	762	728	2445	0	0	0	0	614	0	584
Ideal Flow (vphpl)	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750
Storage Length (ft)	0		590	525		0	0		0	600		375
Storage Lanes	0		2	2		0	0		0	2		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	0.86	0.88	0.97	0.95	1.00	1.00	1.00	1.00	0.97	1.00	0.76
Frnt			0.850									0.850
Flt Protected				0.950						0.950		
Satd. Flow (prot)	0	5845	2542	3131	3228	0	0	0	0	3131	0	3293
Flt Permitted				0.950						0.950		
Satd. Flow (perm)	0	5845	2542	3131	3228	0	0	0	0	3131	0	3293
Right Turn on Red			No			Yes			Yes			No
Satd. Flow (RTOR)												
Link Speed (mph)		45			45			45			45	
Link Distance (ft)		785			724			1234			1189	
Travel Time (s)		11.9			11.0			18.7			18.0	
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj. Flow (vph)	0	3357	762	728	2445	0	0	0	0	614	0	584
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	3357	762	728	2445	0	0	0	0	614	0	584
Turn Type		NA	Perm	Prot	NA					Prot		Prot
Protected Phases		6		5	2					3		8
Permitted Phases			6							3		
Detector Phase		6	6	5	2					3		8
Switch Phase												
Minimum Initial (s)		20.0	20.0	7.0	20.0					7.0		7.0
Minimum Split (s)		26.7	26.7	13.5	26.7					13.5		13.5
Total Split (s)		70.0	70.0	42.0	112.0					38.0		38.0
Total Split (%)		46.7%	46.7%	28.0%	74.7%					25.3%		25.3%
Maximum Green (s)		63.3	63.3	35.5	105.3					31.5		31.5
Yellow Time (s)		4.7	4.7	4.0	4.7					4.0		4.0
All-Red Time (s)		2.0	2.0	2.5	2.0					2.5		2.5
Lost Time Adjust (s)		-3.0	-1.0	-1.5	-3.0					-3.0		-3.0
Total Lost Time (s)		3.7	5.7	5.0	3.7					3.5		3.5
Lead/Lag		Lag	Lag	Lead								
Lead-Lag Optimize?		Yes	Yes	Yes								
Vehicle Extension (s)		3.0	3.0	3.0	3.0					3.0		3.0
Recall Mode		Max	Max	C-Max	C-Max					None		None
Walk Time (s)		5.0	5.0		5.0							
Flash Dont Walk (s)		11.0	11.0		11.0							
Pedestrian Calls (#/hr)		0	0		0							
Act Effct Green (s)		66.3	64.3	37.8	109.1					33.7		33.7
Actuated g/C Ratio		0.44	0.43	0.25	0.73					0.22		0.22
v/c Ratio		1.30	0.70	0.92	1.04					0.87		0.79
Control Delay		166.4	27.9	83.5	58.4					70.1		63.3
Queue Delay		0.1	0.0	0.0	24.3					46.1		0.0
Total Delay		166.5	27.9	83.5	82.6					116.1		63.3

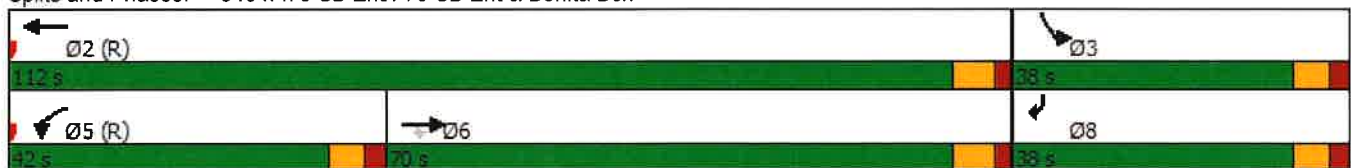


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
LOS		F	C	F	F					F		E
Approach Delay		140.8			82.8						90.4	
Approach LOS		F			F						F	
Queue Length 50th (ft)		~1231	239	387	~739					299		236
Queue Length 95th (ft)		#1262	318	m209	m167					#390		296
Internal Link Dist (ft)		705			644			1154			1109	
Turn Bay Length (ft)			590	525						600		375
Base Capacity (vph)		2583	1089	788	2346					720		757
Starvation Cap Reductn		0	0	0	370					0		0
Spillback Cap Reductn		144	0	0	0					157		0
Storage Cap Reductn		0	0	0	0					0		0
Reduced v/c Ratio		1.38	0.70	0.92	1.24					1.09		0.77

Intersection Summary

Area Type: Other
 Cycle Length: 150
 Actuated Cycle Length: 150
 Offset: 123 (82%), Referenced to phase 2:WBT and 5:WBL, Start of Green
 Natural Cycle: 140
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.30
 Intersection Signal Delay: 112.0 Intersection LOS: F
 Intersection Capacity Utilization 122.2% ICU Level of Service H
 Analysis Period (min) 15
 ~ Volume exceeds capacity, queue is theoretically infinite.
 Queue shown is maximum after two cycles.
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.
 m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 5404: I75 SB Ent/I-75 SB Ent & Bonita Bch



**2026 AM PK HR BACKGROUND +
VESTED + PROJECT TRAFFIC
CONDITIONS**

Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗	↘	↖	↗	↘						
Traffic Vol, veh/h	13	2885	54	7	3409	2	37	0	23	1	0	1
Future Vol, veh/h	13	2885	54	7	3409	2	37	0	23	1	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	300	-	515	410	-	-	-	-	0	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3
Mvmt Flow	13	2885	54	7	3409	2	37	0	23	1	0	1

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	3411	0	0	2939	0	0	4630	6336	1443	4893	6389	1706
Stage 1	-	-	-	-	-	-	2911	2911	-	3424	3424	-
Stage 2	-	-	-	-	-	-	1719	3425	-	1469	2965	-
Critical Hdwy	4.16	-	-	4.16	-	-	7.56	6.56	6.96	7.56	6.56	6.96
Critical Hdwy Stg 1	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Follow-up Hdwy	2.23	-	-	2.23	-	-	3.53	4.03	3.33	3.53	4.03	3.33
Pot Cap-1 Maneuver	75	-	-	117	-	-	0	0	120	0	0	79
Stage 1	-	-	-	-	-	-	~15	34	-	7	18	-
Stage 2	-	-	-	-	-	-	92	18	-	132	32	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	75	-	-	117	-	-	0	0	120	0	0	79
Mov Cap-2 Maneuver	-	-	-	-	-	-	0	0	-	0	0	-
Stage 1	-	-	-	-	-	-	~12	28	-	6	17	-
Stage 2	-	-	-	-	-	-	85	17	-	88	26	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.3	0.1		51.8
HCM LOS			-	F

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	120	75	-	-	117	-	-	79
HCM Lane V/C Ratio	-	0.192	0.173	-	-	0.06	-	-	0.025
HCM Control Delay (s)	-	42	62.8	-	-	37.7	-	-	51.8
HCM Lane LOS	-	E	F	-	-	E	-	-	F
HCM 95th %tile Q(veh)	-	0.7	0.6	-	-	0.2	-	-	0.1

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	4.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕	↗		↕↕			↕			↕	
Traffic Vol, veh/h	31	3065	59	5	3619	11	48	0	6	23	0	67
Future Vol, veh/h	31	3065	59	5	3619	11	48	0	6	23	0	67
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	205	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3
Mvmt Flow	31	3065	59	5	3619	11	48	0	6	23	0	67

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	3630	0	0	3124	0	0	4947	6767	1533	5230	6821	1815
Stage 1	-	-	-	-	-	-	3127	3127	-	3635	3635	-
Stage 2	-	-	-	-	-	-	1820	3640	-	1595	3186	-
Critical Hdwy	4.16	-	-	4.16	-	-	7.56	6.56	6.96	7.56	6.56	6.96
Critical Hdwy Stg 1	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Follow-up Hdwy	2.23	-	-	2.23	-	-	3.53	4.03	3.33	3.53	4.03	3.33
Pot Cap-1 Maneuver	61	-	-	99	-	-	0	0	104	0	0	67
Stage 1	-	-	-	-	-	-	~ 11	26	-	~ 5	13	-
Stage 2	-	-	-	-	-	-	79	13	-	110	24	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	61	-	-	99	-	-	-	0	104	0	0	67
Mov Cap-2 Maneuver	-	-	-	-	-	-	-	0	-	0	0	-
Stage 1	-	-	-	-	-	-	~ 11	26	-	~ 5	13	-
Stage 2	-	-	-	-	-	-	-	13	-	104	24	-



















Approach	EB	WB	NB	SB
HCM Control Delay, s	1.1	0.1		\$ 332
HCM LOS			-	F

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	61	-	-	99	-	-	67
HCM Lane V/C Ratio	-	0.508	-	-	0.051	-	-	1.343
HCM Control Delay (s)	-	113.8	0	-	43.3	0	-	\$ 332
HCM Lane LOS	-	F	A	-	E	A	-	F
HCM 95th %tile Q(veh)	-	2	-	-	0.2	-	-	7.4

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Lanes, Volumes, Timings
5405: I75 NB Ent & Bonita Bch

2026 AM Pk Hr Background + Vested + Project
11/08/2021

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	496	2392	0	0	3085	669	992	0	501	0	0	0
Future Volume (vph)	496	2392	0	0	3085	669	992	0	501	0	0	0
Ideal Flow (vphpl)	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750
Storage Length (ft)	525		0	650		375	830		700	0		0
Storage Lanes	0		0	1		1	2		1	0		0
Taper Length (ft)	25			25			100			25		
Lane Util. Factor	0.97	0.95	1.00	1.00	0.91	1.00	0.97	1.00	0.88	1.00	1.00	1.00
Frnt						0.850			0.850			
Flt Protected	0.950						0.950					
Satd. Flow (prot)	3131	3228	0	0	4638	1444	3131	0	2542	0	0	0
Flt Permitted	0.950						0.950					
Satd. Flow (perm)	3131	3228	0	0	4638	1444	3131	0	2542	0	0	0
Right Turn on Red			Yes			Yes			No			Yes
Satd. Flow (RTOR)						300						
Link Speed (mph)		45			45			45			45	
Link Distance (ft)		724			797			860			629	
Travel Time (s)		11.0			12.1			13.0			9.5	
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj. Flow (vph)	496	2392	0	0	3085	669	992	0	501	0	0	0
Shared Lane Traffic (%)												
Lane Group Flow (vph)	496	2392	0	0	3085	669	992	0	501	0	0	0
Turn Type	Prot	NA			NA	Perm	Prot		Prot			
Protected Phases	1	6			2		3		3			
Permitted Phases						2	3					
Detector Phase	1	6			2	2	3		3			
Switch Phase												
Minimum Initial (s)	7.0	15.0			15.0	15.0	7.0		7.0			
Minimum Split (s)	14.2	21.7			21.7	21.7	13.5		13.5			
Total Split (s)	56.0	98.0			42.0	42.0	52.0		52.0			
Total Split (%)	37.3%	65.3%			28.0%	28.0%	34.7%		34.7%			
Maximum Green (s)	48.8	91.3			35.3	35.3	45.5		45.5			
Yellow Time (s)	4.7	4.7			4.7	4.7	4.0		4.0			
All-Red Time (s)	2.5	2.0			2.0	2.0	2.5		2.5			
Lost Time Adjust (s)	-3.5	-3.5			-3.5	-1.0	-3.0		-1.0			
Total Lost Time (s)	3.7	3.2			3.2	5.7	3.5		5.5			
Lead/Lag	Lag				Lead	Lead						
Lead-Lag Optimize?	Yes				Yes	Yes						
Vehicle Extension (s)	3.0	3.0			3.0	3.0	5.0		5.0			
Recall Mode	C-Max	C-Max			Max	Max	None		None			
Act Effct Green (s)	52.3	94.8			38.8	36.3	48.5		46.5			
Actuated g/C Ratio	0.35	0.63			0.26	0.24	0.32		0.31			
v/c Ratio	0.45	1.17			2.57	1.16	0.98		0.64			
Control Delay	26.3	98.9			732.9	117.9	73.8		48.9			
Queue Delay	0.0	0.2			1.5	0.0	663.5		0.0			
Total Delay	26.3	99.1			734.4	117.9	737.3		48.9			
LOS	C	F			F	F	F		D			
Approach Delay		86.6			624.6		506.3					
Approach LOS		F			F		F					



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Length 50th (ft)	187	~1465			~1846	~538	496		240			
Queue Length 95th (ft)	m189	m#1503			#1917	#784	#642		310			
Internal Link Dist (ft)		644			717			780			549	
Turn Bay Length (ft)	525					375	830		700			
Base Capacity (vph)	1091	2040			1199	576	1012		788			
Starvation Cap Reductn	0	146			0	0	0		0			
Spillback Cap Reductn	0	0			328	0	1012		0			
Storage Cap Reductn	0	0			0	0	0		0			
Reduced v/c Ratio	0.45	1.26			3.54	1.16	992.00		0.64			

Intersection Summary

Area Type: Other

Cycle Length: 150

Actuated Cycle Length: 150

Offset: 56 (37%), Referenced to phase 1:EBL and 6:EBT, Start of Green

Natural Cycle: 150

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 2.57

Intersection Signal Delay: 411.9

Intersection LOS: F

Intersection Capacity Utilization 133.9%

ICU Level of Service H

Analysis Period (min) 15

~ Volume exceeds capacity, queue is theoretically infinite.

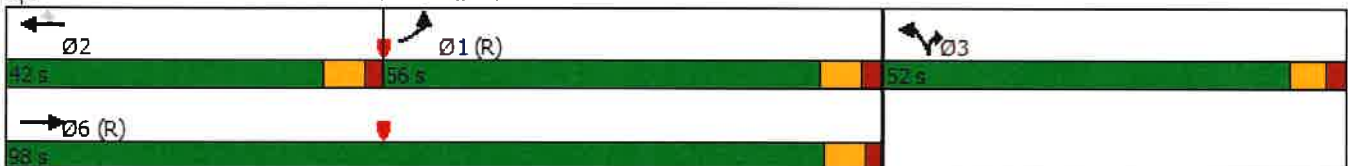
Queue shown is maximum after two cycles.

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 5405: I75 NB Ent & Bonita Bch



Lanes, Volumes, Timings
5404: I75 SB Ent/I-75 SB Ent & Bonita Bch

2026 AM Pk Hr Background + Vested + Project
11/08/2021

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑↑↑↑	↗↗	↖↖	↑↑					↘↘		↗↗
Traffic Volume (vph)	0	2196	744	811	3434	0	0	0	0	781	0	850
Future Volume (vph)	0	2196	744	811	3434	0	0	0	0	781	0	850
Ideal Flow (vphpl)	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750
Storage Length (ft)	0		590	525		0	0		0	600		375
Storage Lanes	0		2	2		0	0		0	2		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	0.86	0.88	0.97	0.95	1.00	1.00	1.00	1.00	0.97	1.00	0.76
Frnt			0.850									0.850
Fit Protected				0.950						0.950		
Satd. Flow (prot)	0	5845	2542	3131	3228	0	0	0	0	3131	0	3293
Fit Permitted				0.950						0.950		
Satd. Flow (perm)	0	5845	2542	3131	3228	0	0	0	0	3131	0	3293
Right Turn on Red			No			Yes			Yes			No
Satd. Flow (RTOR)												
Link Speed (mph)		45			45			45			45	
Link Distance (ft)		785			724			1234			1189	
Travel Time (s)		11.9			11.0			18.7			18.0	
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj. Flow (vph)	0	2196	744	811	3434	0	0	0	0	781	0	850
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	2196	744	811	3434	0	0	0	0	781	0	850
Turn Type		NA	Perm	Prot	NA					Prot		Prot
Protected Phases		6		5	2					3		8
Permitted Phases			6							3		
Detector Phase		6	6	5	2					3		8
Switch Phase												
Minimum Initial (s)		20.0	20.0	7.0	20.0					7.0		7.0
Minimum Split (s)		26.7	26.7	13.5	26.7					13.5		13.5
Total Split (s)		70.0	70.0	42.0	112.0					38.0		38.0
Total Split (%)		46.7%	46.7%	28.0%	74.7%					25.3%		25.3%
Maximum Green (s)		63.3	63.3	35.5	105.3					31.5		31.5
Yellow Time (s)		4.7	4.7	4.0	4.7					4.0		4.0
All-Red Time (s)		2.0	2.0	2.5	2.0					2.5		2.5
Lost Time Adjust (s)		-3.0	-1.0	-1.5	-3.0					-3.0		-3.0
Total Lost Time (s)		3.7	5.7	5.0	3.7					3.5		3.5
Lead/Lag		Lag	Lag	Lead								
Lead-Lag Optimize?		Yes	Yes	Yes								
Vehicle Extension (s)		3.0	3.0	3.0	3.0					3.0		3.0
Recall Mode		Max	Max	C-Max	C-Max					None		None
Walk Time (s)		5.0	5.0		5.0							
Flash Dont Walk (s)		11.0	11.0		11.0							
Pedestrian Calls (#/hr)		0	0		0							
Act Effct Green (s)		66.3	64.3	37.0	108.3					34.5		34.5
Actuated g/C Ratio		0.44	0.43	0.25	0.72					0.23		0.23
v/c Ratio		0.85	0.68	1.05	1.47					1.08		1.12
Control Delay		24.9	23.1	98.9	239.8					111.9		123.4
Queue Delay		6.9	0.0	0.0	0.4					0.0		0.0
Total Delay		31.8	23.1	98.9	240.2					111.9		123.4



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
LOS		C	C	F	F					F		F
Approach Delay		29.6			213.2						117.9	
Approach LOS		C			F						F	
Queue Length 50th (ft)		377	171	~447	~2487					~440		~419
Queue Length 95th (ft)		369	232	m217	m327					#569		#532
Internal Link Dist (ft)		705			644			1154			1109	
Turn Bay Length (ft)			590	525						600		375
Base Capacity (vph)		2583	1089	772	2330					720		757
Starvation Cap Reductn		0	0	0	359					0		0
Spillback Cap Reductn		361	0	0	0					0		0
Storage Cap Reductn		0	0	0	0					0		0
Reduced v/c Ratio		0.99	0.68	1.05	1.74					1.08		1.12

Intersection Summary

Area Type: Other
 Cycle Length: 150
 Actuated Cycle Length: 150
 Offset: 123 (82%), Referenced to phase 2:WBT and 5:WBL, Start of Green
 Natural Cycle: 130
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.47
 Intersection Signal Delay: 134.4
 Intersection Capacity Utilization 133.9%
 Analysis Period (min) 15
 ~ Volume exceeds capacity, queue is theoretically infinite.
 Queue shown is maximum after two cycles.
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.
 m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 5404: I75 SB Ent/I-75 SB Ent & Bonita Bch



**2026 PM PK HR BACKGROUND +
VESTED + PROJECT TRAFFIC
CONDITIONS**

Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗	↘	↖	↗	↘		↖	↗		↖	↗
Traffic Vol, veh/h	3	3441	33	11	3202	0	54	0	28	3	0	14
Future Vol, veh/h	3	3441	33	11	3202	0	54	0	28	3	0	14
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	300	-	515	410	-	-	-	-	0	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3
Mvmt Flow	3	3441	33	11	3202	0	54	0	28	3	0	14

Major/Minor	Major1		Major2		Minor1		Minor2					
Conflicting Flow All	3202	0	0	3474	0	0	5070	6671	1721	4951	6704	1601
Stage 1	-	-	-	-	-	-	3447	3447	-	3224	3224	-
Stage 2	-	-	-	-	-	-	1623	3224	-	1727	3480	-
Critical Hdwy	4.16	-	-	4.16	-	-	7.56	6.56	6.96	7.56	6.56	6.96
Critical Hdwy Stg 1	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Follow-up Hdwy	2.23	-	-	2.23	-	-	3.53	4.03	3.33	3.53	4.03	3.33
Pot Cap-1 Maneuver	92	-	-	71	-	-	0	0	78	0	0	94
Stage 1	-	-	-	-	-	-	~7	17	-	9	23	-
Stage 2	-	-	-	-	-	-	106	23	-	91	16	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	92	-	-	71	-	-	0	0	78	0	0	94
Mov Cap-2 Maneuver	-	-	-	-	-	-	0	0	-	0	0	-
Stage 1	-	-	-	-	-	-	~7	16	-	9	19	-
Stage 2	-	-	-	-	-	-	76	19	-	56	15	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0	0.2		51.6
HCM LOS			-	F

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	78	92	-	-	71	-	-	94
HCM Lane V/C Ratio	-	0.359	0.033	-	-	0.155	-	-	0.181
HCM Control Delay (s)	-	75	45.4	-	-	64.8	-	-	51.6
HCM Lane LOS	-	F	E	-	-	F	-	-	F
HCM 95th %tile Q(veh)	-	1.4	0.1	-	-	0.5	-	-	0.6

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection												
Int Delay, s/veh	4.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕	↕		↕↕			↕↕			↕↕	
Traffic Vol, veh/h	87	3522	60	0	2988	33	66	0	6	25	0	62
Future Vol, veh/h	87	3522	60	0	2988	33	66	0	6	25	0	62
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	205	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3
Mvmt Flow	87	3522	60	0	2988	33	66	0	6	25	0	62

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	3021	0	0	3582	0	0	5190	6717	1761	4940	6761	1511
Stage 1	-	-	-	-	-	-	3696	3696	-	3005	3005	-
Stage 2	-	-	-	-	-	-	1494	3021	-	1935	3756	-
Critical Hdwy	4.16	-	-	4.16	-	-	7.56	6.56	6.96	7.56	6.56	6.96
Critical Hdwy Stg 1	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Follow-up Hdwy	2.23	-	-	2.23	-	-	3.53	4.03	3.33	3.53	4.03	3.33
Pot Cap-1 Maneuver	109	-	-	64	-	-	0	0	73	0	0	108
Stage 1	-	-	-	-	-	-	~ 5	12	-	~ 13	30	-
Stage 2	-	-	-	-	-	-	128	29	-	67	12	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	109	-	-	64	-	-	0	0	73	0	0	108
Mov Cap-2 Maneuver	-	-	-	-	-	-	0	0	-	0	0	-
Stage 1	-	-	-	-	-	-	~ 5	12	-	~ 13	30	-
Stage 2	-	-	-	-	-	-	~ 55	29	-	61	12	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	2.6	0	199.2	112.9
HCM LOS			F	F

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	73	109	-	-	64	-	-	108
HCM Lane V/C Ratio	0.986	0.798	-	-	-	-	-	0.806
HCM Control Delay (s)	199.2	110.6	0	-	0	-	-	112.9
HCM Lane LOS	F	F	A	-	A	-	-	F
HCM 95th %tile Q(veh)	5.1	4.5	-	-	0	-	-	4.5

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Lanes, Volumes, Timings
5405: I75 NB Ent & Bonita Bch

2026 PM Pk Hr Background + Vested + Project
11/08/2021

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	883	2957	0	0	2676	648	473	0	739	0	0	0
Future Volume (vph)	883	2957	0	0	2676	648	473	0	739	0	0	0
Ideal Flow (vphpl)	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750
Storage Length (ft)	525		0	650		375	830		700	0		0
Storage Lanes	0		0	1		1	2		1	0		0
Taper Length (ft)	25			25			100			25		
Lane Util. Factor	0.97	0.95	1.00	1.00	0.91	1.00	0.97	1.00	0.88	1.00	1.00	1.00
Frnt						0.850			0.850			
Flt Protected	0.950						0.950					
Satd. Flow (prot)	3131	3228	0	0	4638	1444	3131	0	2542	0	0	0
Flt Permitted	0.950						0.950					
Satd. Flow (perm)	3131	3228	0	0	4638	1444	3131	0	2542	0	0	0
Right Turn on Red			Yes			Yes			No			Yes
Satd. Flow (RTOR)						335						
Link Speed (mph)		45			45			45			45	
Link Distance (ft)		724			797			860			629	
Travel Time (s)		11.0			12.1			13.0			9.5	
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj. Flow (vph)	883	2957	0	0	2676	648	473	0	739	0	0	0
Shared Lane Traffic (%)												
Lane Group Flow (vph)	883	2957	0	0	2676	648	473	0	739	0	0	0
Turn Type	Prot	NA			NA	Perm	Prot		Prot			
Protected Phases	1	6			2		3		3			
Permitted Phases						2	3					
Detector Phase	1	6			2	2	3		3			
Switch Phase												
Minimum Initial (s)	7.0	15.0			15.0	15.0	7.0		7.0			
Minimum Split (s)	14.2	21.7			21.7	21.7	13.5		13.5			
Total Split (s)	56.0	98.0			42.0	42.0	52.0		52.0			
Total Split (%)	37.3%	65.3%			28.0%	28.0%	34.7%		34.7%			
Maximum Green (s)	48.8	91.3			35.3	35.3	45.5		45.5			
Yellow Time (s)	4.7	4.7			4.7	4.7	4.0		4.0			
All-Red Time (s)	2.5	2.0			2.0	2.0	2.5		2.5			
Lost Time Adjust (s)	-3.5	-3.5			-3.5	-1.0	-3.0		-1.0			
Total Lost Time (s)	3.7	3.2			3.2	5.7	3.5		5.5			
Lead/Lag	Lag				Lead	Lead						
Lead-Lag Optimize?	Yes				Yes	Yes						
Vehicle Extension (s)	3.0	3.0			3.0	3.0	5.0		5.0			
Recall Mode	C-Max	C-Max			Max	Max	None		None			
Act Effct Green (s)	52.3	94.8			38.8	36.3	48.5		46.5			
Actuated g/C Ratio	0.35	0.63			0.26	0.24	0.32		0.31			
v/c Ratio	0.81	1.45			2.23	1.07	0.47		0.94			
Control Delay	25.2	221.6			583.0	83.8	42.3		70.3			
Queue Delay	0.0	0.2			0.5	0.0	56.8		0.0			
Total Delay	25.2	221.8			583.5	83.8	99.2		70.3			
LOS	C	F			F	F	F		E			
Approach Delay		176.6			486.1			81.6				
Approach LOS		F			F			F				



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Length 50th (ft)	410	~2091			~1542	~439	190		401			
Queue Length 95th (ft)	m281	m#1461			#1623	#686	244		#539			
Internal Link Dist (ft)		644			717			780			549	
Turn Bay Length (ft)	525					375	830		700			
Base Capacity (vph)	1091	2040			1199	603	1012		788			
Starvation Cap Reductn	0	166			0	0	0		0			
Spillback Cap Reductn	0	0			128	0	633		0			
Storage Cap Reductn	0	0			0	0	0		0			
Reduced v/c Ratio	0.81	1.58			2.50	1.07	1.25		0.94			

Intersection Summary

Area Type: Other

Cycle Length: 150

Actuated Cycle Length: 150

Offset: 56 (37%), Referenced to phase 1:EBL and 6:EBT, Start of Green

Natural Cycle: 150

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 2.23

Intersection Signal Delay: 285.7

Intersection LOS: F

Intersection Capacity Utilization 124.7%

ICU Level of Service H

Analysis Period (min) 15

~ Volume exceeds capacity, queue is theoretically infinite.

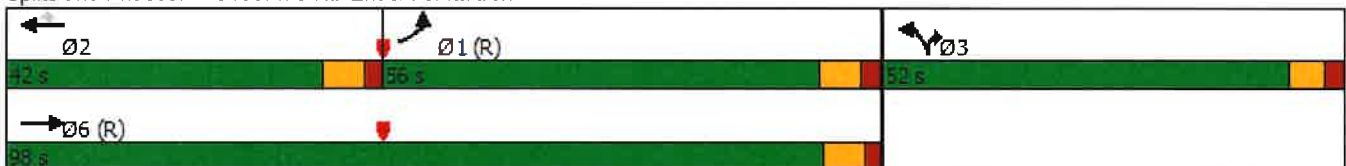
Queue shown is maximum after two cycles.

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 5405: I75 NB Ent & Bonita Bch



Lanes, Volumes, Timings
5404: I75 SB Ent/I-75 SB Ent & Bonita Bch

2026 PM Pk Hr Background + Vested + Project
11/08/2021



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑↑↑↑	↗↗	↖↖	↑↑					↗↗		↖↖
Traffic Volume (vph)	0	3400	762	739	2475	0	0	0	0	636	0	584
Future Volume (vph)	0	3400	762	739	2475	0	0	0	0	636	0	584
Ideal Flow (vphpl)	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750
Storage Length (ft)	0		590	525		0	0		0	600		375
Storage Lanes	0		2	2		0	0		0	2		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	0.86	0.88	0.97	0.95	1.00	1.00	1.00	1.00	0.97	1.00	0.76
Frnt			0.850									0.850
Flt Protected				0.950						0.950		
Satd. Flow (prot)	0	5845	2542	3131	3228	0	0	0	0	3131	0	3293
Flt Permitted				0.950						0.950		
Satd. Flow (perm)	0	5845	2542	3131	3228	0	0	0	0	3131	0	3293
Right Turn on Red			No			Yes			Yes			No
Satd. Flow (RTOR)												
Link Speed (mph)		45			45			45			45	
Link Distance (ft)		785			724			1234			1189	
Travel Time (s)		11.9			11.0			18.7			18.0	
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj. Flow (vph)	0	3400	762	739	2475	0	0	0	0	636	0	584
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	3400	762	739	2475	0	0	0	0	636	0	584
Turn Type		NA	Perm	Prot	NA					Prot		Prot
Protected Phases		6		5	2					3		8
Permitted Phases			6							3		
Detector Phase		6	6	5	2					3		8
Switch Phase												
Minimum Initial (s)		20.0	20.0	7.0	20.0					7.0		7.0
Minimum Split (s)		26.7	26.7	13.5	26.7					13.5		13.5
Total Split (s)		70.0	70.0	42.0	112.0					38.0		38.0
Total Split (%)		46.7%	46.7%	28.0%	74.7%					25.3%		25.3%
Maximum Green (s)		63.3	63.3	35.5	105.3					31.5		31.5
Yellow Time (s)		4.7	4.7	4.0	4.7					4.0		4.0
All-Red Time (s)		2.0	2.0	2.5	2.0					2.5		2.5
Lost Time Adjust (s)		-3.0	-1.0	-1.5	-3.0					-3.0		-3.0
Total Lost Time (s)		3.7	5.7	5.0	3.7					3.5		3.5
Lead/Lag		Lag	Lag	Lead								
Lead-Lag Optimize?		Yes	Yes	Yes								
Vehicle Extension (s)		3.0	3.0	3.0	3.0					3.0		3.0
Recall Mode		Max	Max	C-Max	C-Max					None		None
Walk Time (s)		5.0	5.0		5.0							
Flash Dont Walk (s)		11.0	11.0		11.0							
Pedestrian Calls (#/hr)		0	0		0							
Act Effct Green (s)		66.3	64.3	37.5	108.8					34.0		34.0
Actuated g/C Ratio		0.44	0.43	0.25	0.73					0.23		0.23
v/c Ratio		1.32	0.70	0.94	1.06					0.90		0.78
Control Delay		173.7	28.0	84.6	65.1					72.6		62.9
Queue Delay		0.4	0.0	0.0	18.4					0.0		0.0
Total Delay		174.0	28.0	84.6	83.4					72.6		62.9

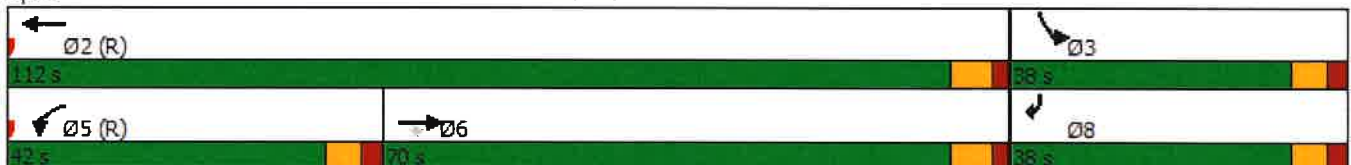


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
LOS		F	C	F	F					E		E
Approach Delay		147.3			83.7						67.9	
Approach LOS		F			F						E	
Queue Length 50th (ft)		~1256	242	393	~776					312		236
Queue Length 95th (ft)		#1287	319	m208	m166					#412		296
Internal Link Dist (ft)		705			644			1154			1109	
Turn Bay Length (ft)			590	525						600		375
Base Capacity (vph)		2583	1089	783	2341					720		757
Starvation Cap Reductn		0	0	0	370					0		0
Spillback Cap Reductn		397	0	0	0					0		0
Storage Cap Reductn		0	0	0	0					0		0
Reduced v/c Ratio		1.56	0.70	0.94	1.26					0.88		0.77

Intersection Summary

Area Type: Other
 Cycle Length: 150
 Actuated Cycle Length: 150
 Offset: 123 (82%), Referenced to phase 2:WBT and 5:WBL, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.32
 Intersection Signal Delay: 112.3 Intersection LOS: F
 Intersection Capacity Utilization 124.7% ICU Level of Service H
 Analysis Period (min) 15
 ~ Volume exceeds capacity, queue is theoretically infinite.
 Queue shown is maximum after two cycles.
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.
 m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 5404: I75 SB Ent/I-75 SB Ent & Bonita Bch



**2026 AM PK HR BACKGROUND +
PROJECT (WITHOUT VESTED)
TRAFFIC CONDITIONS**

Intersection												
Int Delay, s/veh	1.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↗	↘	↘	↗	↘			↗	↘	↗	↘
Traffic Vol, veh/h	13	887	54	7	967	2	37	0	23	1	0	1
Future Vol, veh/h	13	887	54	7	967	2	37	0	23	1	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	300	-	515	410	-	-	-	-	0	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3
Mvmt Flow	13	887	54	7	967	2	37	0	23	1	0	1

Major/Minor	Major1		Major2		Minor1		Minor2					
Conflicting Flow All	969	0	0	941	0	0	1411	1896	444	1452	1949	485
Stage 1	-	-	-	-	-	-	913	913	-	982	982	-
Stage 2	-	-	-	-	-	-	498	983	-	470	967	-
Critical Hdwy	4.16	-	-	4.16	-	-	7.56	6.56	6.96	7.56	6.56	6.96
Critical Hdwy Stg 1	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Follow-up Hdwy	2.23	-	-	2.23	-	-	3.53	4.03	3.33	3.53	4.03	3.33
Pot Cap-1 Maneuver	701	-	-	718	-	-	97	68	559	91	63	525
Stage 1	-	-	-	-	-	-	292	348	-	265	323	-
Stage 2	-	-	-	-	-	-	520	323	-	541	328	-
Platoon blocked, %		-	-	-	-	-						
Mov Cap-1 Maneuver	701	-	-	718	-	-	95	66	559	85	61	525
Mov Cap-2 Maneuver	-	-	-	-	-	-	95	66	-	85	61	-
Stage 1	-	-	-	-	-	-	286	341	-	260	320	-
Stage 2	-	-	-	-	-	-	514	320	-	509	322	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.1	0.1	44.8	30
HCM LOS			E	D

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	95	559	701	-	-	718	-	-	146
HCM Lane V/C Ratio	0.389	0.041	0.019	-	-	0.01	-	-	0.014
HCM Control Delay (s)	65.3	11.7	10.2	-	-	10.1	-	-	30
HCM Lane LOS	F	B	B	-	-	B	-	-	D
HCM 95th %tile Q(veh)	1.6	0.1	0.1	-	-	0	-	-	0

Intersection

Int Delay, s/veh 10.3

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕	↗		↕↕			↕			↕	
Traffic Vol, veh/h	31	1067	59	5	1177	11	48	0	6	23	0	67
Future Vol, veh/h	31	1067	59	5	1177	11	48	0	6	23	0	67
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	205	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3
Mvmt Flow	31	1067	59	5	1177	11	48	0	6	23	0	67

Major/Minor	Major1	Major2	Minor1	Minor2								
Conflicting Flow All	1188	0	0	1126	0	0	1728	2327	534	1789	2381	594
Stage 1	-	-	-	-	-	-	1129	1129	-	1193	1193	-
Stage 2	-	-	-	-	-	-	599	1198	-	596	1188	-
Critical Hdwy	4.16	-	-	4.16	-	-	7.56	6.56	6.96	7.56	6.56	6.96
Critical Hdwy Stg 1	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Follow-up Hdwy	2.23	-	-	2.23	-	-	3.53	4.03	3.33	3.53	4.03	3.33
Pot Cap-1 Maneuver	578	-	-	610	-	-	56	36	488	51	33	446
Stage 1	-	-	-	-	-	-	216	275	-	197	256	-
Stage 2	-	-	-	-	-	-	453	255	-	455	258	-
Platoon blocked, %		-	-	-	-	-						
Mov Cap-1 Maneuver	578	-	-	610	-	-	~ 41	30	488	44	28	446
Mov Cap-2 Maneuver	-	-	-	-	-	-	~ 41	30	-	44	28	-
Stage 1	-	-	-	-	-	-	184	235	-	168	250	-
Stage 2	-	-	-	-	-	-	376	249	-	384	220	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	1	0.1	\$ 329.5	74.5
HCM LOS			F	F

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	46	578	-	-	610	-	-	134
HCM Lane V/C Ratio	1.174	0.054	-	-	0.008	-	-	0.672
HCM Control Delay (s)	\$ 329.5	11.6	0.8	-	10.9	0.1	-	74.5
HCM Lane LOS	F	B	A	-	B	A	-	F
HCM 95th %tile Q(veh)	5	0.2	-	-	0	-	-	3.7

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Lanes, Volumes, Timings
5405: I75 NB Ent & Bonita Bch

2026 AM Pk Hr Background + Project (Without Vested)

11/08/2021

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	496	648	0	0	1041	271	992	0	247	0	0	0
Future Volume (vph)	496	648	0	0	1041	271	992	0	247	0	0	0
Ideal Flow (vphpl)	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750
Storage Length (ft)	525		0	650		375	830		700	0		0
Storage Lanes	0		0	1		1	2		1	0		0
Taper Length (ft)	25			25			100			25		
Lane Util. Factor	0.97	0.95	1.00	1.00	0.91	1.00	0.97	1.00	0.88	1.00	1.00	1.00
Frt						0.850			0.850			
Flt Protected	0.950						0.950					
Satd. Flow (prot)	3131	3228	0	0	4638	1444	3131	0	2542	0	0	0
Flt Permitted	0.950						0.950					
Satd. Flow (perm)	3131	3228	0	0	4638	1444	3131	0	2542	0	0	0
Right Turn on Red			Yes			Yes			No			Yes
Satd. Flow (RTOR)						271						
Link Speed (mph)		45			45			45			45	
Link Distance (ft)		724			797			860			629	
Travel Time (s)		11.0			12.1			13.0			9.5	
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj. Flow (vph)	496	648	0	0	1041	271	992	0	247	0	0	0
Shared Lane Traffic (%)												
Lane Group Flow (vph)	496	648	0	0	1041	271	992	0	247	0	0	0
Turn Type	Prot	NA			NA	Perm	Prot		Prot			
Protected Phases	1	6			2		3		3			
Permitted Phases						2	3					
Detector Phase	1	6			2	2	3		3			
Switch Phase												
Minimum Initial (s)	7.0	15.0			15.0	15.0	7.0		7.0			
Minimum Split (s)	14.2	21.7			21.7	21.7	13.5		13.5			
Total Split (s)	56.0	98.0			42.0	42.0	52.0		52.0			
Total Split (%)	37.3%	65.3%			28.0%	28.0%	34.7%		34.7%			
Maximum Green (s)	48.8	91.3			35.3	35.3	45.5		45.5			
Yellow Time (s)	4.7	4.7			4.7	4.7	4.0		4.0			
All-Red Time (s)	2.5	2.0			2.0	2.0	2.5		2.5			
Lost Time Adjust (s)	-3.5	-3.5			-3.5	-1.0	-3.0		-1.0			
Total Lost Time (s)	3.7	3.2			3.2	5.7	3.5		5.5			
Lead/Lag	Lag				Lead	Lead						
Lead-Lag Optimize?	Yes				Yes	Yes						
Vehicle Extension (s)	3.0	3.0			3.0	3.0	5.0		5.0			
Recall Mode	C-Max	C-Max			Max	Max	None		None			
Act Effct Green (s)	52.3	94.8			38.8	36.3	48.5		46.5			
Actuated g/C Ratio	0.35	0.63			0.26	0.24	0.32		0.31			
v/c Ratio	0.45	0.32			0.87	0.49	0.98		0.31			
Control Delay	16.7	3.7			62.0	8.0	73.8		40.9			
Queue Delay	0.0	0.0			0.0	0.0	0.0		0.0			
Total Delay	16.7	3.7			62.0	8.0	73.8		40.9			
LOS	B	A			E	A	E		D			
Approach Delay		9.4			50.8			67.2				
Approach LOS		A			D			E				

Lanes, Volumes, Timings

2026 AM Pk Hr Background + Project (Without Vested)

5404: I75 SB Ent/I-75 SB Ent & Bonita Bch

11/08/2021



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑↑↑↑	↑↑	↑↑	↑↑					↑↑		↑↑↑↑
Traffic Volume (vph)	0	923	744	324	1877	0	0	0	0	310	0	850
Future Volume (vph)	0	923	744	324	1877	0	0	0	0	310	0	850
Ideal Flow (vphpl)	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750
Storage Length (ft)	0		590	525		0	0		0	600		375
Storage Lanes	0		2	2		0	0		0	2		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	0.86	0.88	0.97	0.95	1.00	1.00	1.00	1.00	0.97	1.00	0.76
Frt			0.850									0.850
Flt Protected				0.950						0.950		
Satd. Flow (prot)	0	5845	2542	3131	3228	0	0	0	0	3131	0	3293
Flt Permitted				0.950						0.950		
Satd. Flow (perm)	0	5845	2542	3131	3228	0	0	0	0	3131	0	3293
Right Turn on Red			No			Yes			Yes			No
Satd. Flow (RTOR)												
Link Speed (mph)		45			45			45			45	
Link Distance (ft)		785			724			1234			1189	
Travel Time (s)		11.9			11.0			18.7			18.0	
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj. Flow (vph)	0	923	744	324	1877	0	0	0	0	310	0	850
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	923	744	324	1877	0	0	0	0	310	0	850
Turn Type		NA	Perm	Prot	NA					Prot		Prot
Protected Phases		6		5	2					3		8
Permitted Phases			6							3		
Detector Phase		6	6	5	2					3		8
Switch Phase												
Minimum Initial (s)		20.0	20.0	7.0	20.0					7.0		7.0
Minimum Split (s)		26.7	26.7	13.5	26.7					13.5		13.5
Total Split (s)		70.0	70.0	42.0	112.0					38.0		38.0
Total Split (%)		46.7%	46.7%	28.0%	74.7%					25.3%		25.3%
Maximum Green (s)		63.3	63.3	35.5	105.3					31.5		31.5
Yellow Time (s)		4.7	4.7	4.0	4.7					4.0		4.0
All-Red Time (s)		2.0	2.0	2.5	2.0					2.5		2.5
Lost Time Adjust (s)		-3.0	-1.0	-1.5	-3.0					-3.0		-3.0
Total Lost Time (s)		3.7	5.7	5.0	3.7					3.5		3.5
Lead/Lag		Lag	Lag	Lead								
Lead-Lag Optimize?		Yes	Yes	Yes								
Vehicle Extension (s)		3.0	3.0	3.0	3.0					3.0		3.0
Recall Mode		Max	Max	C-Max	C-Max					None		None
Walk Time (s)		5.0	5.0		5.0							
Flash Dont Walk (s)		11.0	11.0		11.0							
Pedestrian Calls (#/hr)		0	0		0							
Act Effct Green (s)		66.3	64.3	37.0	108.3					34.5		34.5
Actuated g/C Ratio		0.44	0.43	0.25	0.72					0.23		0.23
v/c Ratio		0.36	0.68	0.42	0.81					0.43		1.12
Control Delay		9.0	13.3	55.8	4.6					51.6		123.4
Queue Delay		0.0	0.0	0.0	3.6					0.0		0.0
Total Delay		9.0	13.3	55.8	8.2					51.6		123.4



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
LOS		A	B	E	A					D		F
Approach Delay		10.9			15.2						104.2	
Approach LOS		B			B						F	
Queue Length 50th (ft)		47	78	117	118					134		~419
Queue Length 95th (ft)		68	128	m133	m73					183		#532
Internal Link Dist (ft)		705			644			1154			1109	
Turn Bay Length (ft)			590	525						600		375
Base Capacity (vph)		2583	1089	772	2330					720		757
Starvation Cap Reductn		0	0	0	359					0		0
Spillback Cap Reductn		0	0	0	1					0		0
Storage Cap Reductn		0	0	0	0					0		0
Reduced v/c Ratio		0.36	0.68	0.42	0.95					0.43		1.12

Intersection Summary

Area Type: Other

Cycle Length: 150

Actuated Cycle Length: 150

Offset: 123 (82%), Referenced to phase 2:WBT and 5:WBL, Start of Green

Natural Cycle: 65

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 1.12

Intersection Signal Delay: 34.3

Intersection LOS: C

Intersection Capacity Utilization 84.5%

ICU Level of Service E

Analysis Period (min) 15

~ Volume exceeds capacity, queue is theoretically infinite.

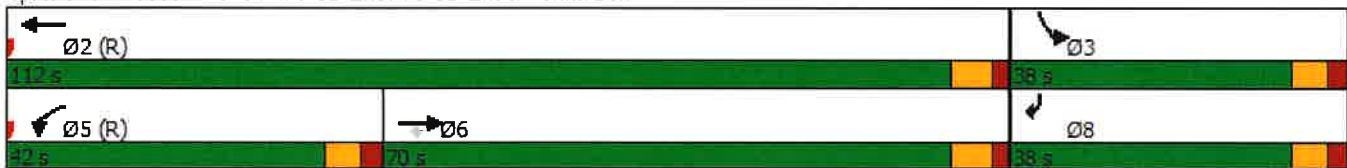
Queue shown is maximum after two cycles.

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 5404: I75 SB Ent/I-75 SB Ent & Bonita Bch



**2026 PM PK HR BACKGROUND +
PROJECT (WITHOUT VESTED)
TRAFFIC CONDITIONS**

Intersection												
Int Delay, s/veh	4.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗	↘	↖	↗	↘			↖	↗	↘	
Traffic Vol, veh/h	3	999	33	11	1204	0	54	0	28	3	0	14
Future Vol, veh/h	3	999	33	11	1204	0	54	0	28	3	0	14
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	300	-	515	410	-	-	-	-	0	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3
Mvmt Flow	3	999	33	11	1204	0	54	0	28	3	0	14

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	1204	0	0	1032
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	4.16	-	-	4.16
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	2.23	-	-	2.23
Pot Cap-1 Maneuver	570	-	-	663
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	570	-	-	663
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0	0.1	120.4	25.8
HCM LOS			F	D

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	64	514	570	-	-	663	-	-	190
HCM Lane V/C Ratio	0.844	0.054	0.005	-	-	0.017	-	-	0.089
HCM Control Delay (s)	176.4	12.4	11.3	-	-	10.5	-	-	25.8
HCM Lane LOS	F	B	B	-	-	B	-	-	D
HCM 95th %tile Q(veh)	3.9	0.2	0	-	-	0.1	-	-	0.3

Intersection												
Int Delay, s/veh	24.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕	↗		↕↕			↕			↕	
Traffic Vol, veh/h	87	1080	60	0	990	33	66	0	6	25	0	62
Future Vol, veh/h	87	1080	60	0	990	33	66	0	6	25	0	62
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	205	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	100	100	100	100	100	100	100	100	100	100	100	100
Heavy Vehicles, %	3	3	3	3	3	3	3	3	3	3	3	3
Mvmt Flow	87	1080	60	0	990	33	66	0	6	25	0	62

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1023	0	0	1140	0	0	1749	2277	540	1721	2321	512
Stage 1	-	-	-	-	-	-	1254	1254	-	1007	1007	-
Stage 2	-	-	-	-	-	-	495	1023	-	714	1314	-
Critical Hdwy	4.16	-	-	4.16	-	-	7.56	6.56	6.96	7.56	6.56	6.96
Critical Hdwy Stg 1	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.56	5.56	-	6.56	5.56	-
Follow-up Hdwy	2.23	-	-	2.23	-	-	3.53	4.03	3.33	3.53	4.03	3.33
Pot Cap-1 Maneuver	668	-	-	603	-	-	~ 54	39	484	57	37	504
Stage 1	-	-	-	-	-	-	180	240	-	256	314	-
Stage 2	-	-	-	-	-	-	522	309	-	386	224	-
Platoon blocked, %		-	-	-	-	-						
Mov Cap-1 Maneuver	668	-	-	603	-	-	~ 34	25	484	40	24	504
Mov Cap-2 Maneuver	-	-	-	-	-	-	~ 34	25	-	40	24	-
Stage 1	-	-	-	-	-	-	115	153	-	163	314	-
Stage 2	-	-	-	-	-	-	458	309	-	243	143	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	2.3	0	\$ 676.4	96.6
HCM LOS			F	F

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	37	668	-	-	603	-	-	116
HCM Lane V/C Ratio	1.946	0.13	-	-	-	-	-	0.75
HCM Control Delay (s)	\$ 676.4	11.2	1.7	-	0	-	-	96.6
HCM Lane LOS	F	B	A	-	A	-	-	F
HCM 95th %tile Q(veh)	7.8	0.4	-	-	0	-	-	4.2

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Lanes, Volumes, Timings
5405: I75 NB Ent & Bonita Bch

2026 PM Pk Hr Background + Project (Without Vested)

11/08/2021

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	883	1011	0	0	968	358	473	0	243	0	0	0
Future Volume (vph)	883	1011	0	0	968	358	473	0	243	0	0	0
Ideal Flow (vphpl)	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750
Storage Length (ft)	525		0	650		375	830		700	0		0
Storage Lanes	0		0	1		1	2		1	0		0
Taper Length (ft)	25			25			100			25		
Lane Util. Factor	0.97	0.95	1.00	1.00	0.91	1.00	0.97	1.00	0.88	1.00	1.00	1.00
Frnt						0.850			0.850			
Flt Protected	0.950						0.950					
Satd. Flow (prot)	3131	3228	0	0	4638	1444	3131	0	2542	0	0	0
Flt Permitted	0.950						0.950					
Satd. Flow (perm)	3131	3228	0	0	4638	1444	3131	0	2542	0	0	0
Right Turn on Red			Yes			Yes			No			Yes
Satd. Flow (RTOR)						358						
Link Speed (mph)		45			45			45			45	
Link Distance (ft)		724			797			860			629	
Travel Time (s)		11.0			12.1			13.0			9.5	
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj. Flow (vph)	883	1011	0	0	968	358	473	0	243	0	0	0
Shared Lane Traffic (%)												
Lane Group Flow (vph)	883	1011	0	0	968	358	473	0	243	0	0	0
Turn Type	Prot	NA			NA	Perm	Prot		Prot			
Protected Phases	1	6			2		3		3			
Permitted Phases						2	3					
Detector Phase	1	6			2	2	3		3			
Switch Phase												
Minimum Initial (s)	7.0	15.0			15.0	15.0	7.0		7.0			
Minimum Split (s)	14.2	21.7			21.7	21.7	13.5		13.5			
Total Split (s)	56.0	98.0			42.0	42.0	52.0		52.0			
Total Split (%)	37.3%	65.3%			28.0%	28.0%	34.7%		34.7%			
Maximum Green (s)	48.8	91.3			35.3	35.3	45.5		45.5			
Yellow Time (s)	4.7	4.7			4.7	4.7	4.0		4.0			
All-Red Time (s)	2.5	2.0			2.0	2.0	2.5		2.5			
Lost Time Adjust (s)	-3.5	-3.5			-3.5	-1.0	-3.0		-1.0			
Total Lost Time (s)	3.7	3.2			3.2	5.7	3.5		5.5			
Lead/Lag	Lag				Lead	Lead						
Lead-Lag Optimize?	Yes				Yes	Yes						
Vehicle Extension (s)	3.0	3.0			3.0	3.0	5.0		5.0			
Recall Mode	C-Max	C-Max			Max	Max	None		None			
Act Effct Green (s)	52.3	108.1			52.1	49.6	35.2		33.2			
Actuated g/C Ratio	0.35	0.72			0.35	0.33	0.23		0.22			
v/c Ratio	0.81	0.43			0.60	0.50	0.64		0.43			
Control Delay	27.1	1.1			43.2	6.4	55.4		51.8			
Queue Delay	0.0	0.1			0.0	0.0	0.0		0.0			
Total Delay	27.1	1.2			43.2	6.4	55.4		51.8			
LOS	C	A			D	A	E		D			
Approach Delay		13.3			33.3			54.2				
Approach LOS		B			C			D				

Lanes, Volumes, Timings
 5405: I75 NB Ent & Bonita Bch

2026 PM Pk Hr Background + Project (Without Vested)

11/08/2021



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Length 50th (ft)	417	9			286	0	217		117			
Queue Length 95th (ft)	494	16			364	83	258		155			
Internal Link Dist (ft)		644			717			780			549	
Turn Bay Length (ft)	525					375	830		700			
Base Capacity (vph)	1091	2325			1609	716	1012		788			
Starvation Cap Reductn	0	351			0	0	0		0			
Spillback Cap Reductn	0	0			0	0	0		0			
Storage Cap Reductn	0	0			0	0	0		0			
Reduced v/c Ratio	0.81	0.51			0.60	0.50	0.47		0.31			

Intersection Summary

Area Type: Other

Cycle Length: 150

Actuated Cycle Length: 150

Offset: 56 (37%), Referenced to phase 1:EBL and 6:EBT, Start of Green

Natural Cycle: 60

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.81

Intersection Signal Delay: 27.5

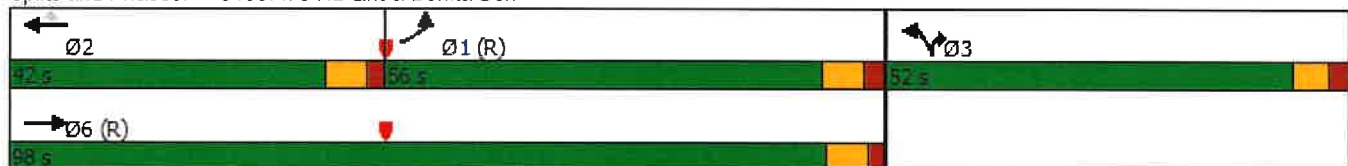
Intersection LOS: C

Intersection Capacity Utilization 77.5%

ICU Level of Service D

Analysis Period (min) 15

Splits and Phases: 5405: I75 NB Ent & Bonita Bch















Lanes, Volumes, Timings

2026 PM Pk Hr Background + Project (Without Vested)

5404: I75 SB Ent/I-75 SB Ent & Bonita Bch

11/08/2021

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑↑↑↑	↗↗	↘↘	↑↑					↗↗		↘↘
Traffic Volume (vph)	0	1843	762	304	1202	0	0	0	0	247	0	584
Future Volume (vph)	0	1843	762	304	1202	0	0	0	0	247	0	584
Ideal Flow (vphpl)	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750	1750
Storage Length (ft)	0		590	525		0	0		0	600		375
Storage Lanes	0		2	2		0	0		0	2		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	0.86	0.88	0.97	0.95	1.00	1.00	1.00	1.00	0.97	1.00	0.76
Frnt			0.850									0.850
Flt Protected				0.950						0.950		
Satd. Flow (prot)	0	5845	2542	3131	3228	0	0	0	0	3131	0	3293
Flt Permitted				0.950						0.950		
Satd. Flow (perm)	0	5845	2542	3131	3228	0	0	0	0	3131	0	3293
Right Turn on Red			No			Yes			Yes			No
Satd. Flow (RTOR)												
Link Speed (mph)		45			45			45			45	
Link Distance (ft)		785			724			1234			1189	
Travel Time (s)		11.9			11.0			18.7			18.0	
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj. Flow (vph)	0	1843	762	304	1202	0	0	0	0	247	0	584
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	1843	762	304	1202	0	0	0	0	247	0	584
Turn Type		NA	Perm	Prot	NA					Prot		Prot
Protected Phases		6		5	2					3		8
Permitted Phases			6							3		
Detector Phase		6	6	5	2					3		8
Switch Phase												
Minimum Initial (s)		20.0	20.0	7.0	20.0					7.0		7.0
Minimum Split (s)		26.7	26.7	13.5	26.7					13.5		13.5
Total Split (s)		70.0	70.0	42.0	112.0					38.0		38.0
Total Split (%)		46.7%	46.7%	28.0%	74.7%					25.3%		25.3%
Maximum Green (s)		63.3	63.3	35.5	105.3					31.5		31.5
Yellow Time (s)		4.7	4.7	4.0	4.7					4.0		4.0
All-Red Time (s)		2.0	2.0	2.5	2.0					2.5		2.5
Lost Time Adjust (s)		-3.0	-1.0	-1.5	-3.0					-3.0		-3.0
Total Lost Time (s)		3.7	5.7	5.0	3.7					3.5		3.5
Lead/Lag		Lag	Lag	Lead								
Lead-Lag Optimize?		Yes	Yes	Yes								
Vehicle Extension (s)		3.0	3.0	3.0	3.0					3.0		3.0
Recall Mode		Max	Max	C-Max	C-Max					None		None
Walk Time (s)		5.0	5.0		5.0							
Flash Dont Walk (s)		11.0	11.0		11.0							
Pedestrian Calls (#/hr)		0	0		0							
Act Effct Green (s)		66.3	64.3	38.8	110.1					32.7		32.7
Actuated g/C Ratio		0.44	0.43	0.26	0.73					0.22		0.22
v/c Ratio		0.71	0.70	0.38	0.51					0.36		0.81
Control Delay		18.6	21.7	63.0	2.2					51.1		65.7
Queue Delay		0.0	0.0	0.0	0.2					0.0		0.0
Total Delay		18.6	21.7	63.0	2.3					51.1		65.7



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
LOS		B	C	E	A					D		E
Approach Delay		19.5			14.6						61.4	
Approach LOS		B			B						E	
Queue Length 50th (ft)		192	158	121	46					105		236
Queue Length 95th (ft)		227	214	170	29					147		296
Internal Link Dist (ft)		705			644			1154			1109	
Turn Bay Length (ft)			590	525						600		375
Base Capacity (vph)		2583	1089	810	2370					720		757
Starvation Cap Reductn		0	0	0	369					0		0
Spillback Cap Reductn		0	0	0	0					0		0
Storage Cap Reductn		0	0	0	0					0		0
Reduced v/c Ratio		0.71	0.70	0.38	0.60					0.34		0.77

Intersection Summary

Area Type: Other

Cycle Length: 150

Actuated Cycle Length: 150

Offset: 123 (82%), Referenced to phase 2:WBT and 5:WBL, Start of Green

Natural Cycle: 55

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.81

Intersection Signal Delay: 25.0

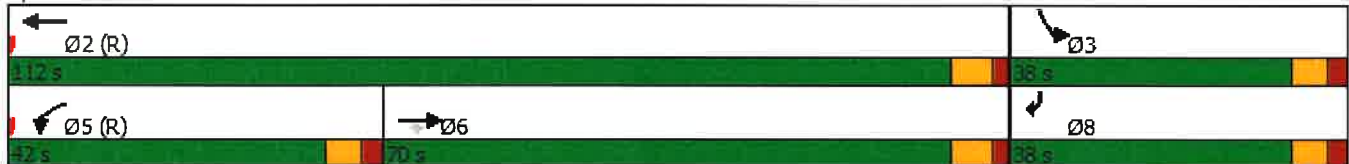
Intersection LOS: C

Intersection Capacity Utilization 77.5%

ICU Level of Service D

Analysis Period (min) 15

Splits and Phases: 5404: I75 SB Ent/I-75 SB Ent & Bonita Bch



ITE PASS-BY RATES

Vehicle Pass-By Rates by Land Use

Source: ITE Trip Generation Manual, 11th Edition

Land Use Code	912									
Land Use	Drive-In Bank									
Setting	General Urban/Suburban									
Time Period	Weekday AM Peak Period									
# Data Sites	8									
Average Pass-By Rate	29%									
Pass-By Characteristics for Individual Sites										
GFA (000)	State or Province	Survey Year	# Interviews	Pass-By Trip (%)	Non-Pass-By Trips			Adj Street Peak Hour Volume	Source	
					Primary (%)	Diverted (%)	Total (%)			
3.8	Pennsylvania	2005	11	27	—	—	73	—	19	
3.8	Pennsylvania	2005	9	24	—	—	76	—	19	
3.8	Pennsylvania	2005	22	34	—	—	66	—	19	
3.8	Pennsylvania	2005	30	27	—	—	73	—	19	
3.8	Pennsylvania	2005	34	40	—	—	60	—	19	
3.8	Pennsylvania	2005	7	27	—	—	73	—	19	
3.8	Pennsylvania	2005	15	16	—	—	84	—	19	
3.8	Pennsylvania	2005	27	36	—	—	64	—	19	

Vehicle Pass-By Rates by Land Use

Source: ITE Trip Generation Manual, 11th Edition

Land Use Code	912
Land Use	Drive-In Bank
Setting	General Urban/Suburban
Time Period	Weekday PM Peak Period
# Data Sites	19
Average Pass-By Rate	35%

Pass-By Characteristics for Individual Sites

GFA (000)	State or Province	Survey Year	# Interviews	Pass-By Trip (%)	Non-Pass-By Trips			Total (%)	Adj Street Peak Hour Volume	Source
					Primary (%)	Diverted (%)	Total (%)			
2.7	Washington	2007	—	26	66	8	74	—	11	
2.8	Washington	2007	—	21	55	24	79	—	11	
3.3	Kentucky	1993	—	48	22	30	52	2570	34	
3.4	Kentucky	1993	—	64	22	14	36	2266	34	
3.4	Kentucky	1993	75	57	11	32	43	1955	34	
3.5	Kentucky	1993	53	47	32	21	53	2785	2	
3.6	Washington	2007	—	42	50	8	58	—	11	
3.6	Washington	2007	—	29	—	—	71	—	11	
3.8	Pennsylvania	2005	56	43	—	—	57	—	19	
3.8	Pennsylvania	2005	38	41	—	—	59	—	19	
3.8	Pennsylvania	2005	14	24	—	—	76	—	19	
3.8	Pennsylvania	2005	63	29	—	—	71	—	19	
3.8	Pennsylvania	2005	70	29	—	—	71	—	19	
3.8	Pennsylvania	2005	29	27	—	—	73	—	19	
3.8	Pennsylvania	2005	41	25	—	—	75	—	19	
3.8	Pennsylvania	2005	37	31	—	—	69	—	19	
3.8	Pennsylvania	2005	19	29	—	—	71	—	19	
3.8	Pennsylvania	2005	34	21	—	—	79	—	19	
3.8	Pennsylvania	2005	36	29	—	—	71	—	19	

Vehicle Pass-By Rates by Land Use

Source: ITE Trip Generation Manual , 11th Edition

Land Use Code	945										
Land Use	Convenience Store/Gas Station										
Setting	General Urban/Suburban										
Time Period	Weekday AM Peak Period										
# Data Sites	16 Sites with between 2 and 8 VFP					28 Sites with between 9 and 20 VFP					
Average Pass-By Rate	60% for Sites with between 2 and 8 VFP					76% for Sites with between 9 and 20 VFP					
Pass-By Characteristics for Individual Sites											
GFA (000)	VFP	State or Province	Survey Year	# Interviews	Pass-By Trip (%)	Non-Pass-By Trips			Adj Street Peak Hour Volume	Source	
						Primary (%)	Diverted (%)	Total (%)			
2	8	Maryland	1992	46	87	13	0	13	2235	25	
2.1	6	Maryland	1992	26	58	23	19	42	2080	25	
2.1	6	Maryland	1992	26	58	23	19	42	2080	25	
2.2	8	Maryland	1992	31	47	34	19	53	1785	25	
2.2	< 8	Indiana	1993	79	56	6	38	44	635	2	
2.2	8	Maryland	1992	35	78	9	13	22	7080	25	
2.3	6	Maryland	1992	37	32	41	27	68	2080	25	
2.3	< 8	Kentucky	1993	58	64	5	31	36	1255	2	
2.3	6	Maryland	1992	37	32	41	27	68	2080	25	
2.4	< 8	Kentucky	1993	—	48	17	35	52	1210	2	
2.6	< 8	Kentucky	1993	—	72	15	13	28	940	2	
2.8	< 8	Kentucky	1993	—	54	11	35	46	1240	2	
3	< 8	Indiana	1993	62	74	10	16	26	790	2	
3.6	< 8	Kentucky	1993	49	67	4	29	33	1985	2	
3.7	< 8	Kentucky	1993	49	66	16	18	34	990	2	
4.694	12	Maryland	2000	—	72	—	—	28	2440	30	
4.694	12	Maryland	2000	—	78	—	—	22	1561	30	
4.694	12	Maryland	2000	—	79	—	—	21	2764	30	
4.848	12	Virginia	2000	—	55	—	—	45	1398	30	
5.06	12	Pennsylvania	2000	—	84	—	—	16	3219	30	
5.242	12	Virginia	2000	—	74	—	—	26	1160	30	
5.242	12	Virginia	2000	—	71	—	—	29	548	30	
5.488	12	Delaware	2000	—	80	—	—	20	—	30	
5.5	12	Pennsylvania	2000	—	85	—	—	15	2975	30	
4.2	< 8	Kentucky	1993	47	62	19	19	38	1705	2	
4.694	16	Maryland	2000	—	90	—	—	10	2278	30	
4.694	16	Delaware	2000	—	74	—	—	26	2185	30	
4.694	16	Delaware	2000	—	58	—	—	42	962	30	
4.694	16	Delaware	2000	—	84	—	—	16	2956	30	
4.694	16	New Jersey	2000	—	79	—	—	21	1859	30	
4.694	20	Delaware	2000	—	84	—	—	16	3864	30	
4.848	16	Virginia	2000	—	68	—	—	32	2106	30	
4.848	16	Virginia	2000	—	85	—	—	15	2676	30	
4.848	16	Virginia	2000	—	75	—	—	25	3244	30	
4.848	16	Virginia	2000	—	71	—	—	29	1663	30	
4.993	16	Pennsylvania	2000	—	75	—	—	25	1991	30	
5.094	16	New Jersey	2000	—	86	—	—	14	1260	30	
5.5	16	Pennsylvania	2000	—	82	—	—	18	1570	30	
5.543	16	Pennsylvania	2000	—	84	—	—	16	1933	30	
5.565	16	Pennsylvania	2000	—	77	—	—	23	2262	30	
5.565	16	Pennsylvania	2000	—	68	—	—	32	2854	30	
5.565	16	New Jersey	2000	—	58	—	—	42	1253	30	
5.565	16	New Jersey	2000	—	79	—	—	21	1928	30	
5.565	16	New Jersey	2000	---	84	---	---	16	1953	30	

Vehicle Pass-By Rates by Land Use										
Source: ITE Trip Generation Manual, 11th Edition										
Land Use Code	945									
Land Use	Convenience Store/Gas Station									
Setting	General Urban/Suburban									
Time Period	Weekday PM Peak Period									
# Data Sites	12 Sites with between 2 and 8 VFP					28 Sites with between 9 and 20 VFP				
Average Pass-By Rate	56% for Sites with between 2 and 8 VFP					75% for Sites with between 9 and 20 VFP				
Pass-By Characteristics for Individual Sites										
GFA (000)	VFP	State or Province	Survey Year	# Interviews	Pass-By Trip (%)	Non-Pass-By Trips			Adj Street Peak Hour Volume	Source
						Primary (%)	Diverted (%)	Total (%)		
2.1	8	Maryland	1992	31	52	13	35	48	1785	25
2.1	6	Maryland	1992	30	53	20	27	47	1060	25
2.2	< 8	Indiana	1993	115	48	16	36	52	820	2
2.3	< 8	Kentucky	1993	67	57	16	27	43	1954	2
2.3	6	Maryland	1992	55	40	11	49	60	2760	25
2.4	< 8	Kentucky	1993	—	58	13	29	42	2655	2
2.6	< 8	Kentucky	1993	68	67	15	18	33	950	2
2.8	< 8	Kentucky	1993	—	62	11	27	38	2875	2
3	< 8	Indiana	1993	80	65	15	20	35	1165	2
3.6	< 8	Kentucky	1993	60	56	17	27	44	2505	2
3.7	< 8	Kentucky	1993	70	61	16	23	39	2175	2
4.2	< 8	Kentucky	1993	61	58	26	16	42	2300	2
4.694	12	Maryland	2000	—	78	—	—	22	3549	30
4.694	12	Maryland	2000	—	67	—	—	33	2272	30
4.694	12	Maryland	2000	—	66	—	—	34	3514	30
4.848	12	Virginia	2000	—	71	—	—	29	2350	30
5.06	12	Pennsylvania	2000	—	91	—	—	9	4181	30
5.242	12	Virginia	2000	—	70	—	—	30	2445	30
5.242	12	Virginia	2000	—	56	—	—	44	950	30
5.488	12	Delaware	2000	—	73	—	—	27	—	30
5.5	12	Pennsylvania	2000	—	84	—	—	16	4025	30
4.694	16	Maryland	2000	—	89	—	—	11	2755	30
4.694	16	Delaware	2000	—	73	—	—	27	1858	30
4.694	16	Delaware	2000	—	59	—	—	41	1344	30
4.694	16	Delaware	2000	—	72	—	—	28	3434	30
4.694	16	New Jersey	2000	—	81	—	—	19	1734	30
4.694	20	Delaware	2000	—	76	—	—	24	1616	30
4.848	16	Virginia	2000	—	67	—	—	33	2,954	30
4.848	16	Virginia	2000	—	78	—	—	22	3086	30
4.848	16	Virginia	2000	—	83	—	—	17	4143	30
4.848	16	Virginia	2000	—	73	—	—	27	2534	30
4.993	16	Pennsylvania	2000	—	72	—	—	28	2917	30
5.094	16	New Jersey	2000	—	86	—	—	14	1730	30
5.5	16	Pennsylvania	2000	—	90	—	—	10	2616	30
5.543	16	Pennsylvania	2000	—	87	—	—	13	2363	30
5.565	16	Pennsylvania	2000	—	81	—	—	19	2770	30
5.565	16	Pennsylvania	2000	—	76	—	—	24	3362	30
5.565	16	New Jersey	2000	—	61	—	—	39	1713	30
5.565	16	New Jersey	2000	—	86	—	—	14	1721	30
5.565	16	New Jersey	2000	—	81	—	—	19	2227	30

TRIP GENERATION EQUATIONS

Mini-Warehouse (151)

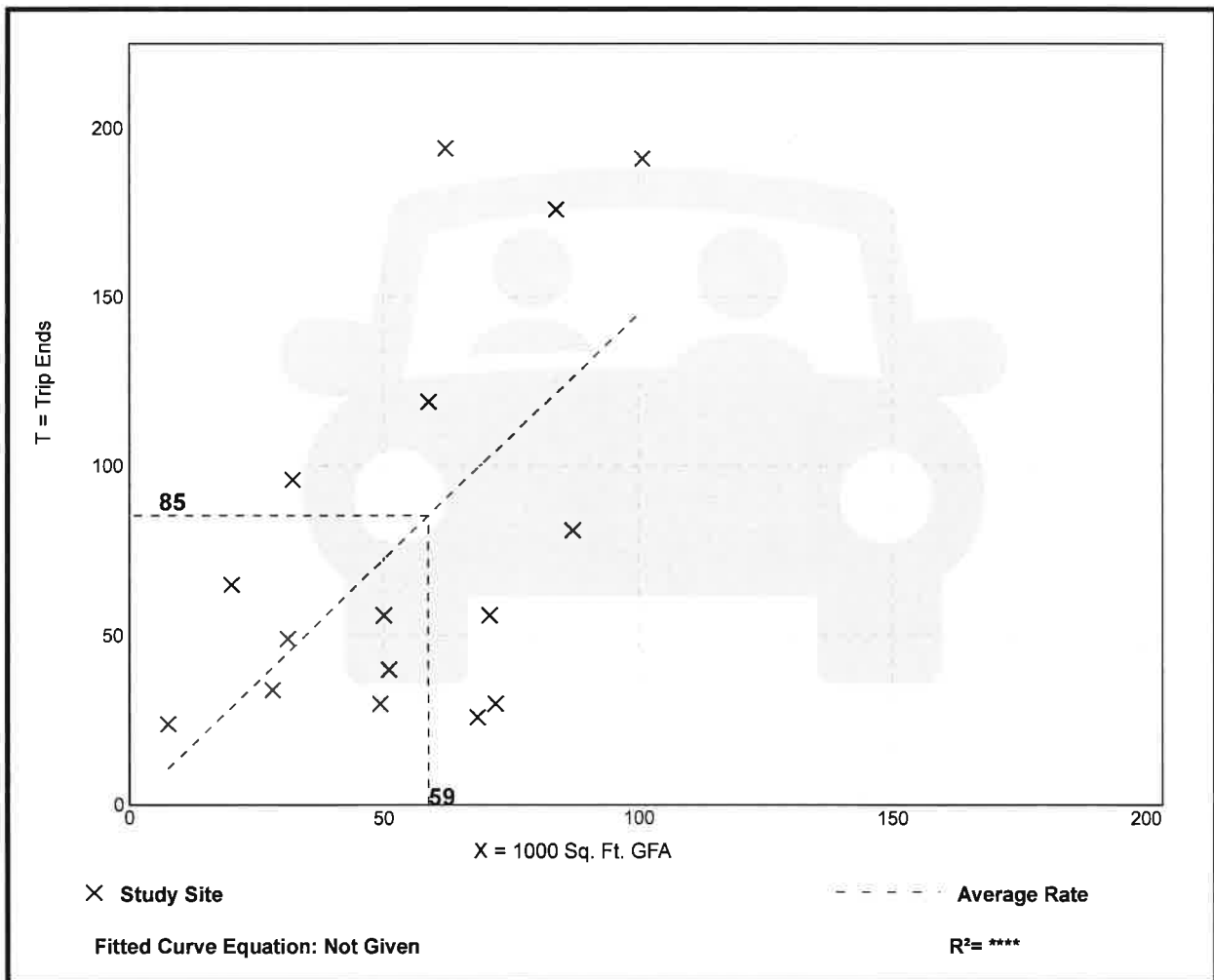
Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 16
Avg. 1000 Sq. Ft. GFA: 55
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
1.45	0.38 - 3.25	0.92

Data Plot and Equation



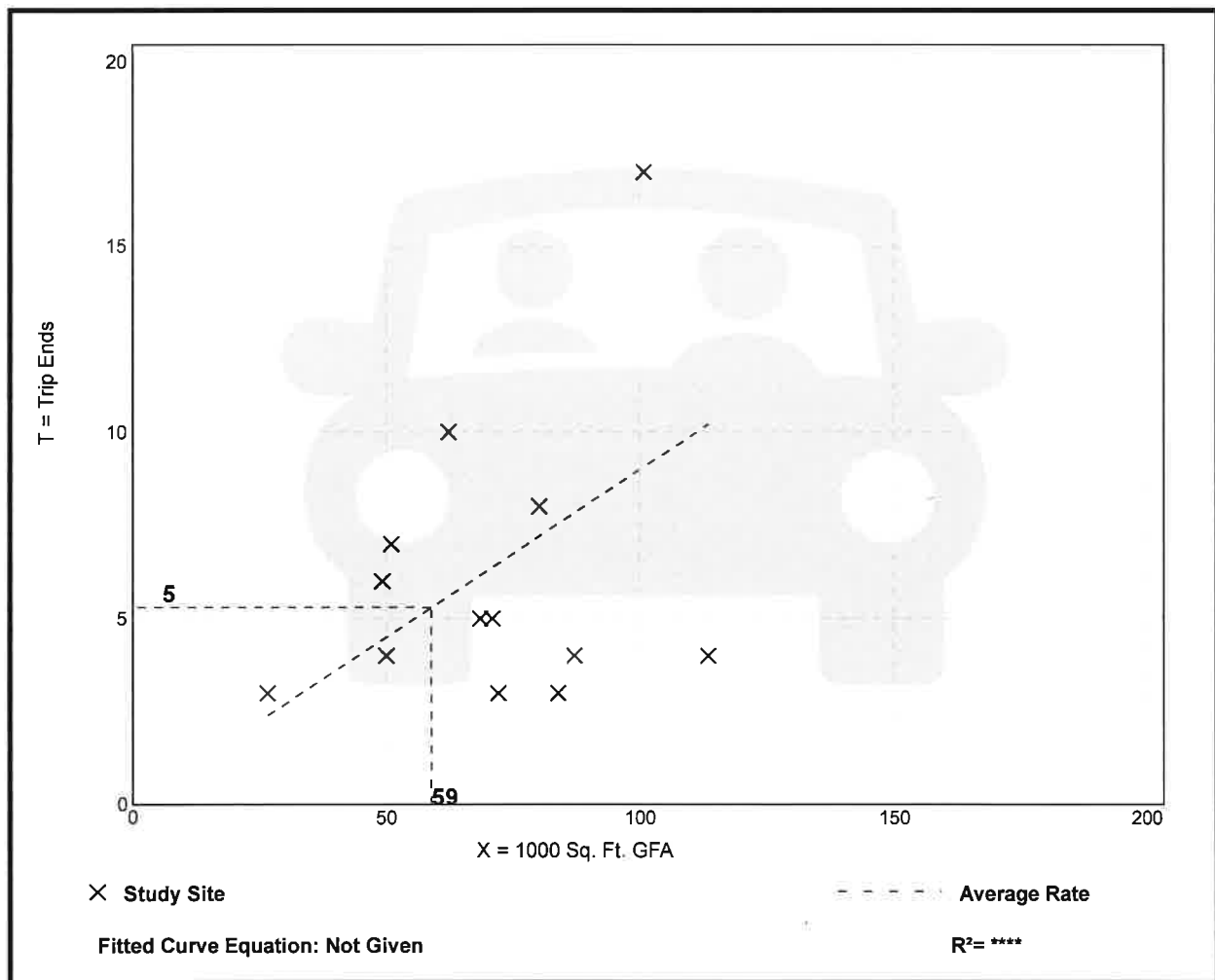
Mini-Warehouse (151)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 13
 Avg. 1000 Sq. Ft. GFA: 70
 Directional Distribution: 59% entering, 41% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
0.09	0.04 - 0.17	0.05

Data Plot and Equation



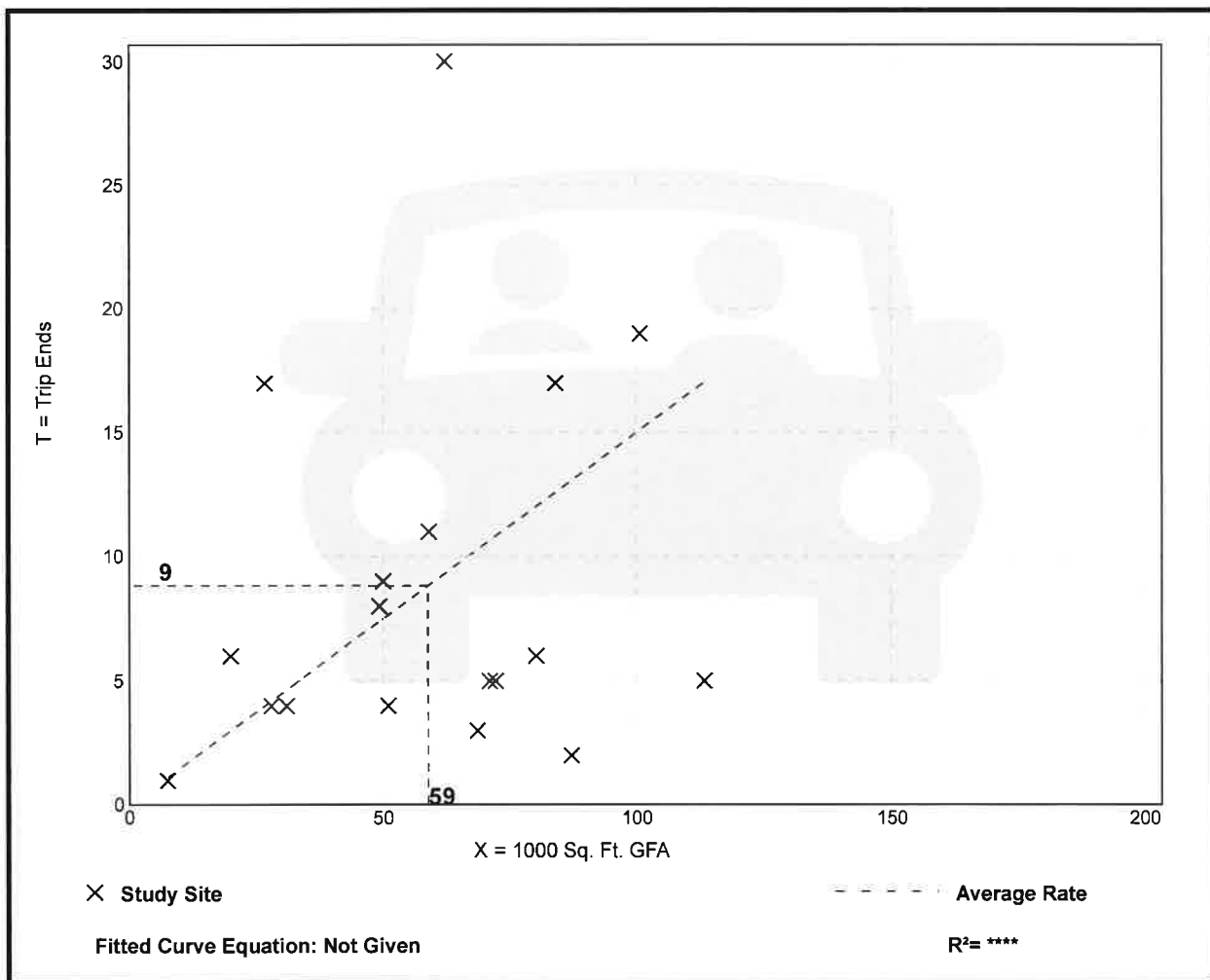
Mini-Warehouse (151)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 18
 Avg. 1000 Sq. Ft. GFA: 59
 Directional Distribution: 47% entering, 53% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
0.15	0.02 - 0.64	0.14

Data Plot and Equation



Multifamily Housing (Low-Rise) Not Close to Rail Transit (220)

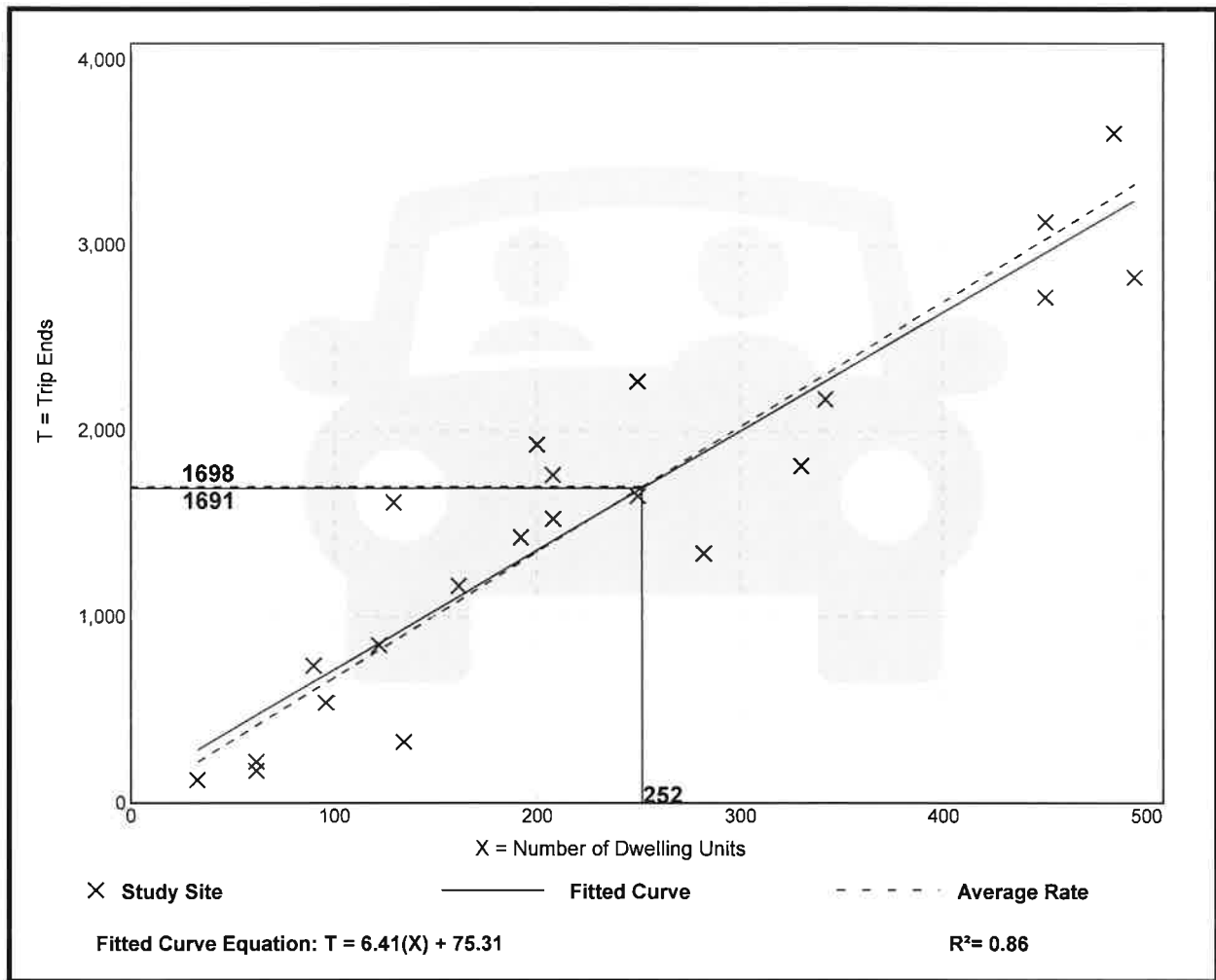
Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 22
Avg. Num. of Dwelling Units: 229
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
6.74	2.46 - 12.50	1.79

Data Plot and Equation



Multifamily Housing (Low-Rise) Not Close to Rail Transit (220)

Vehicle Trip Ends vs: Dwelling Units
 On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 49

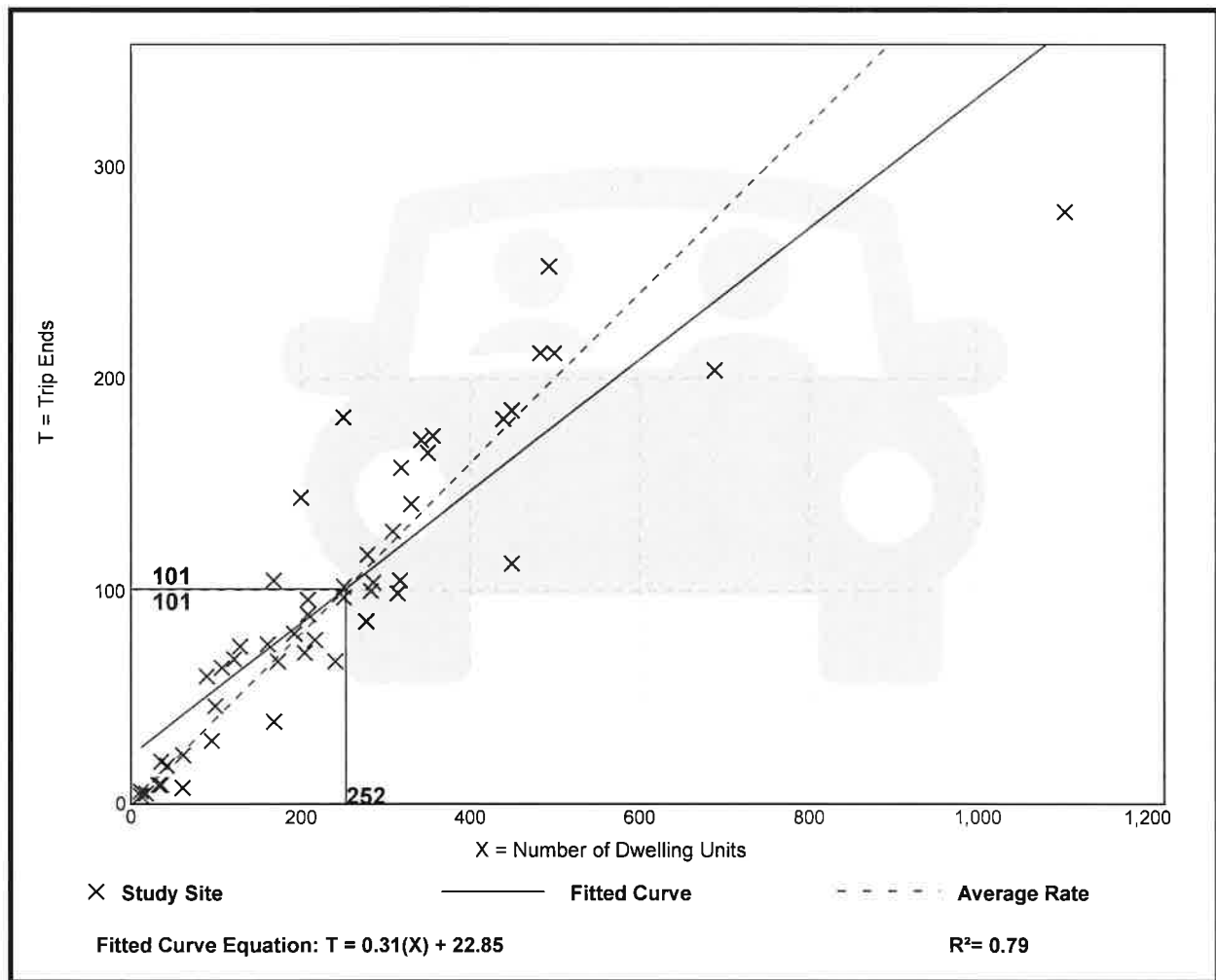
Avg. Num. of Dwelling Units: 249

Directional Distribution: 24% entering, 76% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.40	0.13 - 0.73	0.12

Data Plot and Equation



Multifamily Housing (Low-Rise) Not Close to Rail Transit (220)

Vehicle Trip Ends vs: Dwelling Units
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 59

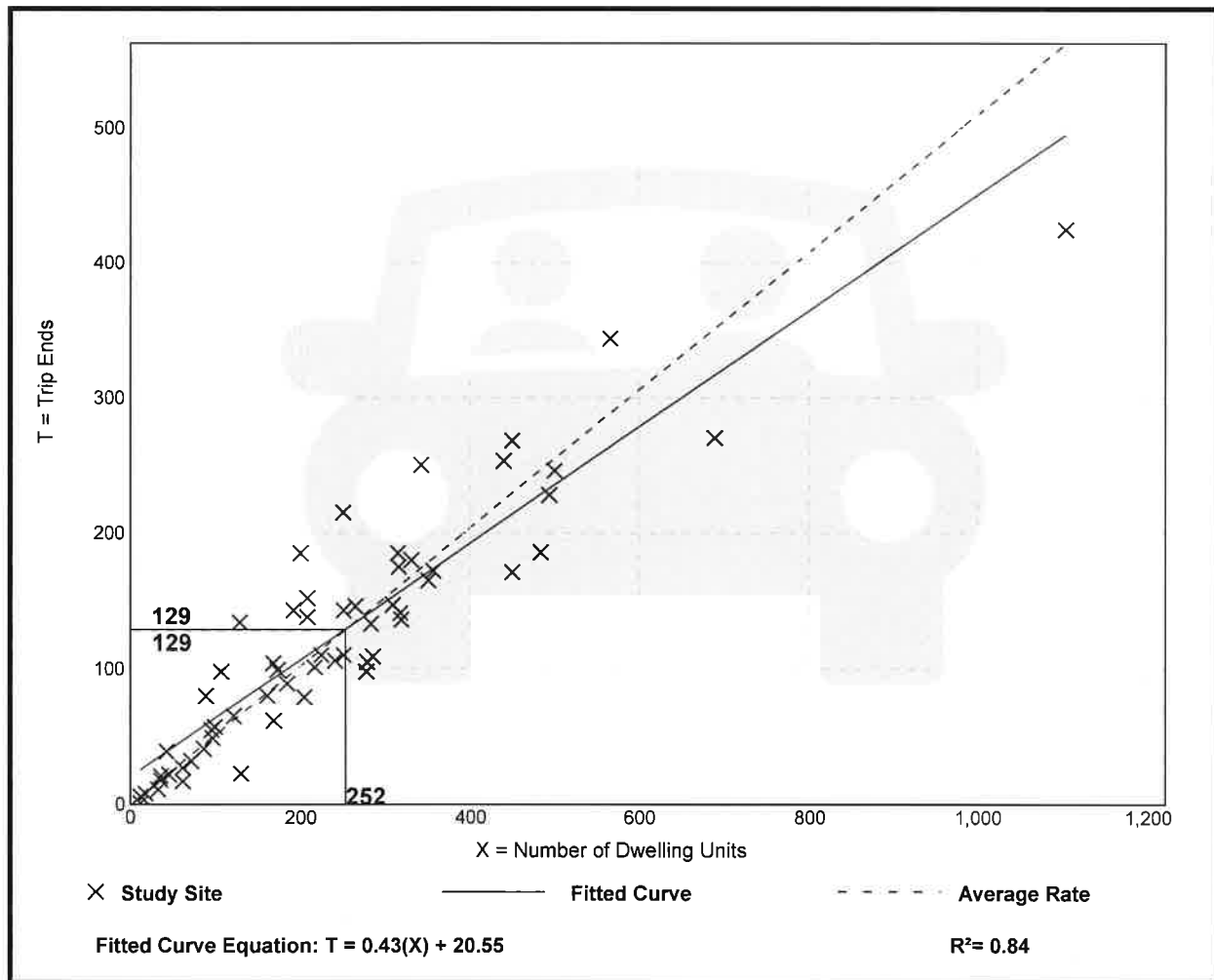
Avg. Num. of Dwelling Units: 241

Directional Distribution: 63% entering, 37% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.51	0.08 - 1.04	0.15

Data Plot and Equation



Hotel (310)

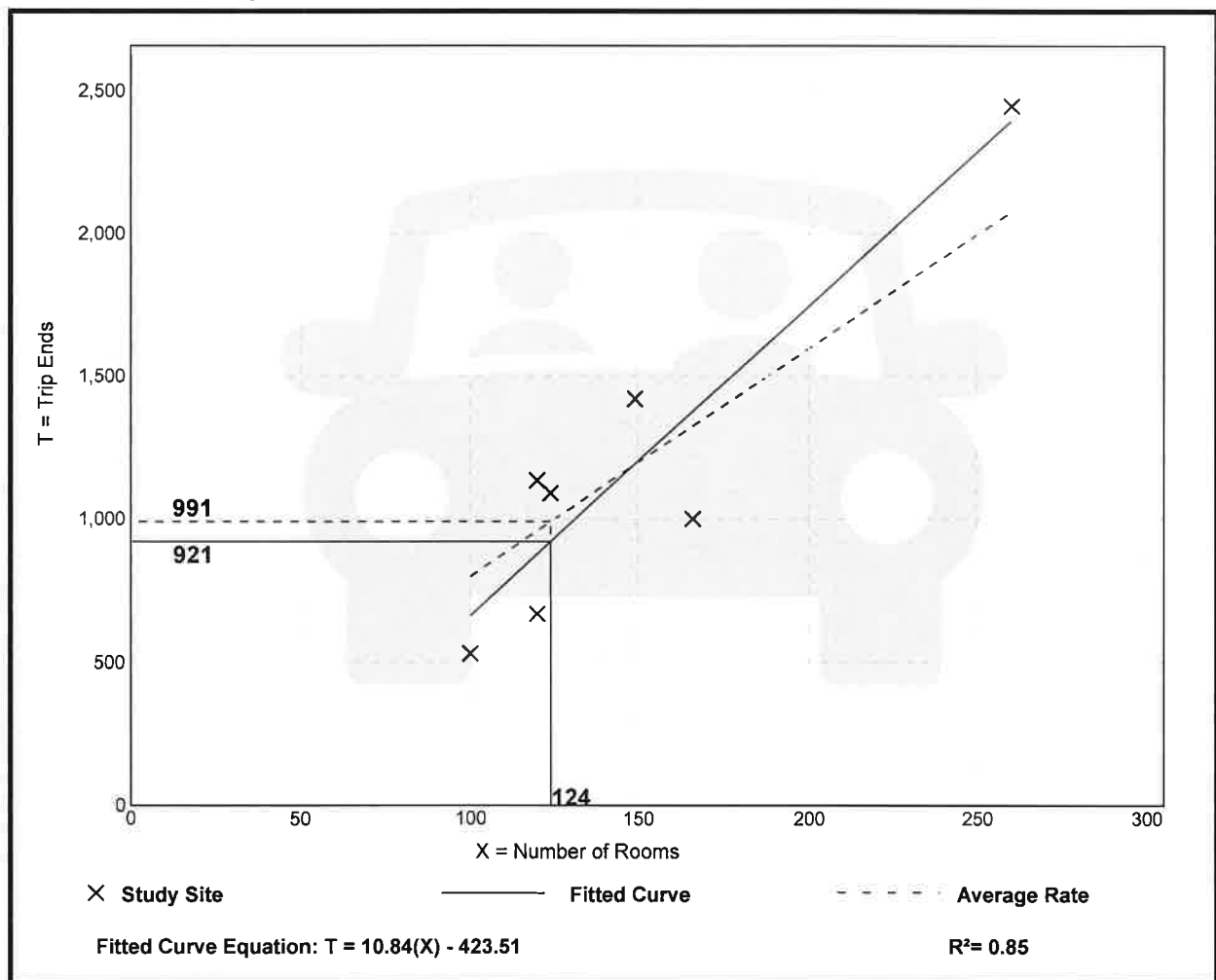
Vehicle Trip Ends vs: Rooms
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 7
Avg. Num. of Rooms: 148
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Room

Average Rate	Range of Rates	Standard Deviation
7.99	5.31 - 9.53	1.92

Data Plot and Equation



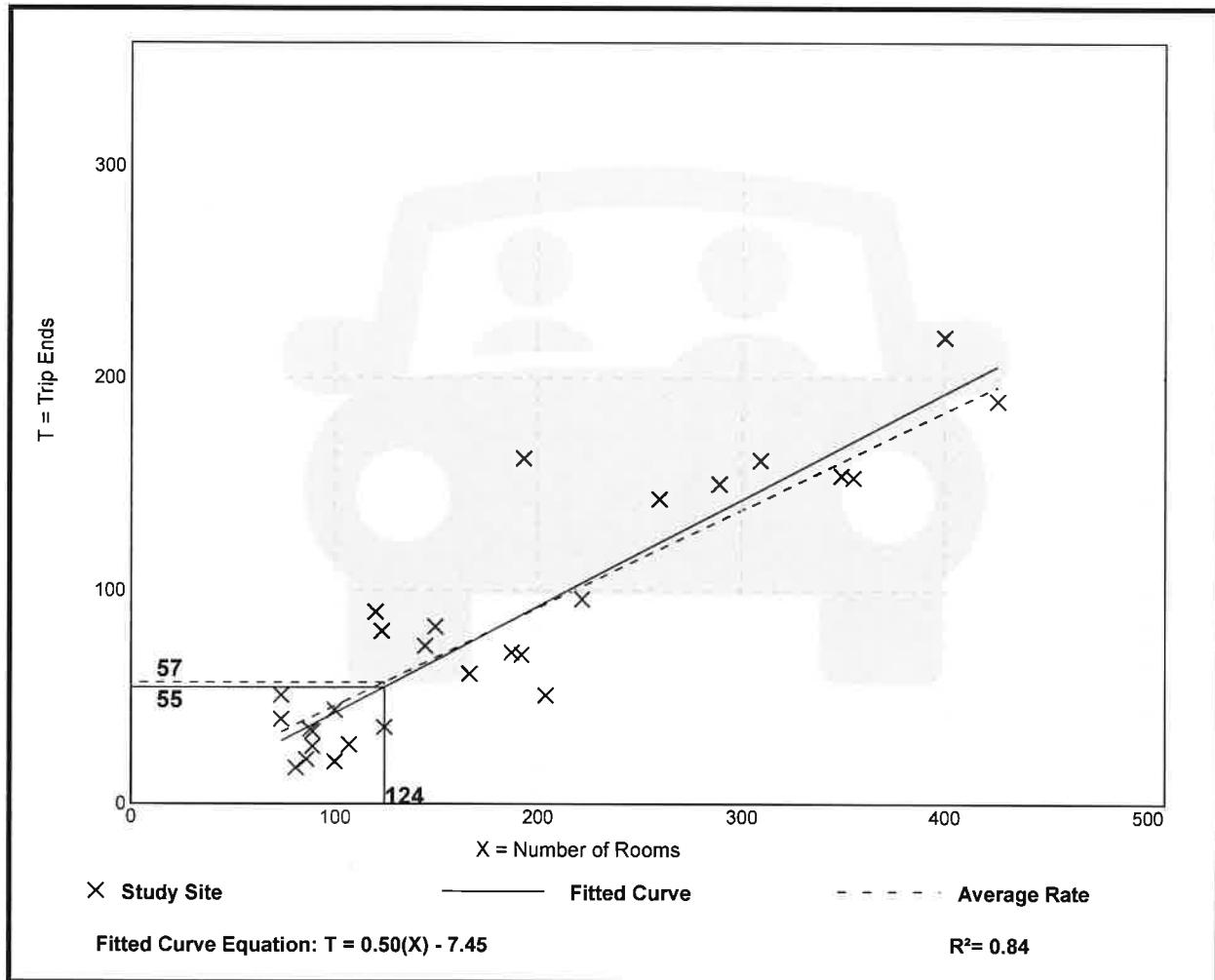
Hotel (310)

Vehicle Trip Ends vs: Rooms
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 28
 Avg. Num. of Rooms: 182
 Directional Distribution: 56% entering, 44% exiting

Vehicle Trip Generation per Room

Average Rate	Range of Rates	Standard Deviation
0.46	0.20 - 0.84	0.14

Data Plot and Equation



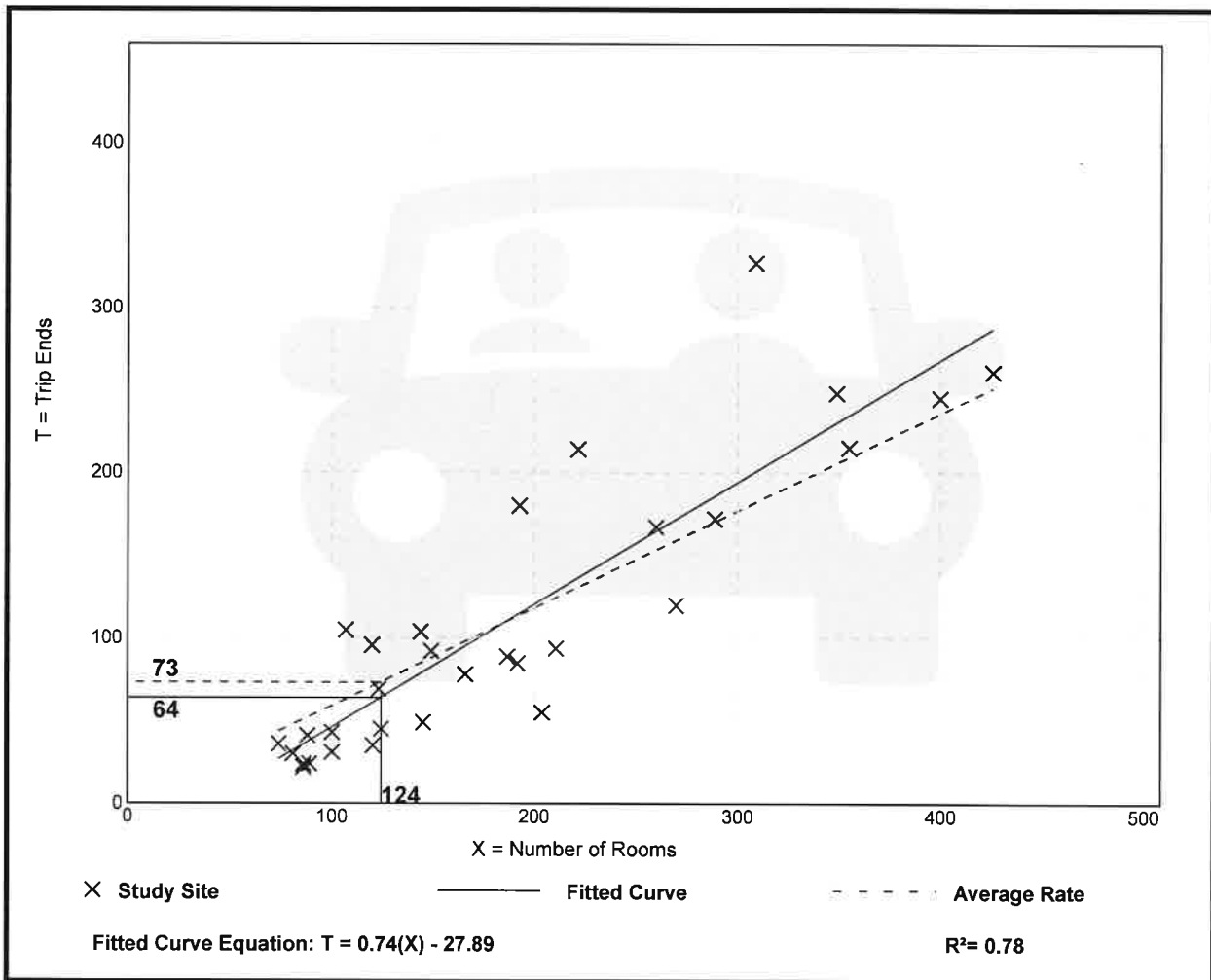
Hotel (310)

Vehicle Trip Ends vs: Rooms
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 31
 Avg. Num. of Rooms: 186
 Directional Distribution: 51% entering, 49% exiting

Vehicle Trip Generation per Room

Average Rate	Range of Rates	Standard Deviation
0.59	0.26 - 1.06	0.22

Data Plot and Equation



Shopping Plaza (40-150k) - Supermarket - Yes (821)

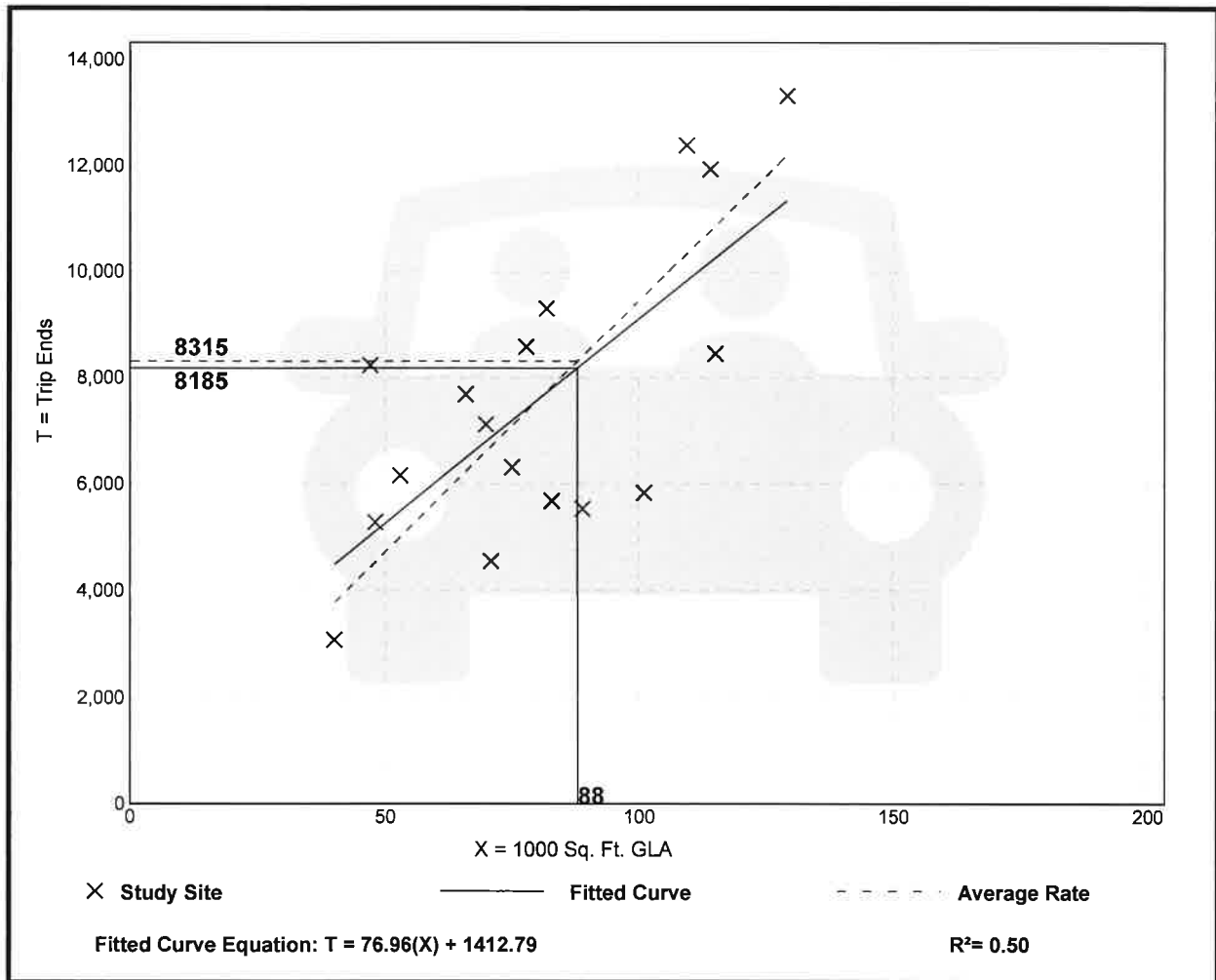
Vehicle Trip Ends vs: 1000 Sq. Ft. GLA
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 17
Avg. 1000 Sq. Ft. GLA: 81
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
94.49	57.86 - 175.32	26.55

Data Plot and Equation



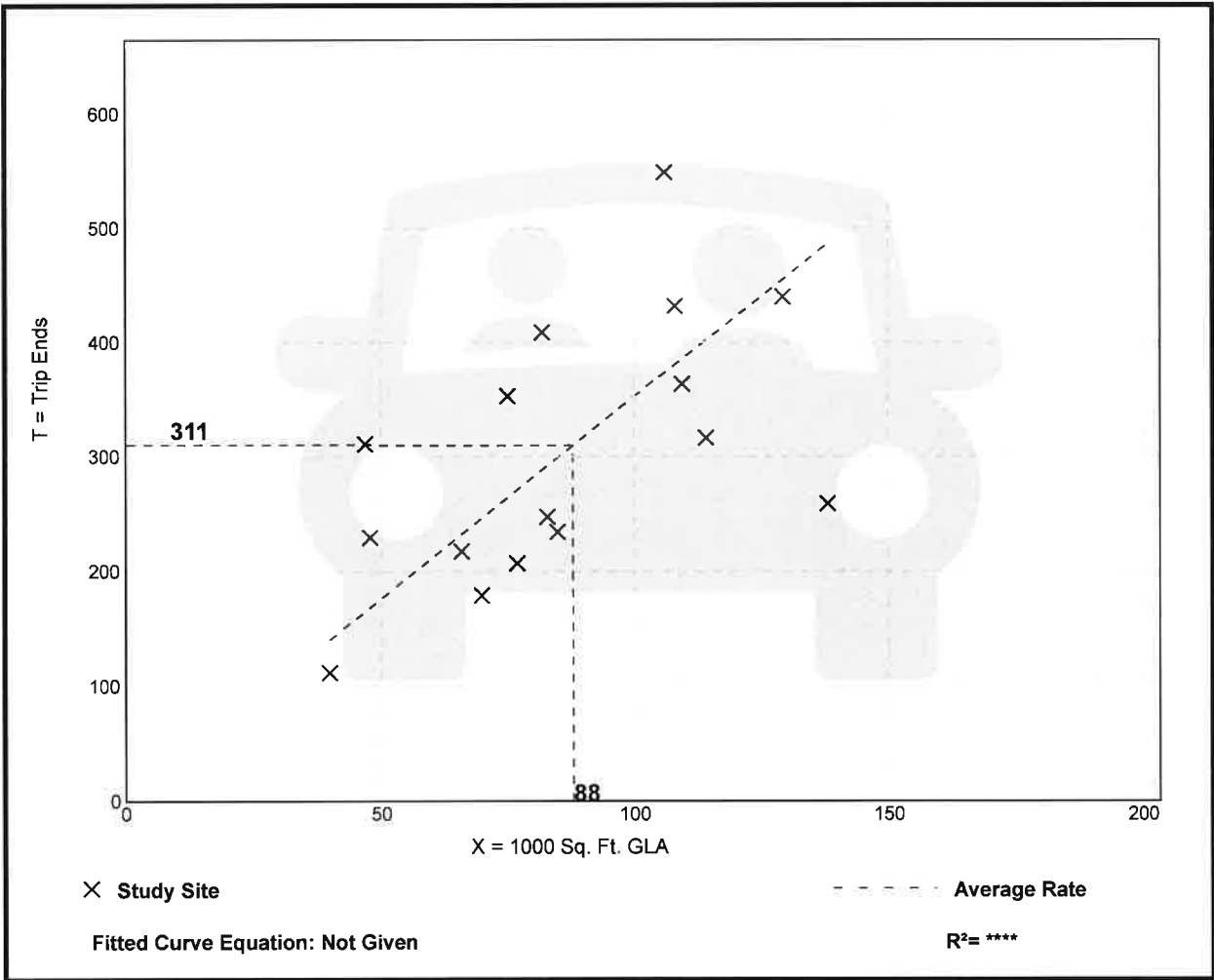
Shopping Plaza (40-150k) - Supermarket - Yes (821)

Vehicle Trip Ends vs: 1000 Sq. Ft. GLA
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 16
 Avg. 1000 Sq. Ft. GLA: 86
 Directional Distribution: 62% entering, 38% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
3.53	1.88 - 6.62	1.17

Data Plot and Equation



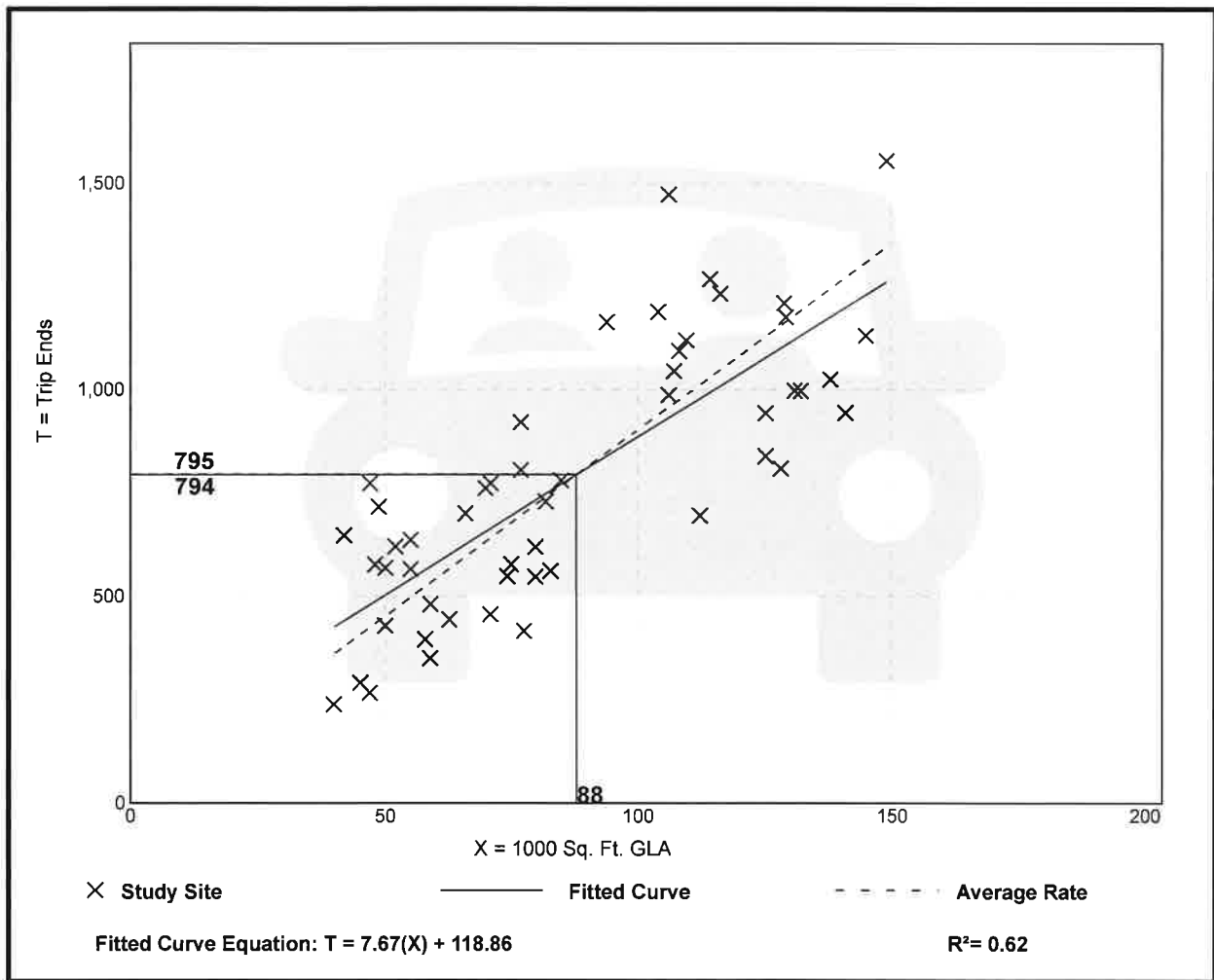
Shopping Plaza (40-150k) - Supermarket - Yes (821)

Vehicle Trip Ends vs: 1000 Sq. Ft. GLA
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 51
 Avg. 1000 Sq. Ft. GLA: 87
 Directional Distribution: 48% entering, 52% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
9.03	5.35 - 16.45	2.37

Data Plot and Equation



Strip Retail Plaza (<40k) (822)

**Vehicle Trip Ends vs: 1000 Sq. Ft. GLA
On a: Weekday**

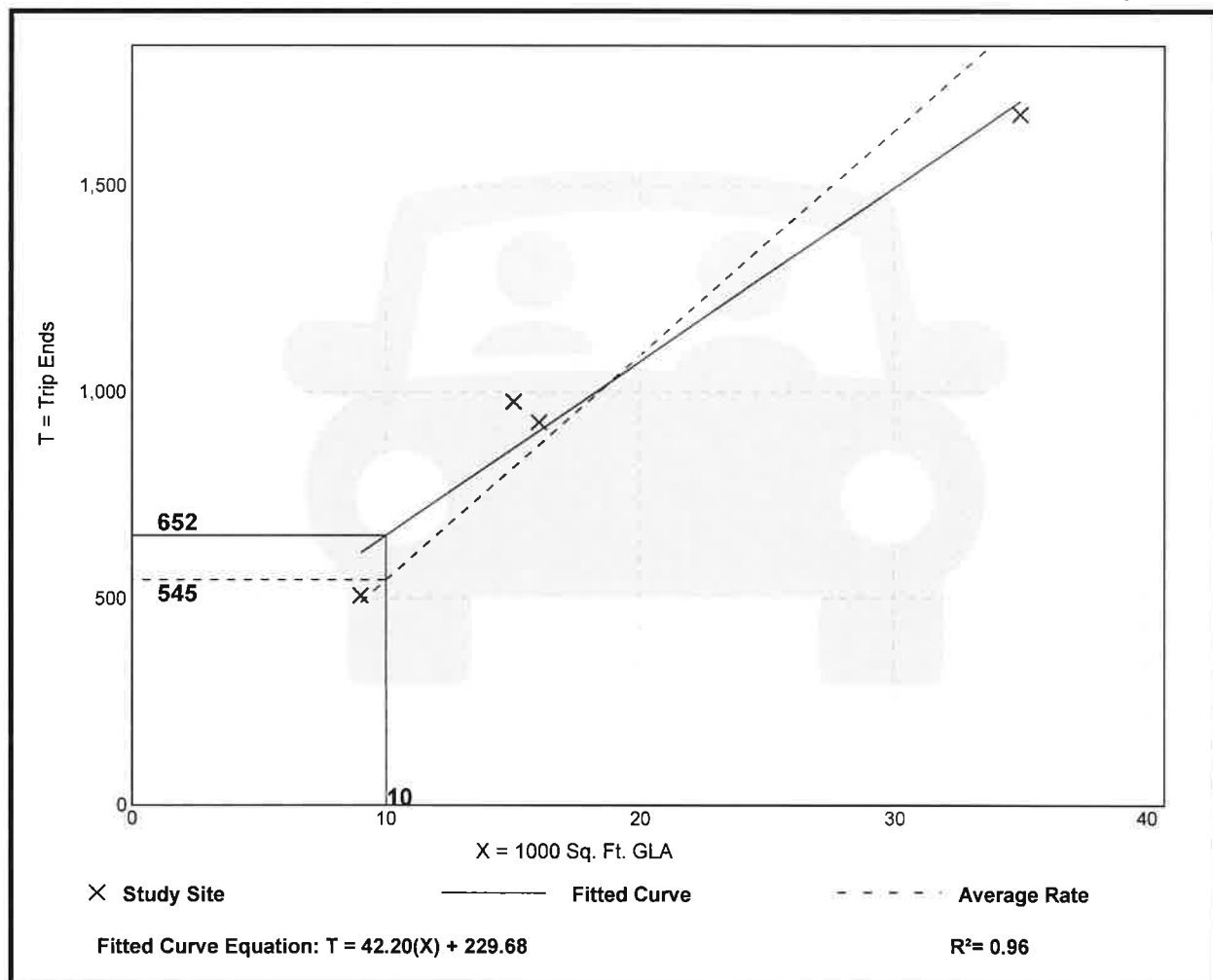
Setting/Location: General Urban/Suburban
 Number of Studies: 4
 Avg. 1000 Sq. Ft. GLA: 19
 Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
54.45	47.86 - 65.07	7.81

Data Plot and Equation

Caution – Small Sample Size



Strip Retail Plaza (<40k) (822)

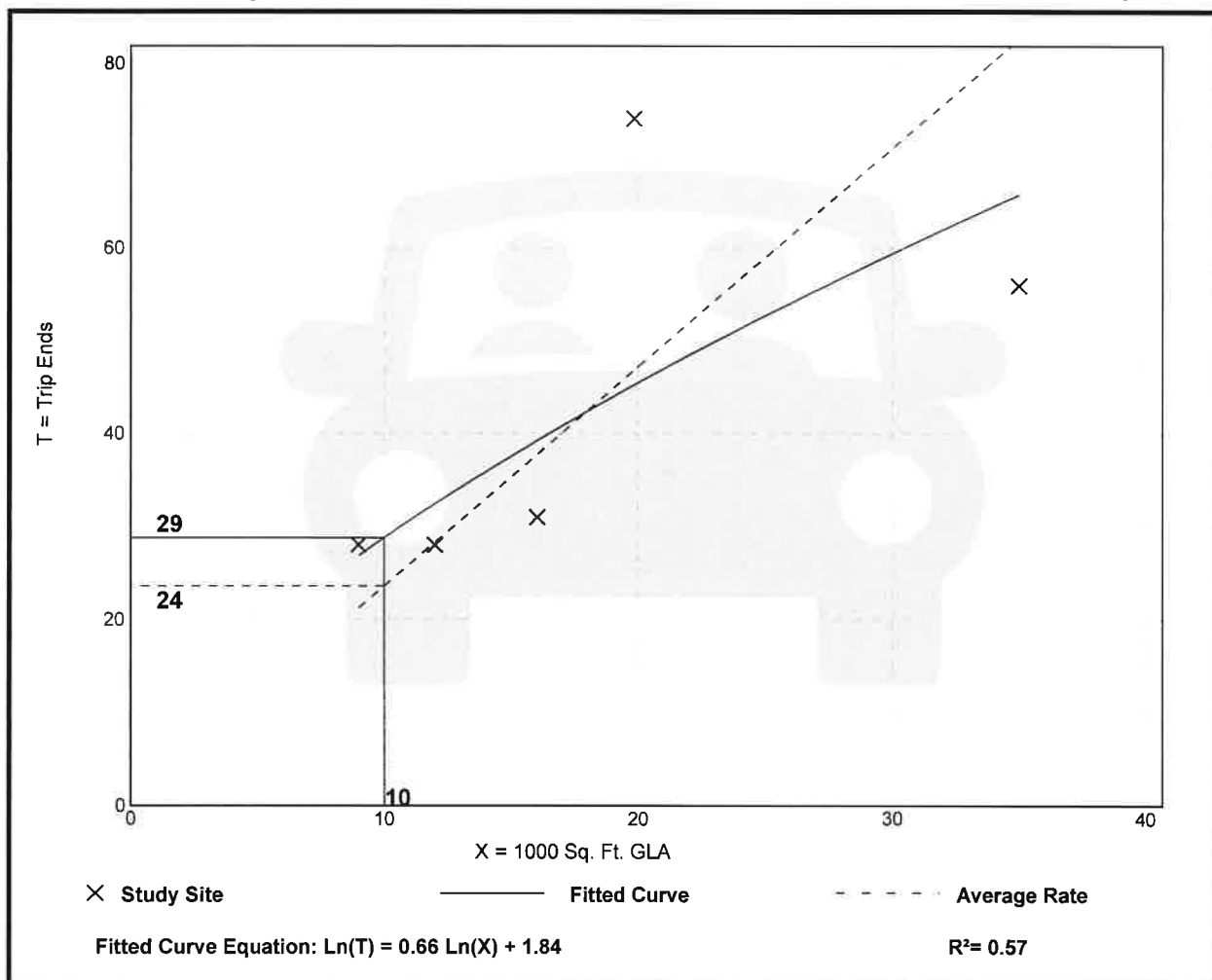
Vehicle Trip Ends vs: 1000 Sq. Ft. GLA
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 5
 Avg. 1000 Sq. Ft. GLA: 18
 Directional Distribution: 60% entering, 40% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
2.36	1.60 - 3.73	0.94

Data Plot and Equation

Caution – Small Sample Size



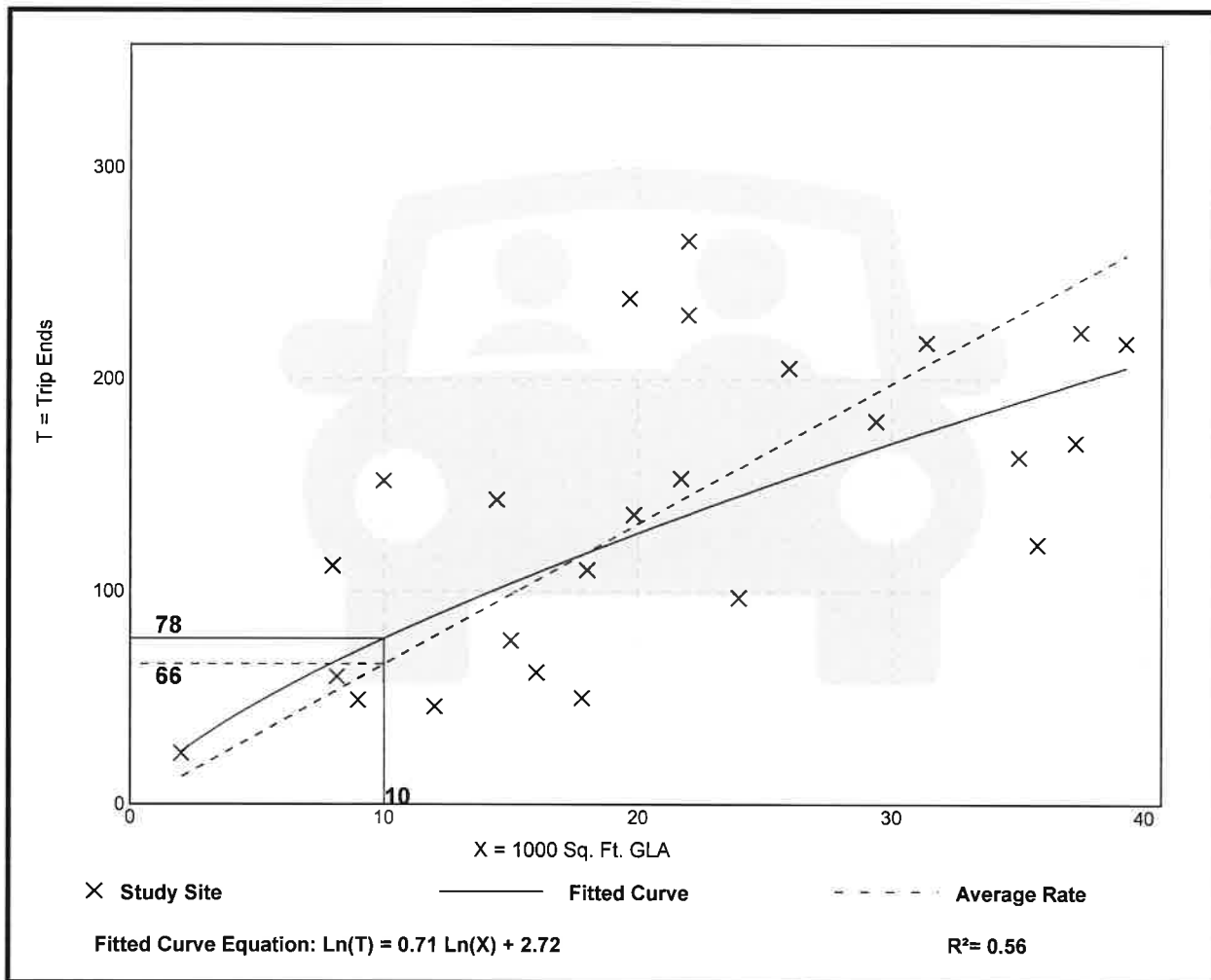
Strip Retail Plaza (<40k) (822)

Vehicle Trip Ends vs: 1000 Sq. Ft. GLA
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 25
 Avg. 1000 Sq. Ft. GLA: 21
 Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
6.59	2.81 - 15.20	2.94

Data Plot and Equation



Drive-in Bank (912)

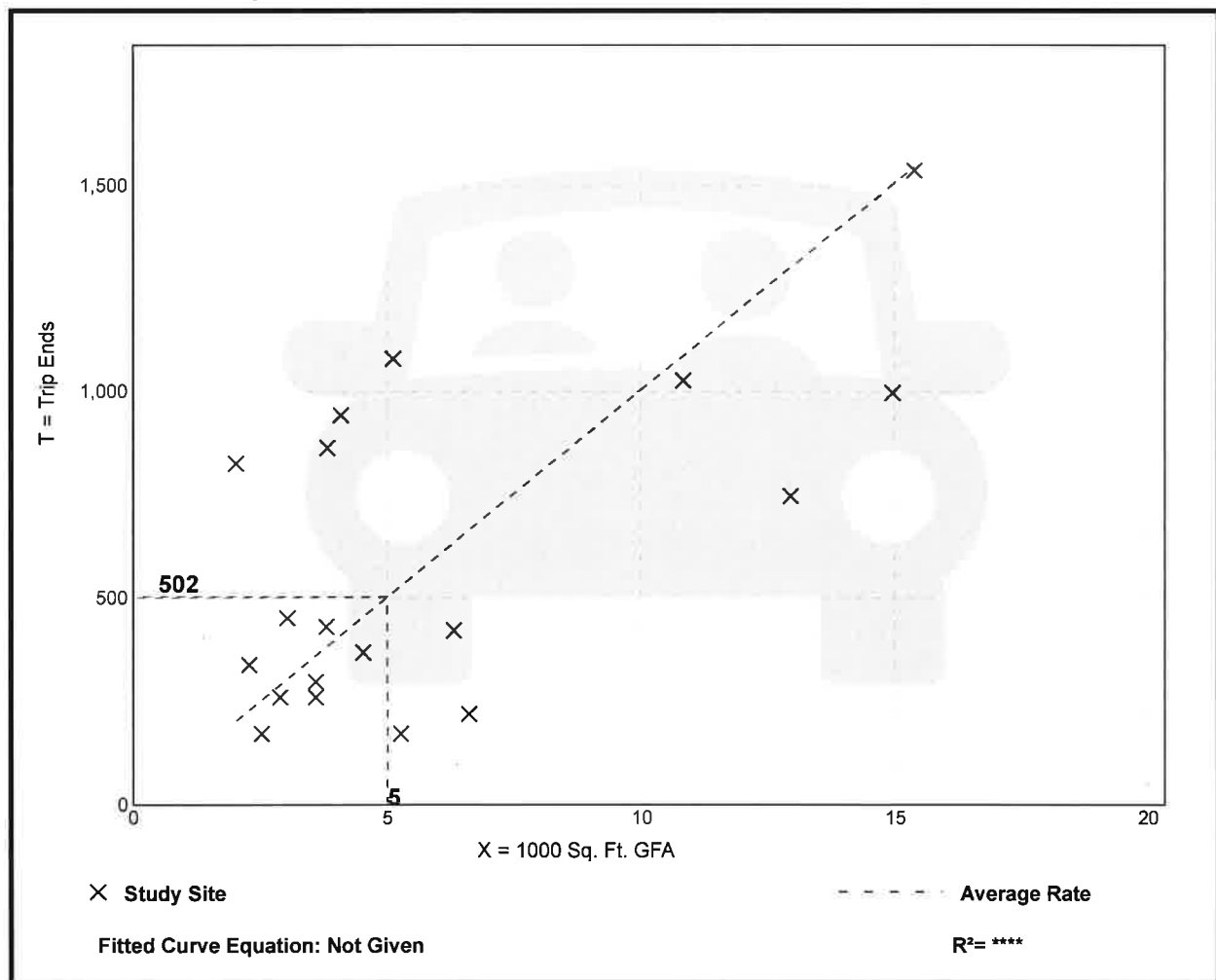
Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 19
Avg. 1000 Sq. Ft. GFA: 6
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
100.35	32.67 - 408.42	68.62

Data Plot and Equation



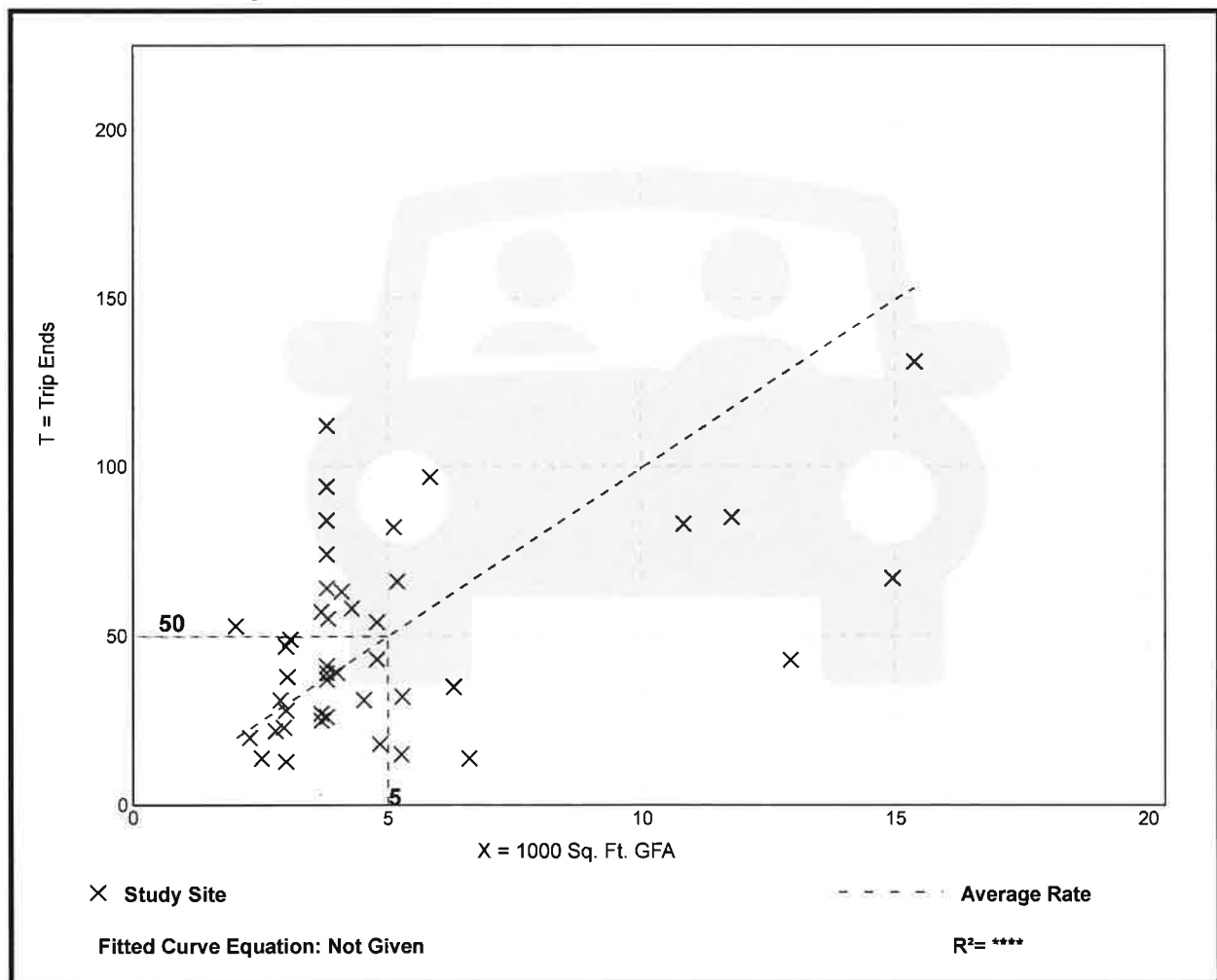
Drive-in Bank (912)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 44
 Avg. 1000 Sq. Ft. GFA: 5
 Directional Distribution: 58% entering, 42% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
9.95	2.12 - 29.47	6.00

Data Plot and Equation



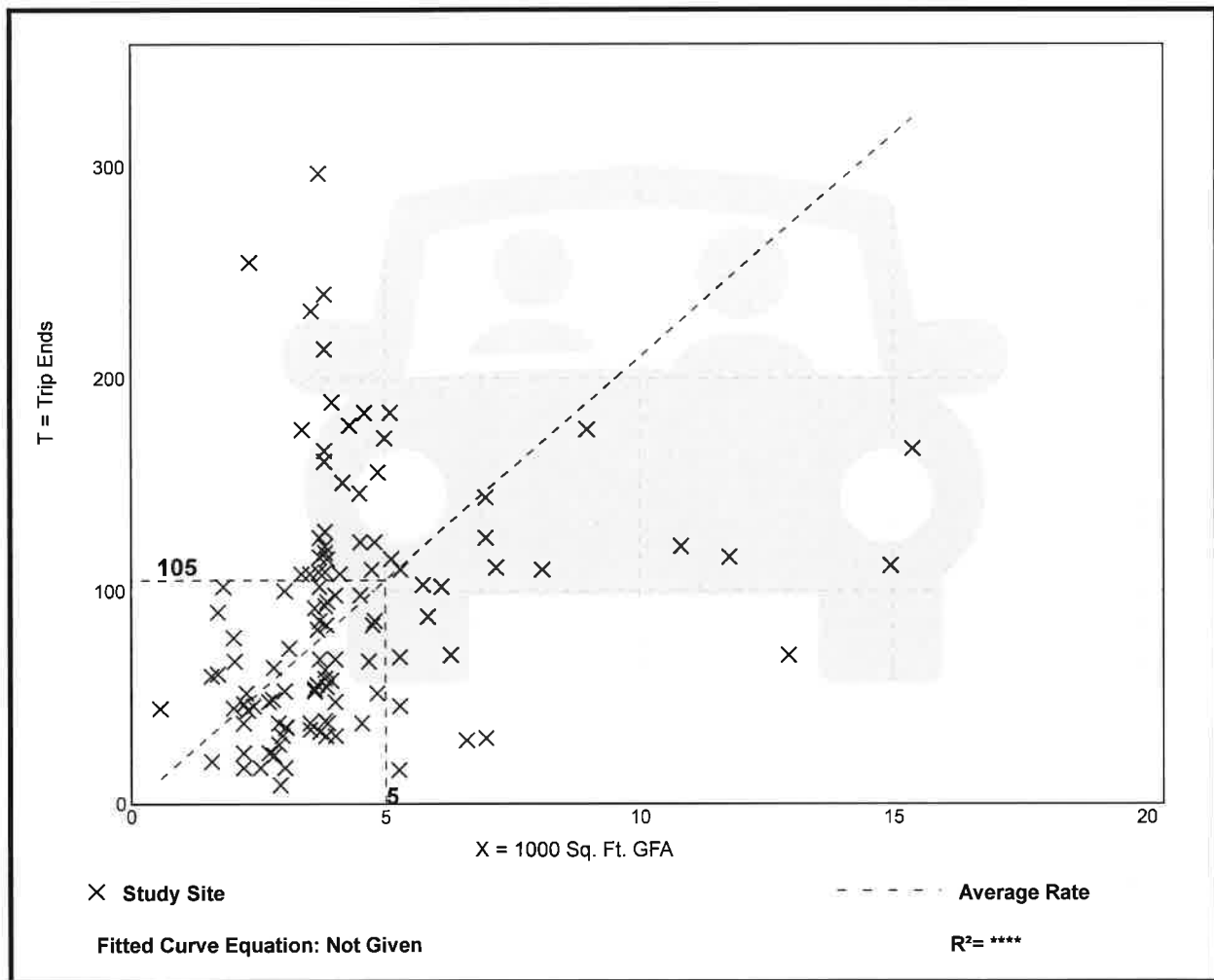
Drive-in Bank (912)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 114
 Avg. 1000 Sq. Ft. GFA: 4
 Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
21.01	3.04 - 109.91	15.13

Data Plot and Equation



Convenience Store/Gas Station - GFA (4-5.5k) (945)

Vehicle Trip Ends vs: Vehicle Fueling Positions
On a: Weekday

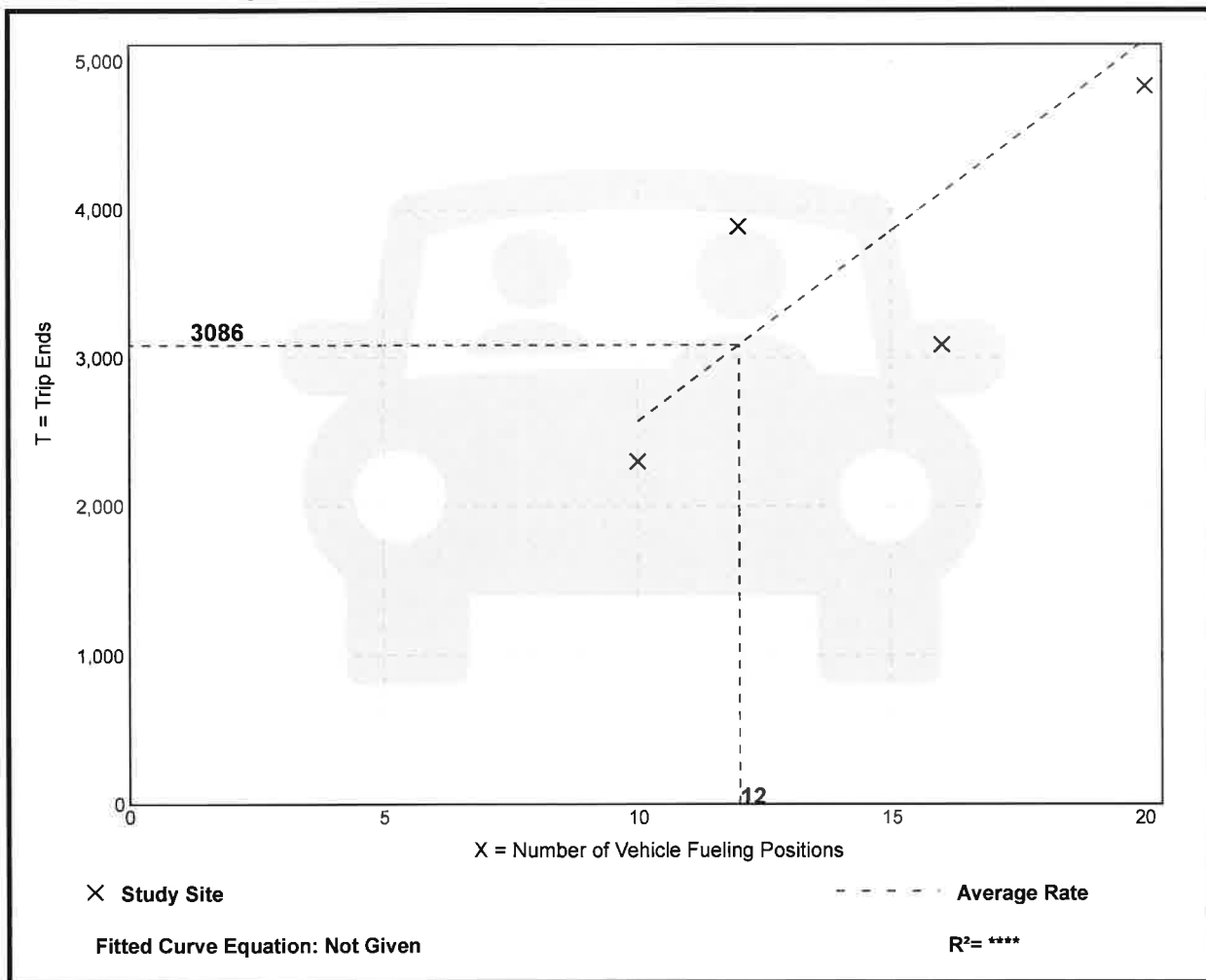
Setting/Location: General Urban/Suburban
Number of Studies: 5
Avg. Num. of Vehicle Fueling Positions: 14
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Vehicle Fueling Position

Average Rate	Range of Rates	Standard Deviation
257.13	193.00 - 324.17	57.53

Data Plot and Equation

Caution – Small Sample Size



Convenience Store/Gas Station - GFA (4-5.5k) (945)

Vehicle Trip Ends vs: Vehicle Fueling Positions
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.

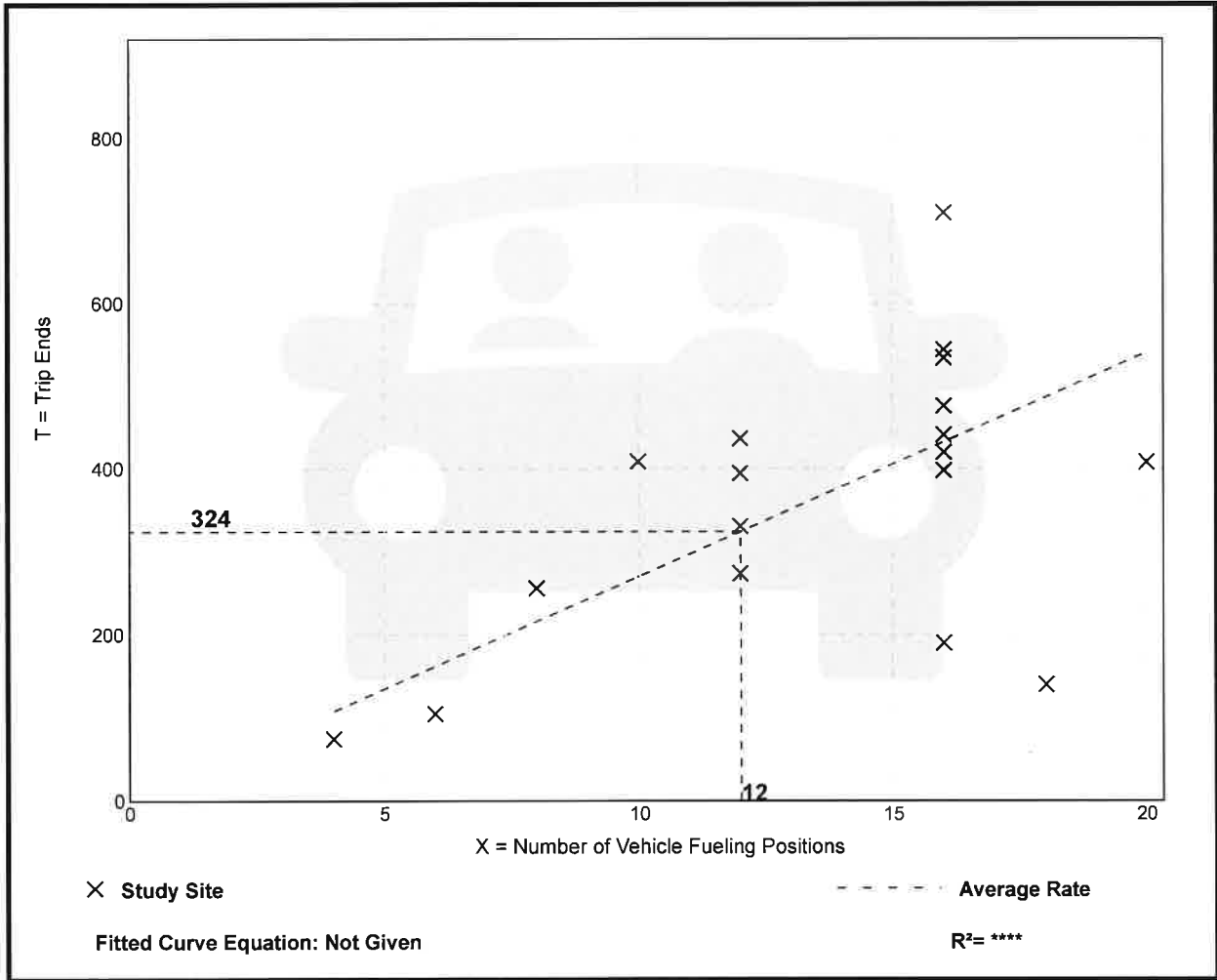
Setting/Location: General Urban/Suburban

Number of Studies: 18
 Avg. Num. of Vehicle Fueling Positions: 13
 Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Vehicle Fueling Position

Average Rate	Range of Rates	Standard Deviation
27.04	7.78 - 44.38	9.88

Data Plot and Equation



Convenience Store/Gas Station - GFA (4-5.5k) (945)

Vehicle Trip Ends vs: Vehicle Fueling Positions
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.

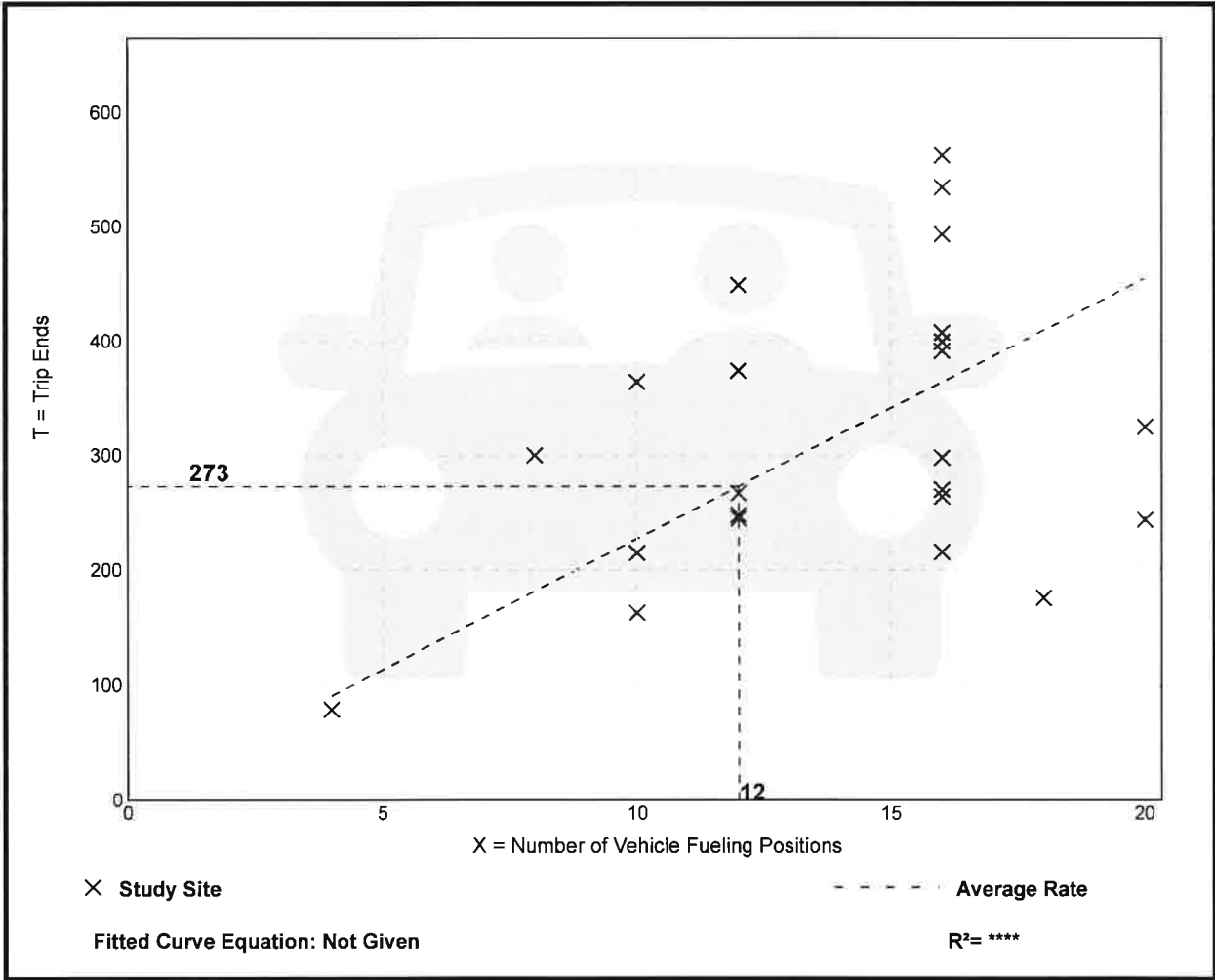
Setting/Location: General Urban/Suburban

Number of Studies: 23
 Avg. Num. of Vehicle Fueling Positions: 14
 Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Vehicle Fueling Position

Average Rate	Range of Rates	Standard Deviation
22.76	9.78 - 37.50	8.49

Data Plot and Equation



A written description of the surface water management plan that includes:

- a. The runoff characteristics of the property in its existing state.

Runoff is generally from the south to the north to the river area. Most of the site is relatively high compared to surrounding areas.

- b. In general terms, the drainage concept proposed, including the outfall to canals or natural water bodies, including how drainage flows from adjacent properties will be maintained.

The proposed water management system will consist of either wet or dry detention (or combination of both) to treat and attenuate the 25 yr storm event. Water quality treatment (150% of calculated SFWMD requirement) will be provided. The previously issued permit for the site allows for discharge to the river by way of an onsite ditch. No change is anticipated for new permit. Refer to the stormwater plan for approximate location of the stormwater control structure. There are no offsite flows entering the property. ICPR routing will be provided at the time of DO permitting.

- c. The retention features (including existing natural features) that will be incorporated into the drainage system and the legal mechanism which will guarantee their maintenance.

Lake(s) and/or dry detention will be created and incorporated into the system. There will be one property owners' association governing maintenance.

- d. How existing natural features will be preserved. Include an estimate of the ranges of existing and post development water table elevations, where appropriate.

There are no natural features to preserve other than heritage trees. They shall be incorporated where feasible. The permitted control elevation shall be utilized.

- e. If the property is subject to seasonal inundation or subject to inundation by a stream swollen by the rains of a 100-year storm event, indicate the measures that will be taken to mitigate the effects of expectable flooding.

The project previously received a map revision from FEMA to alter the floodway line within the project. As part of the SFWMD permitting, floodplain compensation will be provided onsite with shallow excavation of existing higher areas. These areas will not be utilized for onsite attenuation or treatment, only for river overbank flooding.

ENVIRONMENTAL DATA

BONITA - 75

LEE COUNTY, FLORIDA

MAY 2021

Prepared By:

**Collier Environmental Consultants, Inc.
3211 68th Street SW
Naples, Florida 34105
(239) 263 - 2687
marcoe@prodigy.net**

The site consists of approximately 20.93 acre and is in Section 31, Township 47 S, Range 26 E in Lee County (Bonita Springs), Florida. The subject parcel is located at the northeast corner of Interstate 75 and Bonita Beach Road in Bonita Springs.

This parcel has been previously cleared and developed. The entire parcel has signs of past disturbance. Remnants of old structures still exists. In addition, there are some other surface waters on and near the site. To the north is an existing FDOT stormwater treatment area and on site is an excavated man-made tributary of the Kehl Canal.

This parcel consists of several parcels. Several of these parcels had homes with accessory structures. Some of these parcels were partially cleared. Remnants of structures / debris are still evident in some areas. The area does have some established trees. The site does have scattered live oaks, Slash pines with scattered cabbage palms. The ground cover is predominantly cleared. The ground cover consists of grasses mostly pioneer, and secondary growth species. Again, the site does not have what would be called native habitat rather there are some scattered canopy trees. Indigenous vegetation means those plants which are characteristic of the major plant communities of the city. As such a tree survey was conducted.

This project was previously permitted with South Florid Water Management District Permit # 36-07489-P. There are no wetlands located within or affected by the proposed project.

Policy 7.2.2 a) Large developments must provide 50 percent of their open space percent requirement using existing indigenous native vegetation.

Indigenous vegetation means those plants which are characteristic of the major plant communities of the city.

This site does not have any indigenous native vegetation as defined. The site does have scattered native trees.

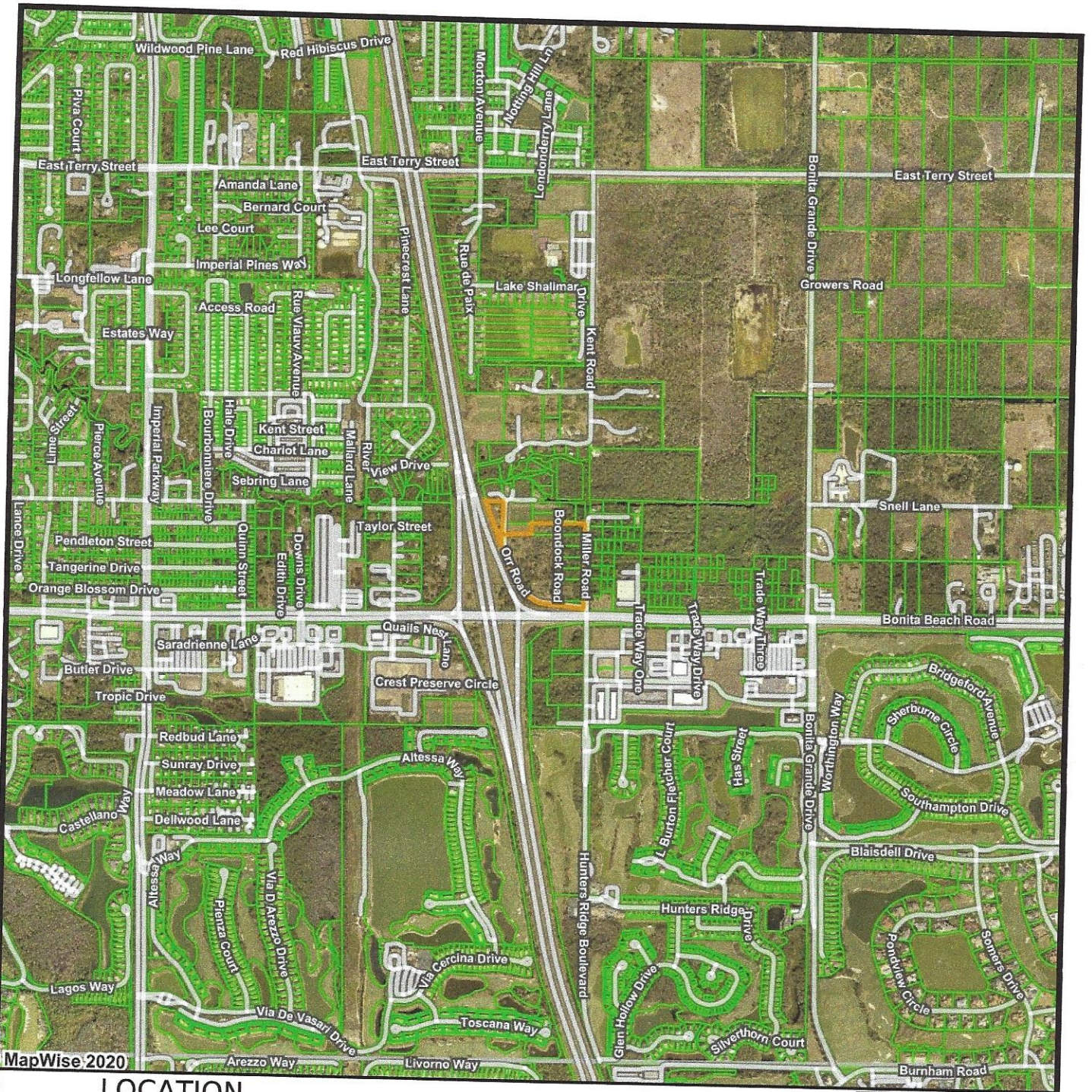
LDC Sec 3-417 Open Space

(b) (1) b. If the development area does not contain existing indigenous native vegetation communities, but does contain existing indigenous native tress, then 50 % of their open space percentage requirement must be met through the on-site preservation of existing native trees consistent with requirement must be met through the on-site preservation of existing native trees consistent with

(b)(1)(b) 4. Effort must be made to preserve heritage trees (live oak, South Florida slash pine, or longleaf pine with minimum 20-inch caliper dbh). If a heritage tree must be removed from a site then a replacement tree with a minimum 20-foot height must be planted within an appropriate open space area

Two heritage tree surveys have been conducted. Boylan Environmental conducted a study on July 2008. Collier Environmental Consultants Inc. verified and conducted their own inventory. (18) Live oaks, (4) Slash Pines

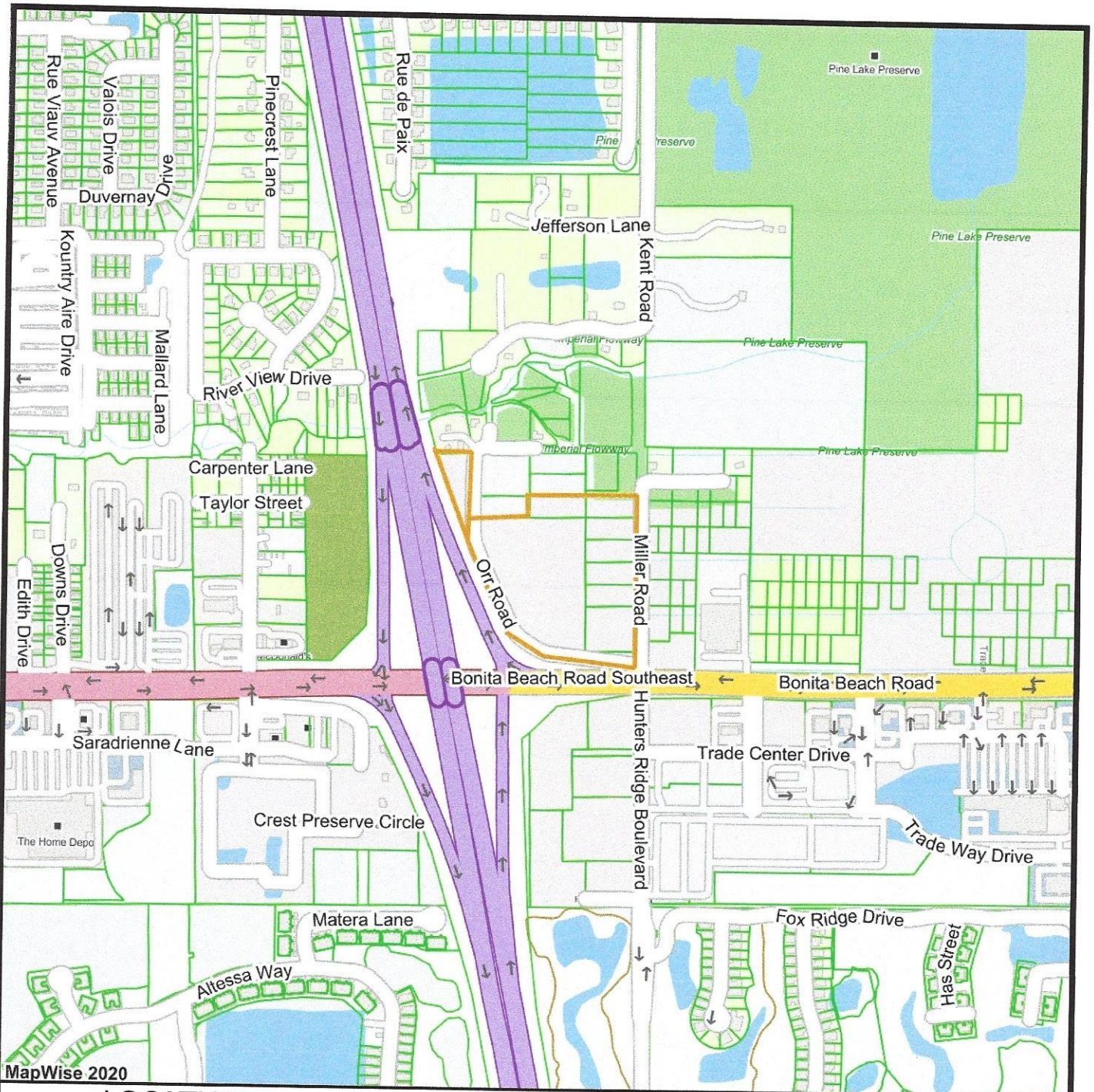
Included with this brief summary are of exhibits of existing photos and some historical aerals.



LOCATION



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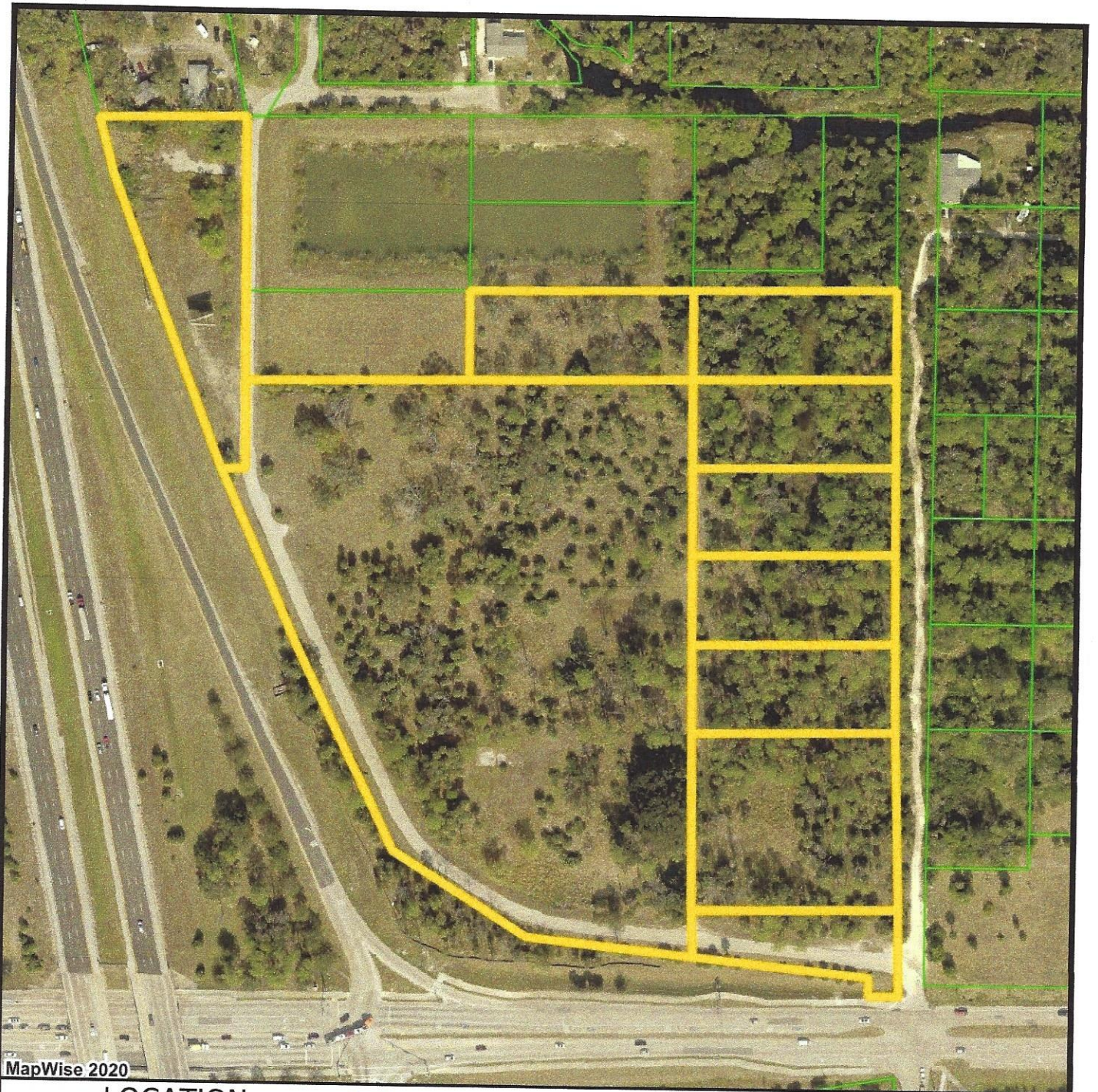
MapWise 2020

LOCATION

- Labels Drawing
- Points Drawing
- Lines Drawing
- Polygons Drawing
- Selected Custom Parcels
- County Boundaries
- Parcel Outlines



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MapWise 2020

LOCATION

- Selected Custom Parcels
- County Boundaries
- Parcel Outlines



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MapWise 2020

FLUCCS CODE

- Labels Drawing
- Points Drawing
-  Lines Drawing
- Polygons Drawing
-  Selected Custom Parcels
-  Streets OSM Hybrid
-  County Boundaries
-  Parcel Outlines



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FLUCCS CODE AND VEGETATION INVENTORY

FLUCCS CODE	(Description)	Scientific Name	Indicator Status
Common Name			

111 Single Family Homes

These are parcels with single family homes on them. These are along the north end of the subject parcel.

500 Other Surface Water

This is areas that fill has been excavated leaving impoundments.

740 Disturbed Lands

This parcel consists of several parcels. Several of these parcels had homes with accessory structures. Several of these parcels were partially cleared. Remnants of debris are still evident in some areas. The area has some established live oaks, Slash pines with scattered cabbage palms. The ground cover is predominantly cleared. The ground cover consists of grasses, pioneer, and secondary growth species. There are a several native trees such as live oaks and Slash pine.

Live oak	<i>Quercus virginiana</i>	FACU	Cp,O
Slash pine	<i>Pinus elliotti</i>	FACW	Cp,O
Cabbage palm	<i>Sabal palmetto</i>	FAC	M,O
Ear leaf acacia	<i>Acacia auriculiformis</i>	Exotic	M,C
java plum	<i>Syzygium cumini</i>	Exotic	Cp,M,C
Brazilian pepper	<i>Schinus terebinthifolius</i>	Exotic	M,C
Guineagrass	<i>Panicum maximum</i>	FAC-	G,C
Bahia grass	<i>Paspalum notatum</i>	UPL	G,C
Boston fern	<i>Nephrolepis exaltata</i>	FAC	G,O
Sida	<i>Sida acuta</i>	UPL	G,O
Bahia grass	<i>Paspalum notatum</i>	UPL	G,C
Walters ground cherry	<i>Physalis wateri</i>	UPL	G,O
Lantana	<i>Lantana camara</i>	FACU	G,O
Sandspur	<i>Cenchrus echinatus</i>	FAC	G,C
Beggars ticks	<i>Bidens alba</i>	FACW	G,O
Beauty bush	<i>Callicarpa Americana</i>	UPL	M,O
Air potato	<i>Dioscorea bulbifera</i>	Exotic	M,O
Whitehead broom	<i>Spermacoce verticillata</i>	FAC	G,C
Chocolate weed	<i>Melochia corchorifolia</i>	FAC	G,O
Broom sedge	<i>Andropogon virginicus</i>	FAC-	G,C
Whitehead broom	<i>Spermacoce verticillata</i>	FAC	G,O
Winged Sumac	<i>Rhus copallina</i>	UPL	M,O

Strawberry guava	<i>Psidium littorsle</i>	Exotic	M,O
Wedelia	<i>Wedelia trilobata</i>	Exotic	G,O
Swamp fern	<i>Blechnum serrulatum</i>	FACW+	G,C
Oyster plant	<i>Rhoeo spathacea</i>	Exotic	G,O
Dog fennel	<i>Eupatorium capillifolium</i>	FACW	G,O
Grape vine	<i>Vitis rotundifolia</i>	FAC	G,O
Virginia creeper	<i>Ampelopsis quinquefolia</i>	FAC	G,O
Cat-briar	<i>Smilax spp.</i>	FAC	G,C






814 Roadway

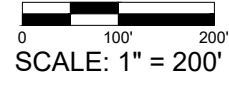
This is a roadway that parallels I-75 giving access to some homes to the north of the subject parcel.

**ABREVIATIONS – Canopy (Cp), Midstory (M), Ground Cover (G)
Dominant (D), Common (C), Occasional (O)**

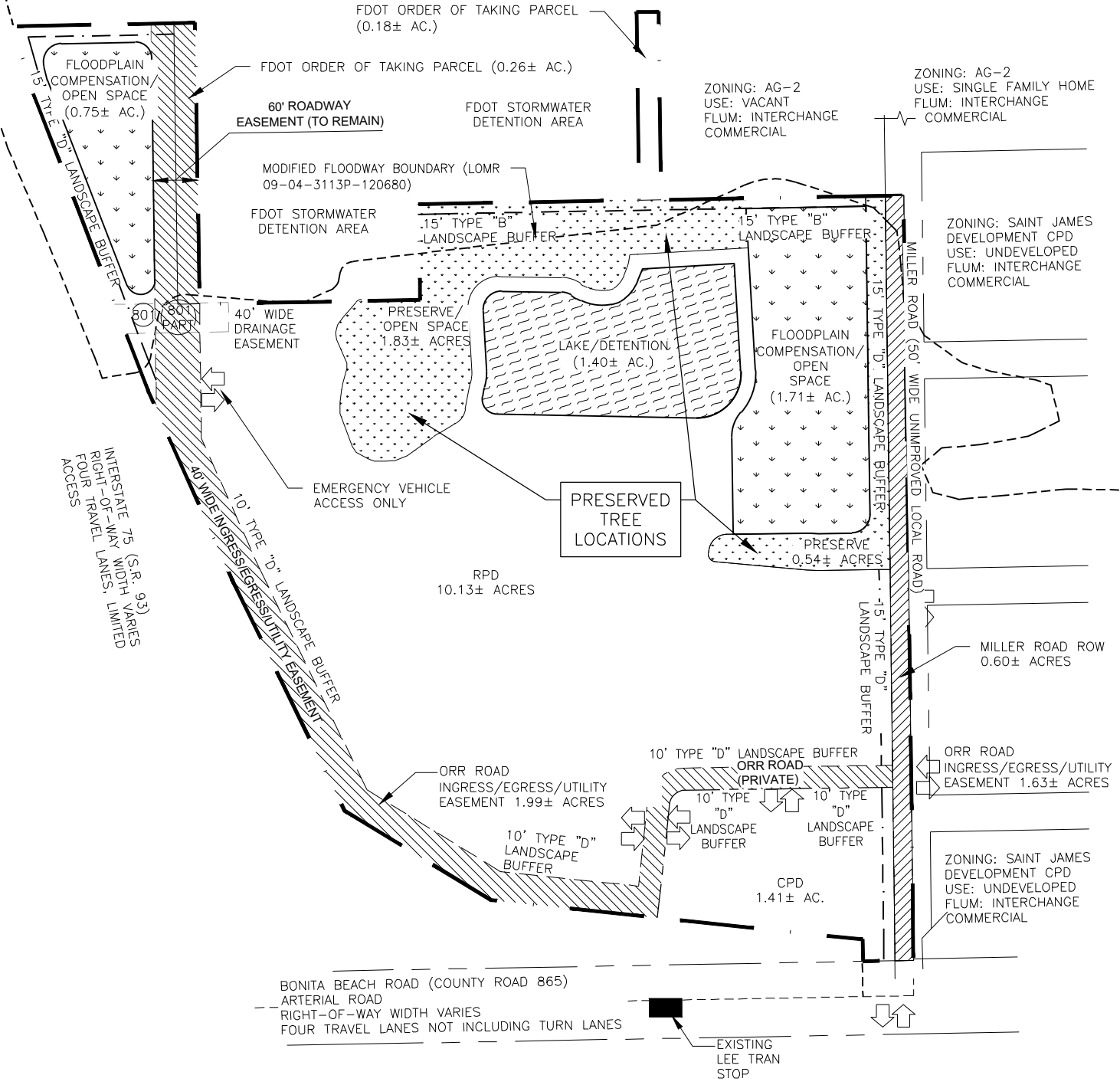
INDICATOR STATUS - Obligate (OBL), Facultative wet plants (FACW), Facultative plants (FAC), Upland (UPL)

LEGEND

-  ACCESS
-  INDIGENOUS PRESERVE
-  EASEMENT
-  FLOODPLAIN COMPENSATION/OPEN SPACE
-  LAKE



ZONING: AG-2
 USE: SINGLE FAMILY HOME
 FLUM: INTERCHANGE
 COMMERCIAL



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 Bonita Springs: 239.947.1144 www.GradyMinor.com Fort Myers: 239.690.4380

BONITA 75 CPD/RPD

TREE PRESERVATION PLAN
 APRIL 11, 2022

SCALE:	1" = 200'
JOB CODE:	B75MC
DATE:	APR 11, 2022
FILE NAME:	B75MC TPP (APRIL 2022)
SHEET	1 OF 1

G:\PLANNING\PROJ - PLANNING\B75MC BONITA 75 CPDA (P021-83966-BOS)\DRAWINGS\B75MC TPP (APRIL 2022)DWG 4/12/2022 8:33 AM

LISTED SPECIES SURVEY

MAY 2021

BONITA -75

BONITA SPRINGS - LEE COUNTY, FLORIDA

Collier Environmental Consultants Inc.
3211 68th Street SW
Naples, Florida 34104
(239) 263-2687
marcoe@prodigy.net

I **Introduction / Purpose**

This report is an account of a listed species survey recently performed on a 20.93 acre tract and on recently added preserve parcels. Its purpose is to identify and describe key habitats and report any listed species utilizing the site that would be at risk due to possible future development actions. This survey and report are based on fieldwork performed during May 2021. This parcel was previously examined in December 2003, May 2005 and July 2008

II **Site Description**

The site consists of approximately 20.93 acre and is in Section 31, Township 47 S, Range 26 E in Lee County (Bonita Springs), Florida. The subject parcel is located at the northeast corner of Interstate 75 and Bonita Beach Road in Bonita Springs.

This parcel has been previously cleared and developed. The entire parcel has signs of disturbance. Remnants of old structures still exists.

See Location Maps Exhibit 1

FLUCCS Code and Descriptions

500	Other Surface Water	0.47 acres.
740	Disturbed Lands	18.68 acres
814	Road	1.78 acres

TOTAL 20.93 acres

This site consists of approximately 20.93 acres. The majority of the site has been impacted by prior usage. The site still has cleared areas with debris. The site does have a few scattered trees. Some very mature specimens. Those include Slash pines, live oaks and cabbage palms. The site does have a variety of exotics. There is also an interesting excavation located towards the north east of the property. There is an excavation with very steep, deep, vertical walls. The vegetation consists of cattails, willow and Brazilian pepper.

III Listed Species Survey

This survey method consisted of overlapping transects in compliance with Lee County Endangered Species Ordinance N 89-34

Wednesday May 19, 2021 - Morning	hot,	low 90's
Thursday May 20, 2021 -Morning, afternoon	nice breeze	high 80's
Friday May 21, 2021 - Afternoon	nice breeze	high 80's

The required survey for a Listed Species Survey calls for a survey covering 100 % of the site, at prescribed transect distances per site acreage. Such a survey was conducted in May 2021. Established transects were oriented north - south and east - west and superimposed on an aerial map of the site. These transects were ground - located and walked by compass bearing. Early morning (0730 - 1000), mid-day (1100 - 1500) and late-day (1500 - 1800) time periods were chosen to survey these transects. All possible species of plants and animals listed by state and federal agencies were noted.

A previous survey and report were performed in December 2003, May 2005, July 2008. Those surveys were performed by another firm and found no listed vertebrate species.

IV Results & Discussions

Listed Flora.

Refer to Exhibit # 3- Cumulative Plant List

Listed Fauna

Refer to Exhibit # 4- Wildlife Species Observed

Key Species Discussion :

Florida Fish and Wildlife Conservation Commission Listed Species Site Analysis has been included.

Red Cockaded Woodpeckers

Red-Cockaded woodpeckers are known to inhabit Pine Flatwoods. Observations were keyed to searching for signs or calls of these animals. All mature pines were checked along the transect routes. Particular attention was paid to the south and west faces of the trees, as that seems to be the predominant location of cavity openings. No individuals or cavity trees were identified during this survey.

Gopher Tortoise

This site does offer some suitable habitat for Gopher tortoise. Searches were keyed in the best areas such as the pine-palmetto areas. Specific attention was given looking along the face of any spoil material mounds. However, no signs or burrows were identified on the subject parcel.

Big Cypress Fox Squirrels

Big Cypress Fox Squirrels are known to use similar habitat as Red - Cockaded Woodpeckers. Observations were keyed to searching for signs or calls of these animals, such as leaf nests in canopy trees or the distinctive chattering of territorial squirrels. No Fox squirrels were identified on site during this survey.

Florida Black Bear

No individuals were observed during this survey even though Black bears are known to inhabit similar habitat. Special attention was for given for signs such as scraps, tracks and scat. This mammal also has a large home range and is known to inhabit surrounding areas.

Bonneted Bat

The Florida bonneted bat is the largest species of bat in Florida. Previously known as the Florida (Wagner's) mastiff bats these bats were reclassified as a separate species unique to Florida. The species can grow to a length of 6.5 inches with a wingspan of 20 inches. This is a flying mammal free to traverse great distances.

Examination of all trees with potential cavities was conducted. The site does have a few dead pines trees. The dead trees seem recent probably caused by Hurricane Irma. Special examination of all these trees were conducted. Specifically, these trees were examined for any cavities and/or snags. No individuals and/or guano were found. The density of Brazilian pepper and other midstory plants in some other areas may impede flight patterns. This project is not anticipated to negatively impact the species.

Florida Panther

No individuals have been documented utilizing this project site or identified during this survey. This species does have a large home range and is known to inhabit surrounding areas. The site is located outside the Secondary Panther Zone. This project is located entirely outside the Panther Consultation Zones. A copy of this has been provided as an exhibit.

Conclusions

Our survey found no listed species on this site. Transects were walked on straight compass bearings along a grid spaced at approximately 40 yards apart for the entire parcel. Other transects were primarily meandering transects through areas of prime habitat. All transects were walked at varying times from post-dawn & mid-day to pre-sunset hours.

This site is at a busy intersection and has been cleared of native habitat. The land has been modified remnants of some old structures are evident. The site does have a few nice, scattered trees such as live oaks, cabbage palms and Slash pine. Again, the entire site has been previously cleared. The ground cover mainly consists of bahia, guinea grass and vines. The area is extremely limited as suitable native habitat. The main vertebrates were birds traversing the site. Prints such as raccoons and opossums were seen.

Due to lack of native habitat and contiguous habitat wildlife utilization is very limited. In addition, this site is at a remarkably busy intersection in the urban area. The nearby roadways and homes provide barriers for some animal movements.

This study found no listed species habitation and/or utilization. Previous surveys performed by another firm were performed in December 2003, May 2005, July 2008. Those surveys found no listed vertebrate species.

Threatened, Endangered and Species of Special Concern

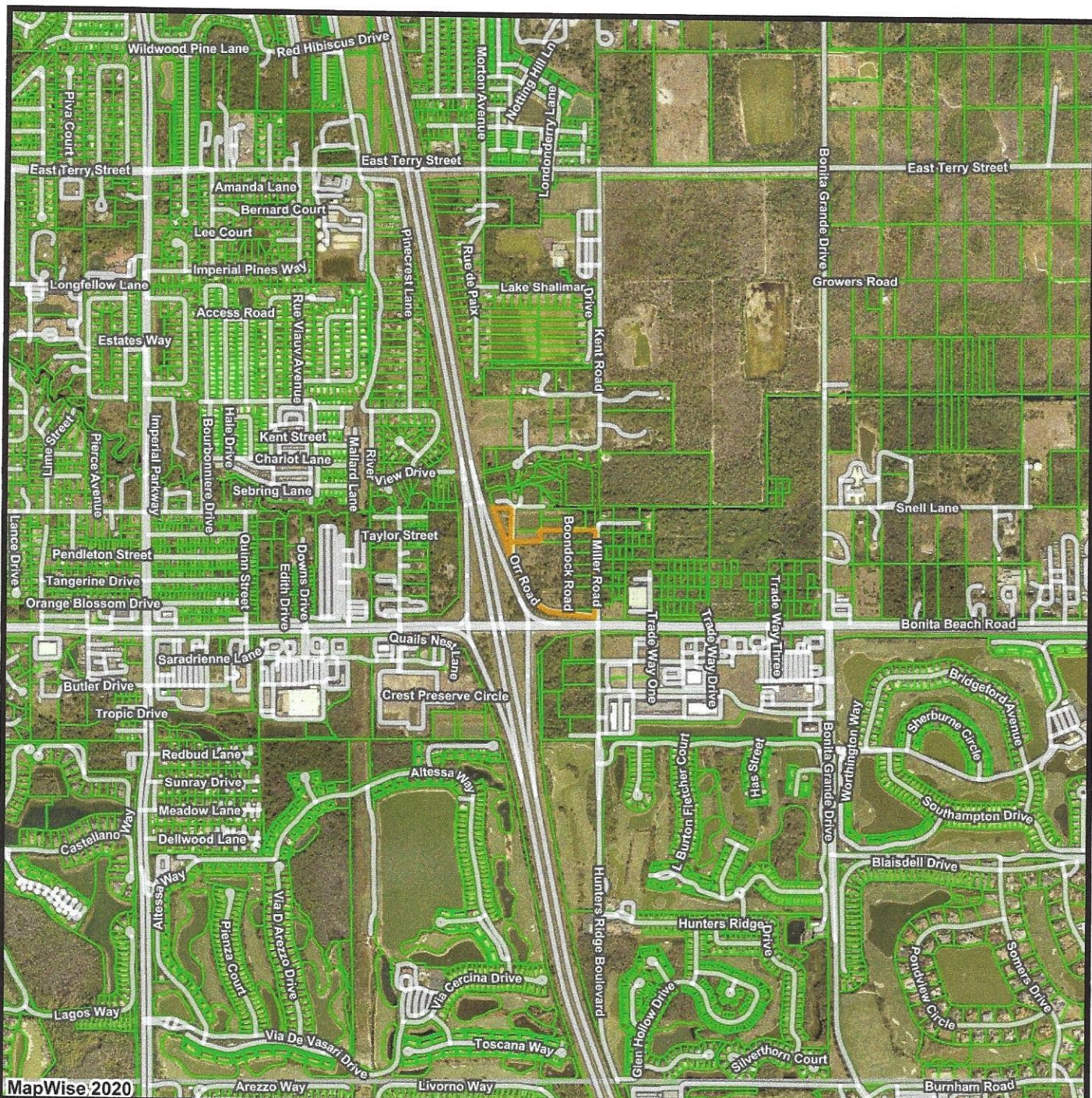
Species

Black bear	321, 411 ,414,425,427,428, 438, 612, 617, 621, 624, 630
Florida panther	211, 411 , 414,425,427,428, 434, 617, 621, 624, 630
Everglades mink	500 series 620,630, 641, 643
Big Cypress Fox squirrel	411 ,610, 620
Indigo snake	320, series 411 , 412, 414, 421, 425, 426, 427, 528
American alligator	500 series 610, 621, 630, 641, 653
Gopher tortoise	320 series 411 , 412, 421, 426, 427, 432, 743
Gopher Frog	320 series 411 , 412, 421, 426, 560, 620, 630
Southeastern American kestrel	321 411 435
Red-Cockaded woodpecker	411
Florida Scrub Jay	412, 421, 432
Wood stork	560, 610, 621, 630, 640, 650
Snail kite	520, 641, 643
Burrowing owl	191,192,310

Limpkin	500 series 617, 621, 630, 641, 643
Caracara	321 428
Little blue heron	500 series 600 series
Tricolored heron	500 series 600 series
snowy egret	500 series 600 series
Reddish egret	500 series 610, 640, 650

Vertebrates - Threatened, Endangered and Species of Special Concern

<u>Species</u>	<u>Present</u>	<u>Absent</u>
Black bear		x
Florida panther		x
Everglades mink		x
Big Cypress Fox squirrel		x
Bonneted Bat		x
Indigo snake		x
American alligator		x
Gopher tortoise		x
Gopher Frog		x
Southeastern American kestrel		x
Red-Cockaded woodpecker		x
Florida Scrub Jay		x
Wood stork		x
Snail kite		x
Bald eagle		x
Limpkin		x
Osprey		x
White ibis		x
Tricolored heron		x
Snowy egret		x
Reddish egret		x
Little blue heron		x



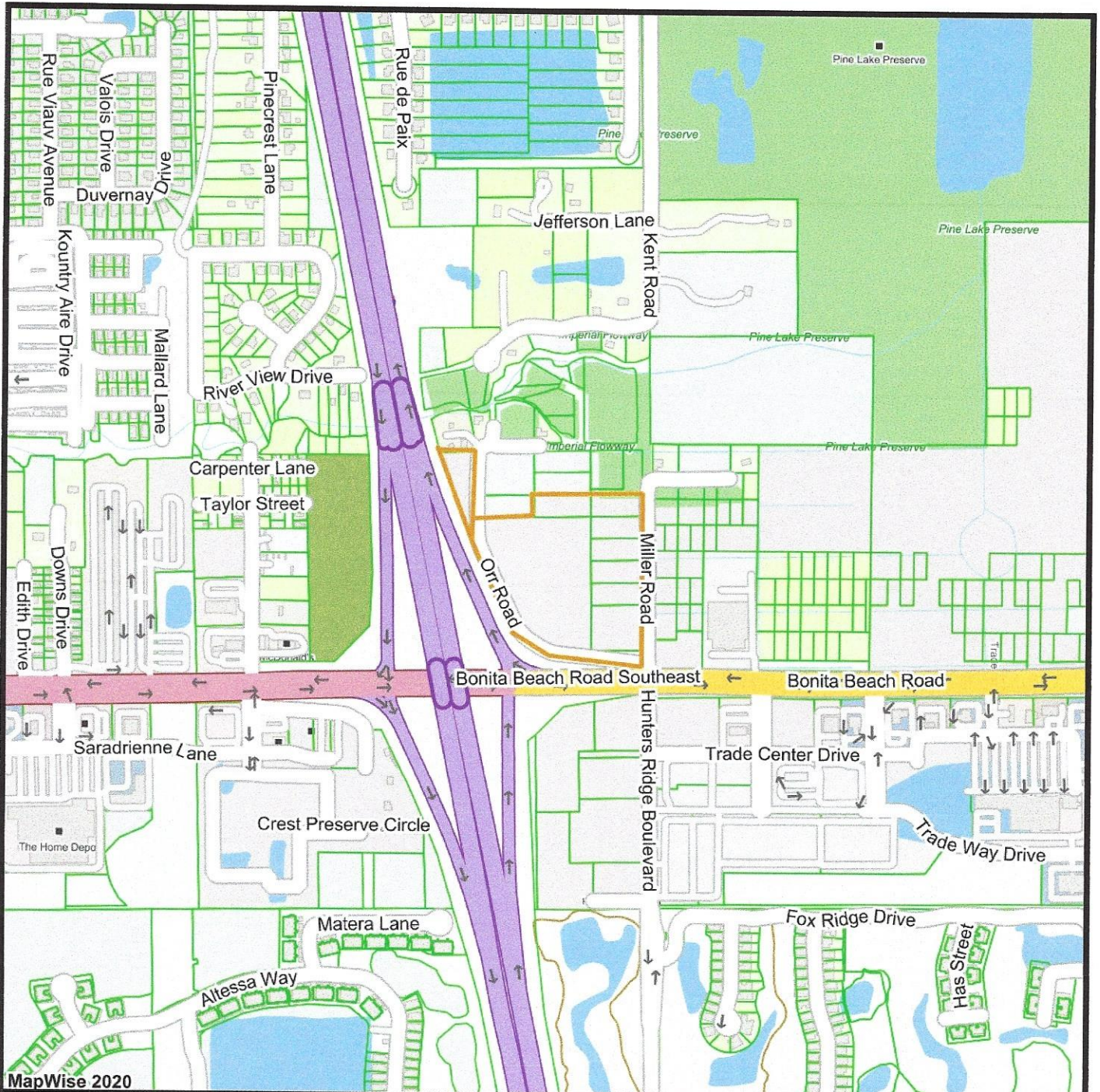
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LOCATION

- Labels Drawing
- Points Drawing
- Lines Drawing
- Polygons Drawing
- Selected Custom Parcels
- Streets OSM Hybrid
- County Boundaries
- Parcel Outlines



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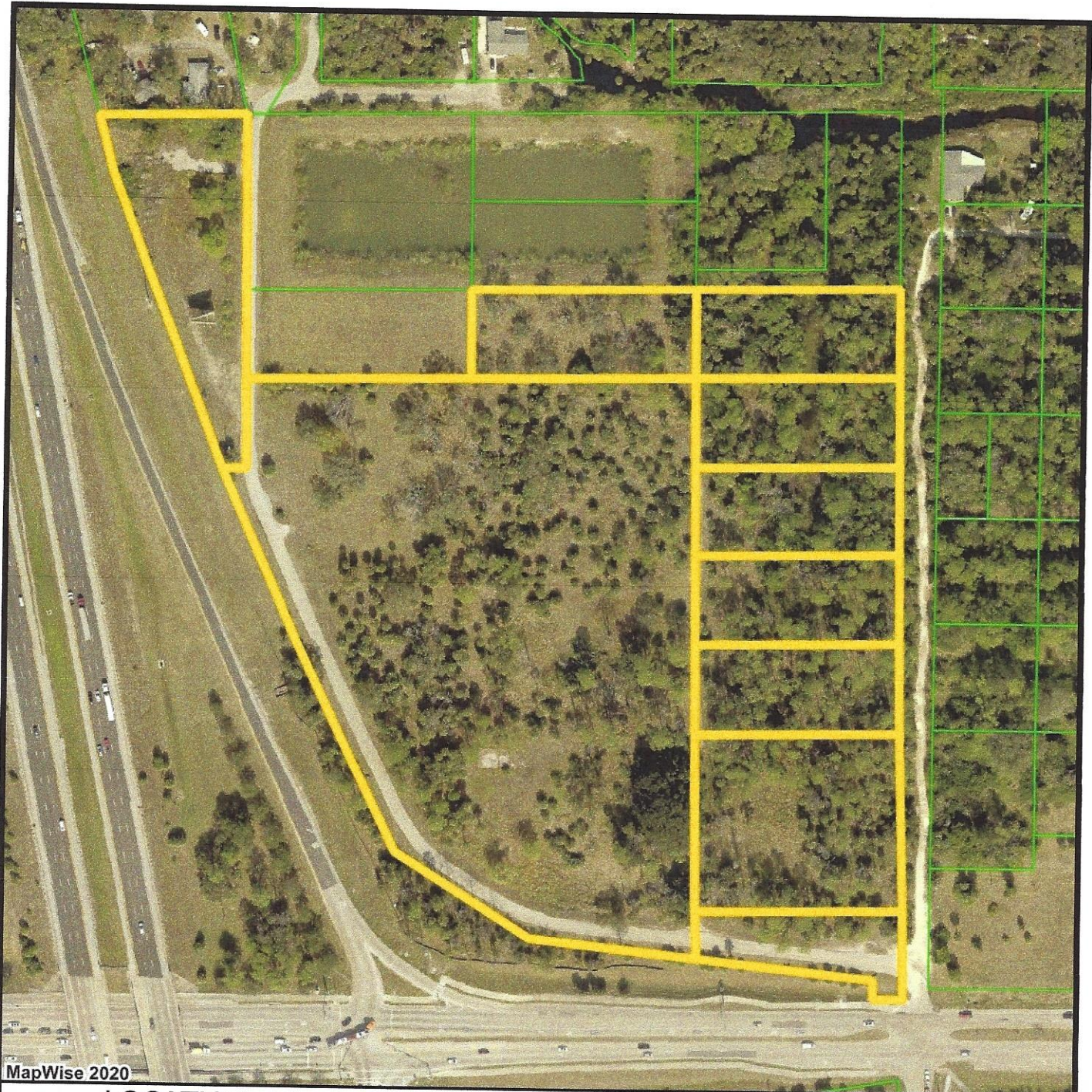
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LOCATION

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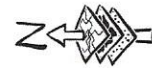
LOCATION

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FLUCCS CODE

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FLUCCS CODE AND VEGETATION INVENTORY

FLUCCS CODE	(Description)	Scientific Name	Indicator Status
Common Name			

111 Single Family Homes

These are parcels with single family homes on them. These are along the north end of the subject parcel.

500 Other Surface Water

This is areas that fill has been excavated leaving impoundments.

740 Disturbed Lands

This parcel consists of several parcels. Several of these parcels had homes with accessory structures. Several of these parcels were partially cleared. Remnants of debris are still evident in some areas. The area has some established live oaks, Slash pines with scattered cabbage palms. The ground cover is predominantly cleared. The ground cover consists of grasses, pioneer, and secondary growth species. There are a several native trees such as live oaks and Slash pine.

Live oak	<i>Quercus virginiana</i>	FACU	Cp,O
Slash pine	<i>Pinus elliotti</i>	FACW	Cp,O
Cabbage palm	<i>Sabal palmetto</i>	FAC	M,O
Ear leaf acacia	<i>Acacia auriculiformis</i>	Exotic	M,C
java plum	<i>Syzygium cumini</i>	Exotic	Cp,M,C
Brazilian pepper	<i>Schinus terebinthifolius</i>	Exotic	M,C
Guineagrass	<i>Panicum maximum</i>	FAC-	G,C
Bahia grass	<i>Paspalum notatum</i>	UPL	G,C
Boston fern	<i>Nephrolepis exaltata</i>	FAC	G,O
Sida	<i>Sida acuta</i>	UPL	G,O
Bahia grass	<i>Paspalum notatum</i>	UPL	G,C
Walters ground cherry	<i>Physalis wateri</i>	UPL	G,O
Lantana	<i>Lantana camara</i>	FACU	G,O
Sandspur	<i>Cenchrus echinatus</i>	FAC	G,C
Beggars ticks	<i>Bidens alba</i>	FACW	G,O
Beauty bush	<i>Callicarpa Americana</i>	UPL	M,O
Air potato	<i>Dioscorea bulbifera</i>	Exotic	M,O
Whitehead broom	<i>Spermacoce verticillata</i>	FAC	G,C
Chocolate weed	<i>Melochia corchorifolia</i>	FAC	G,O
Broom sedge	<i>Andropogon virginicus</i>	FAC-	G,C
Whitehead broom	<i>Spermacoce verticillata</i>	FAC	G,O
Winged Sumac	<i>Rhus copallina</i>	UPL	M,O

Strawberry guava	<i>Psidium littorale</i>	Exotic	M,O
Wedelia	<i>Wedelia trilobata</i>	Exotic	G,O
Swamp fern	<i>Blechnum serrulatum</i>	FACW+	G,C
Oyster plant	<i>Rhoeo spathacea</i>	Exotic	G,O
Dog fennel	<i>Eupatorium capillifolium</i>	FACW	G,O
Grape vine	<i>Vitis rotundifolia</i>	FAC	G,O
Virginia creeper	<i>Ampelopsis quinquefolia</i>	FAC	G,O
Cat-briar	<i>Smilax spp.</i>	FAC	G,C

814 Roadway

This is a roadway that parallels I-75 giving access to some homes to the north of the subject parcel.

ABBREVIATIONS -

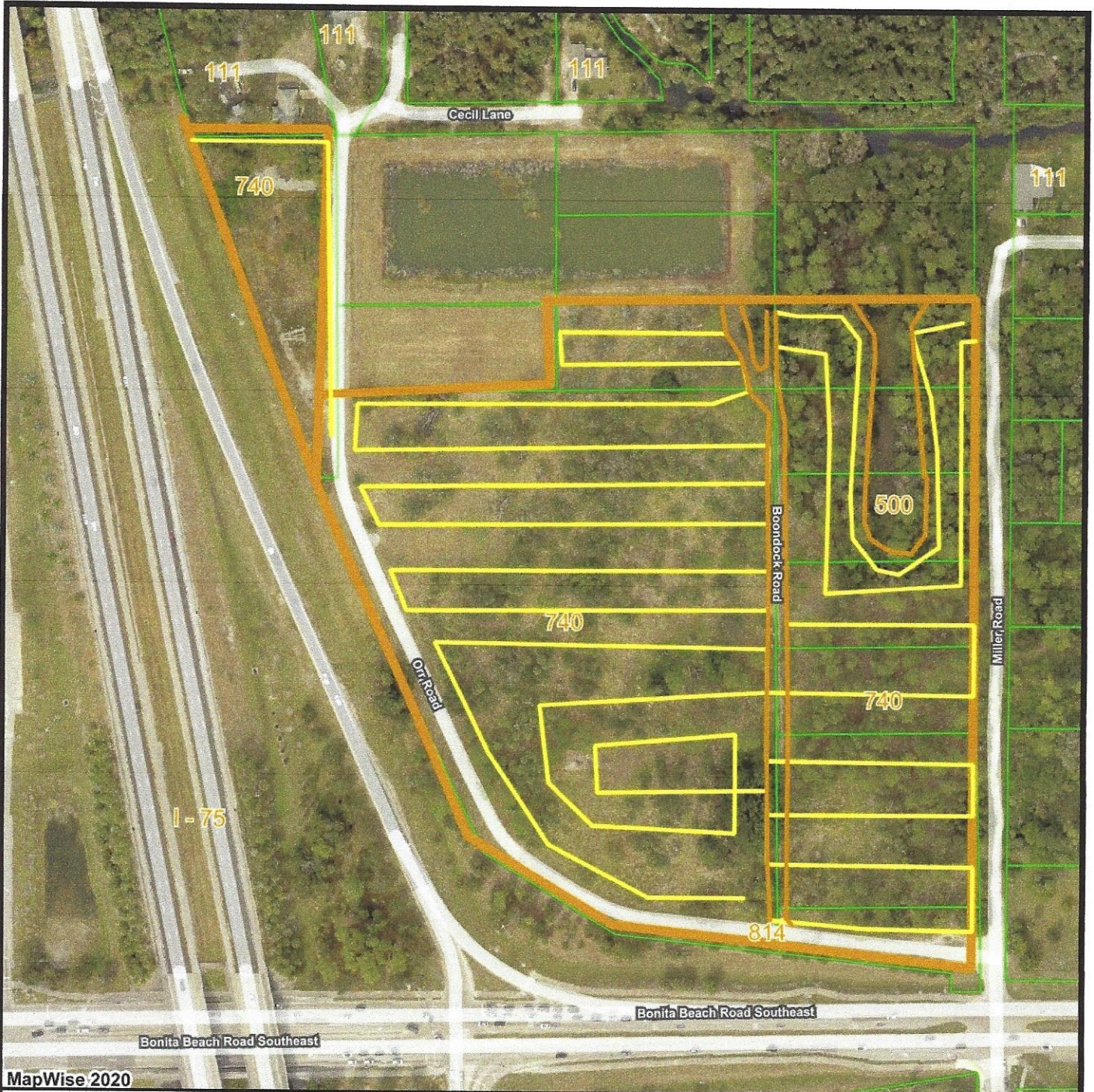
**Canopy (Cp), Midstory (M), Ground Cover (G)
Dominant (D), Common (C), Occasional (O)**

INDICATOR STATUS -

Obligate (OBL), Facultative wet plants (FACW), Facultative plants (FAC), Upland (UPL)

WILDLIFE SPECIES OBSERVED

<u>Common Name</u>	<u>Species</u>	<u>Status</u>	<u>Observation</u>
Amphibian & Reptiles:			
Brown anole	<u>Anolis sagrei</u>		Sight
Black racer	<u>Columber constrictor</u>		Sight
Birds:			
Dove- ground	<u>Columbina passerina</u>		Sight
Dove- mourning	<u>Zenaida macroura</u>		Sight
mockingbird	<u>Mimus polyglottos</u>		Sight
Vulture, turkey	<u>Cathartes aura</u>		Sight
Vulture, Black	<u>Coragyps atratus</u>		Sight
Red shoulder hawk	<u>Buteo lineatus</u>		Call/Sight
Crow, American	<u>Corvus brachyrhynchos</u>		Sight
Bluejay	<u>Cyanocitta cristata</u>		Call/Sight
Gray catbird	<u>Dumetella carolinensis</u>		Call
Cardinal	<u>Richmondia cardinalis</u>		Call
Turkey	<u>Meleagris gallopavo</u>		Sight
Pileated woodpecker	<u>Dryocopus pileatus</u>		Sight
Northern flicker	<u>Colaptes auratus</u>		Sight
Cattle egret	<u>Bubulcus ibis</u>		Sight
Mammals:			
Bobcat	<u>Felis rufus</u>		Scat /Tracks
Virginia opossum	<u>Didelphis virginiana</u>		Tracks
Raccoon	<u>Procyon lotor</u>		Tracks
Nine-banded armadillo	<u>Dasyus novemcinctus</u>		Signs
Hispid cotton rat	<u>Sigmodon hispidus</u>		Sight



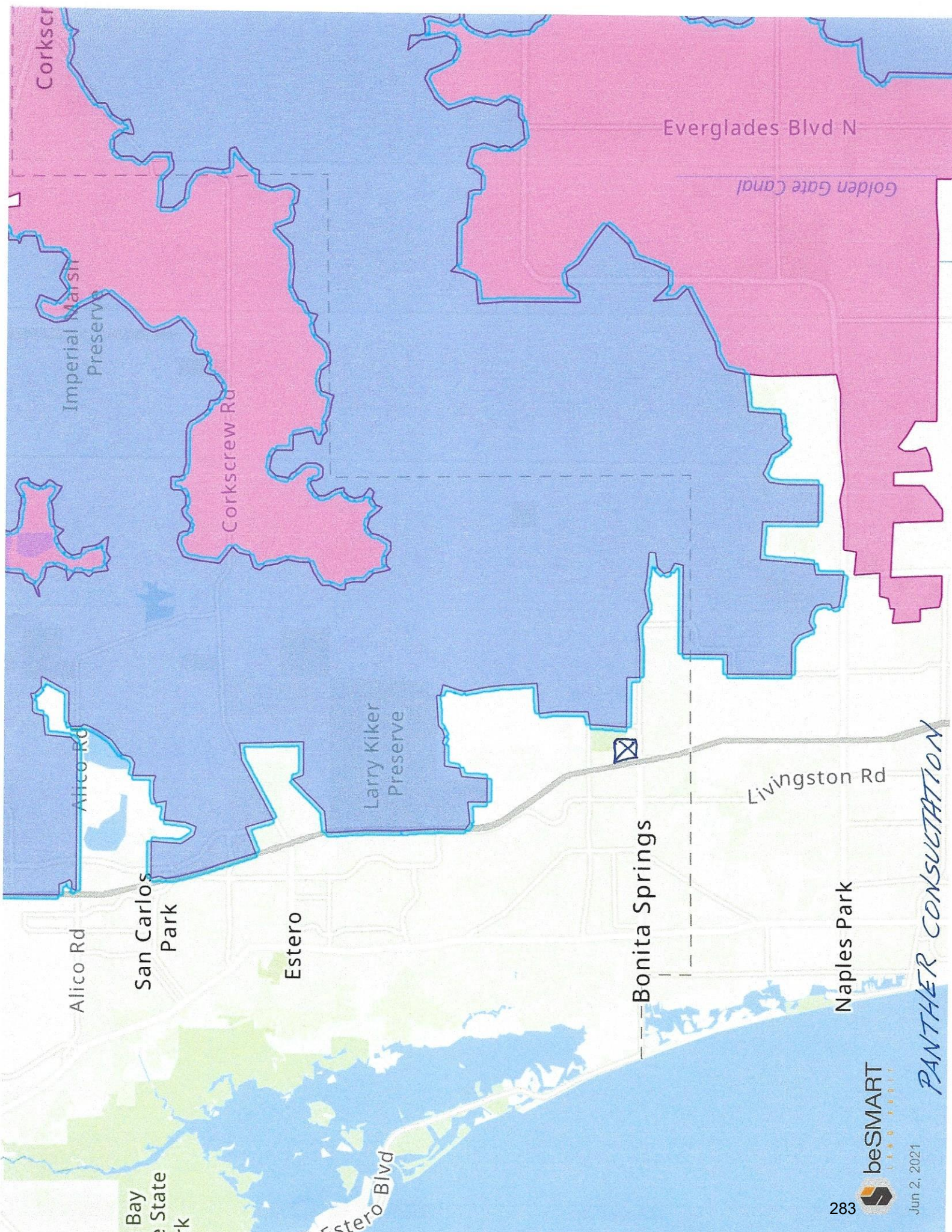
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TRANSECTS WALKED

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- Points Drawing
- Lines Drawing
- Polygons Drawing
- Selected Custom Parcels
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Bay State Park

Alico Rd

San Carlos Park

Estero

Larry Kiker Preserve

Bonita Springs

Livingston Rd

Naples Park

Everglades Blvd N

Golden Gate Canal

Corkscrew Rd

Corkscrew

Imperial Marsh Preserve

SOILS

# 6 Hallandale Fine Sand	Hydric
# 61 Orsino Fine Sand	Non -Hydric



MapWise 2020

SOILS

- Labels Drawing
- Points Drawing
- Lines Drawing
- Polygons Drawing
- Selected Custom Parcels
- County Boundaries
- Parcel Outlines
- Soils Boundaries






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HISTORICAL AERIALS



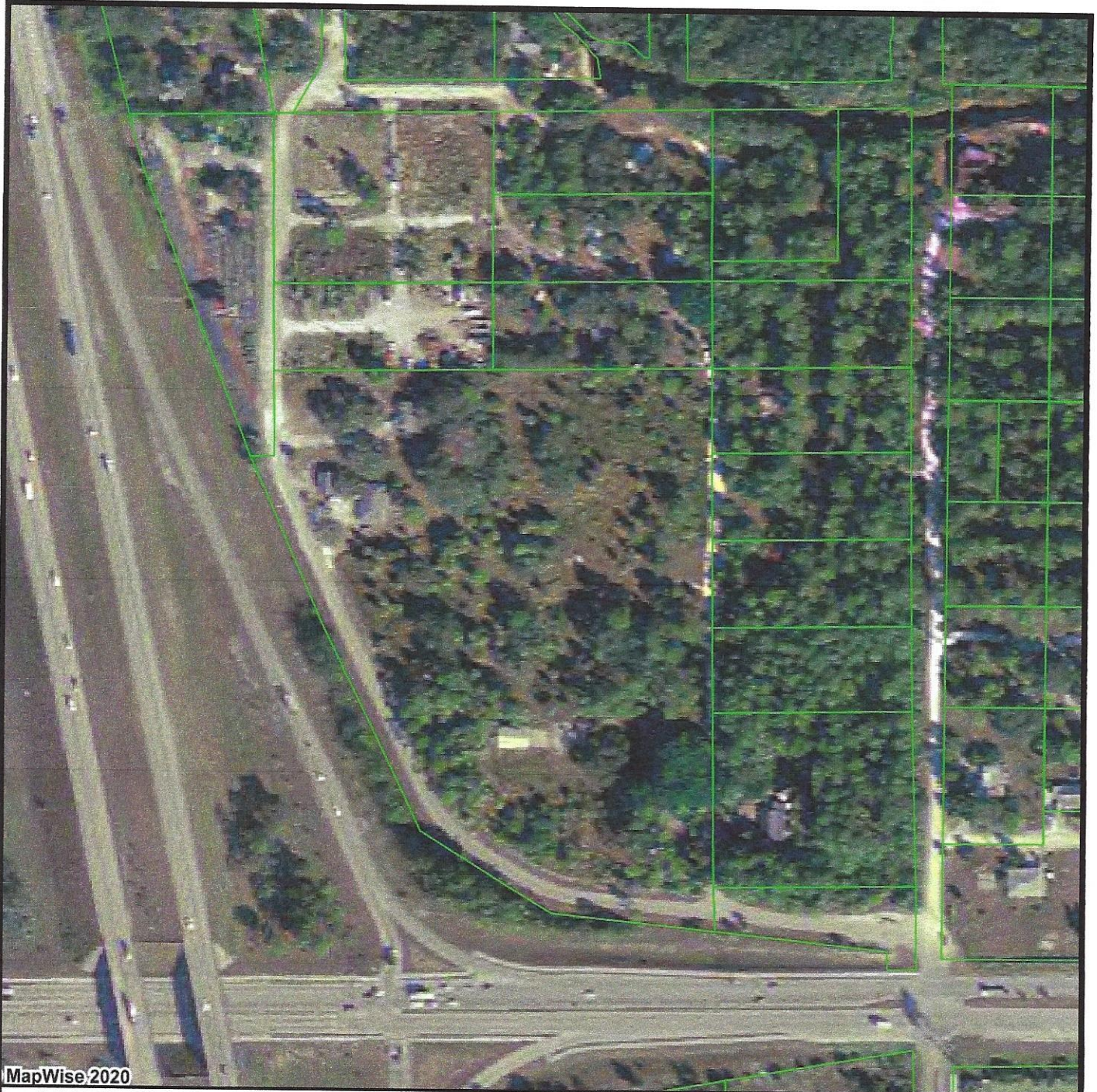
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HISTORICAL AERIAL 1995

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-  County Boundaries
-  Parcel Outlines






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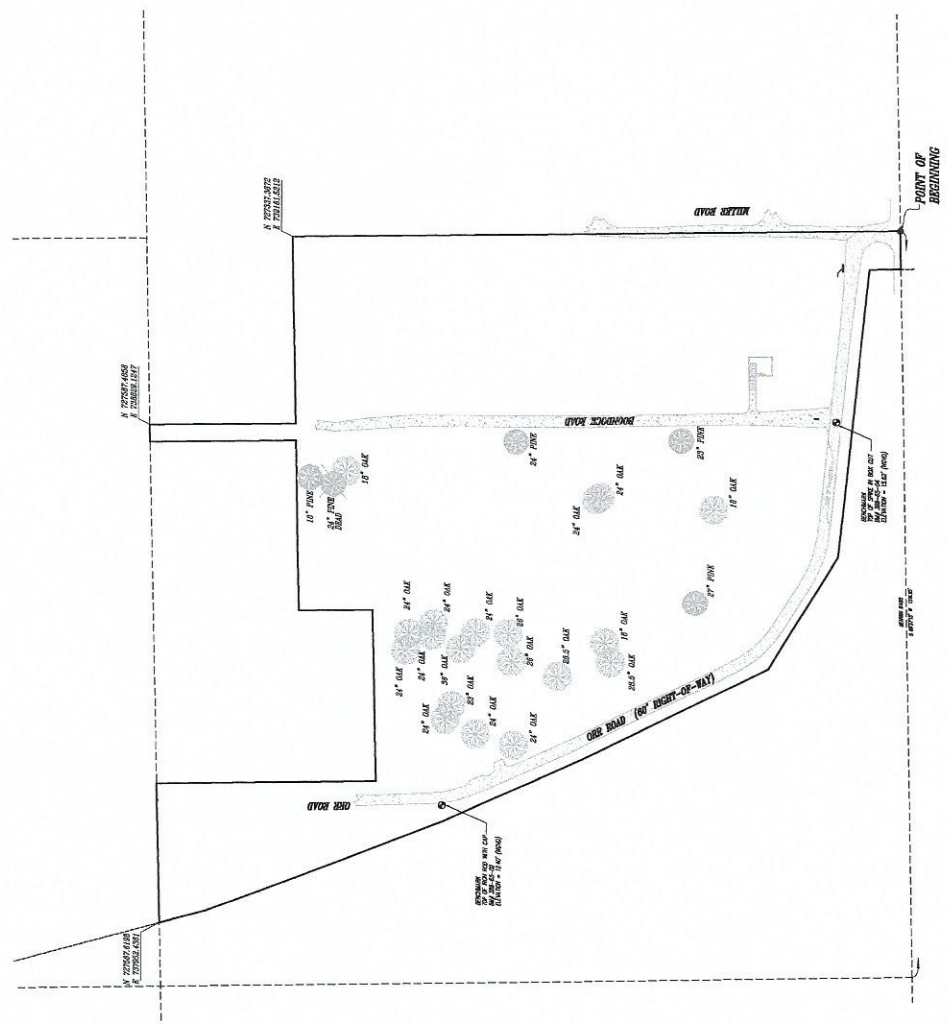
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HISTORICAL AERIAL 2004

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-  County Boundaries
-  Parcel Outlines



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FIELD BOOKS/DIAGRAMS:
 485/68, 90, 95-98
 2007 CHECK: MJI
 2008 CHECK: JJA
 2008 COLOR: RUC70
 SCALE: 1/4" = 1'-0"

Q. GRADY MINOR AND ASSOCIATES, P.A.
 CIVIL ENGINEERS • LAND SURVEYORS • PLANNERS
 3900 VIA DEL REY
 BONITA SPRINGS, FLORIDA 34134
 PHONE: (239) 947-1144 FAX: (239) 947-0376
 CERTIFICATE OF AUTHORIZATION NUMBER LB 5151

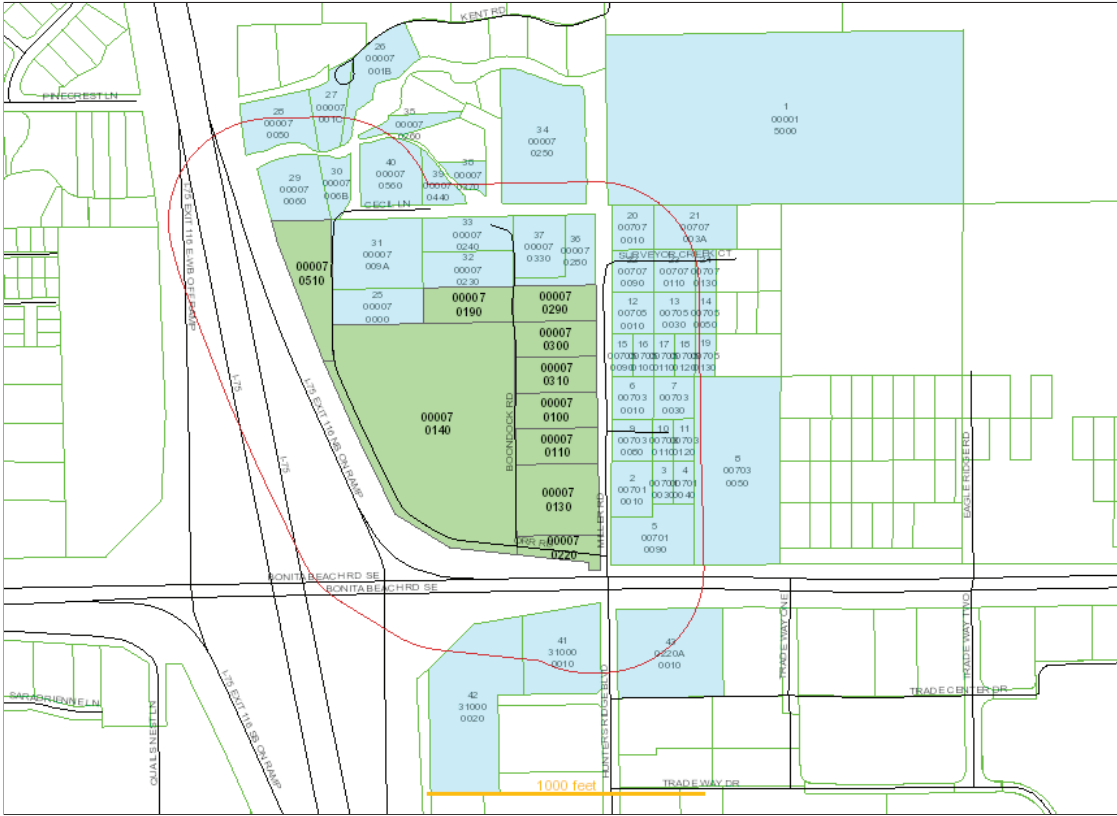
TREE LOCATION EXHIBIT
 A PORTION OF
 SECTION 31, TOWNSHIP 17 SOUTH, RANGE 26 EAST
 LEE COUNTY, FLORIDA
 DATE: APRIL 2008
 DRAWING: C-3285-1

Bonita 75 MPD

Exhibit IV-O Historical & Archaeological Map



Exhibit I-B-5 and I-B-6 Surrounding Property Owner Map and List



Date of Report: May 24, 2021
 Buffer Distance: 375 feet
 Parcels Affected: 43
 Subject Parcels: 31-47-26-B4-00007.0100, 31-47-26-B4-00007.0110, 31-47-26-B4-00007.0130, 31-47-26-B4-00007.0140, 31-47-26-B4-00007.0190, 31-47-26-B4-00007.0220, 31-47-26-B4-00007.0290, 31-47-26-B4-00007.0300, 31-47-26-B4-00007.0310, 31-47-26-B4-00007.0510

[Click here to download the map image, mailing labels \(Avery 5161\) and CSV formatted information.](#)

To change, add or remove subject parcels please change the parcel selection in [GeoView](#)

OWNER NAME AND ADDRESS	STRAP AND LOCATION	LEGAL DESCRIPTION	MAP INDEX
CITY OF BONITA SPRINGS 9101 BONITA BEACH RD BONITA SPRINGS FL 34135	31-47-26-B3-00001.5000 KENT RD BONITA SPRINGS FL 34135	S 1/2 OF NW 1/4 OF SE 1/4	1
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00701.0010 27925 MILLER RD BONITA SPRINGS FL 34135	THE N 179 FT OF S 378 FT OF E 151 FT OF W 176 FT OF SE1/4(BLK 1 LTS 1+2)	2
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00701.0030 27951 MILLER RD BONITA SPRINGS FL 34135	TH W 75.5 FT OF THE N 154 FT OF THE S 378 FT OF THE E 151 FT OF THE W 327 FT	3
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00701.0040 27955 MILLER RD BONITA SPRINGS FL 34135	THE E 75 FT OF N 154 FT OF S 378 FT OF E 151 FT OF W 327 FT OF SE 1/4	4
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00701.0090 27961 MILLER RD BONITA SPRINGS FL 34135	SUNCOAST LAKES UNREC UN 2 BLK 1 LTS9-12 OR 1944/3067 OR 1464/658 + OR 1689/4584	5
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00703.0010 27877 MILLER RD BONITA SPRINGS FL 34135	PAR IN SW 1/4 OF SE 1/4 DESC IN OR 1322 PG 1233 AKA LTS 1+2 BLK 3	6
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00703.0030 27887 MILLER RD BONITA SPRINGS FL 34135	PARL IN S W 1/4 OF S E 1/4 DESC IN OR 1373 PG 684 AKA LTS 3+4 BLK 3	7
CITY MATTRESS OF FLORIDA INC 12660 BONITA BEACH RD SE BONITA SPRINGS FL 34135	31-47-26-B3-00703.0050 12660 BONITA BEACH RD SE BONITA SPRINGS FL 34135	E 1/2 OF THE SW 1/4 OF THE SW 1/4 OF THE SE 1/4	8
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00703.0080 MILLER RD BONITA SPRINGS FL 34135	FR SW CNR OF SE1/4 RUN N 532 TO POB E 176 S 149 W 176 N149 AKA LTS 9+10 BK 3	9
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00703.0110 MILLER RD BONITA SPRINGS FL 34135	PARC IN SW 1/4 OF SE 1/4 DESC IN OR 1327 PG 1769 AKA LT 11 BK 3 SUNCOAST LK	10
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00703.0120 MILLER RD BONITA SPRINGS FL 34135	PARL IN SW 1/4 OF SE 1/4 AS DESC OR 1445 PG 2255 AKA LT 12 BK 3 SUNCOAST LK	11
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0010 27825 MILLER RD BONITA SPRINGS FL 34135	PAR IN W 1/2 OF SW 1/4 OF SE 1/4 DESC OR 1940/2656 AKA BLK 5 LOTS 1 + 2	12

2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0030 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN SW1/4 OF SE1/4 IN OR1691/4560 AKA LTS3+4 BLK 5 UT2 SUNCOAST LAKES UNR	13
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0050 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN SW 1/4 OF SE 1/4 DESC IN OR 1687 PG 3276 AKA LT 5 BLK 5 UN 2 SUNCOAST LAKES UNREC	14
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0090 27853 MILLER RD BONITA SPRINGS FL 34135	PARL IN S 1/2 OF S 1/2 OF NW 1/4 OF SW 1/4 OF SE 1/4	15
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0100 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL LOC IN THE SW 1/4 OF THE SE 1/4 AS DESC IN OR 3689 PG 2514 AKA LOT 10 BLK 5 UNIT 2 OF SUNCOAST LAKES UNREC SUBD	16
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0110 27857 MILLER RD BONITA SPRINGS FL 34135	PARL IN W 1/2 OF SW 1/4 OF S E 1/4 DESC OR 1584 PG 963 AKA LT 11 BLK 5 SUNCOAST LAKES	17
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0120 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN W 1/2 OF S W 1/4 OF S E 1/4 DESC OR 1584 PG 961 AKA LT 12 BLK 5 SUNCOAST LAKES	18
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0130 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN W 1/2 OF S W 1/4 OF S E 1/4 DESC OR 1584 PG 964 AKA LT 13 BLK 5 SUNCOAST LAKES	19
THOMPSON OLIVE ELAINE 27751 MILLER RD BONITA SPRINGS FL 34135	31-47-26-B3-00707.0010 27751 MILLER RD BONITA SPRINGS FL 34135	PARL IN W 1/2 OF SW 1/4 OF SE 1/4 AS DESC IN OR 1921 PG 2852	20
CITY OF BONITA SPRINGS 9101 BONITA BEACH RD BONITA SPRINGS FL 34135	31-47-26-B3-00707.003A ACCESS UNDETERMINED BONITA SPRINGS FL	PARL LYING IN W 1/2 OF SW 1/4 OF SE 1/4 AS DESC IN OR 467 PG 82 + OR 250 PG 911 + OR 1584 PG 961 AKA SUNCOAST LAKES UNIT II LOTS 3 THRU 6	21
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B3-00707.0090 12550 SURVEYOR CREEK CT BONITA SPRINGS FL 34135	PAR IN W 1/2 OF SW 1/4 OF SE 1/4 DESC OR 1940/2656 AKA BLK 7 LOTS 9 + 10	22
TIITF/SFWM D DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B3-00707.0110 12570 SURVEYOR CREEK CT BONITA SPRINGS FL 34135	PARL IN SW1/4 OF SE1/4 IN OR1326/224 AKA LTS11+12 BLK7 U2 SUNCOAST LAKES UNR	23
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B3-00707.0130 12590 SURVEYOR CREEK CT BONITA SPRINGS FL 34135	PARL IN W 1/2 OF S W 1/4 OF S E 1/4 DESC IN OR 1537 PG 47	24
STATE OF FL DOT PO BOX 1249 BARTOW FL 33831	31-47-26-B4-00007.0000 27801 ORR RD BONITA SPRINGS FL 34135	COM SE COR OF SW 1/4 OF SEC 31 TH AL S LI OF SD SEC 987.78 FT TH N 0 DEG	25
HASTY CULLUM L/E 27675 KENT RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.001B 27675 KENT RD BONITA SPRINGS FL 34135	PARL IN NE 1/4 OF SW 1/4 DESC IN OR 1942 PG 2238 + VAC ROW OR 2850/288	26
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.001C 27681 KENT RD BONITA SPRINGS FL 34135	PARL IN NE 1/4 OF SW 1/4 AS DESC IN OR 1956 PG 1597 + VAC ROW OR 2850/288	27
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0050 27701 KENT RD BONITA SPRINGS FL 34135	BEG 424.99 FT N OF SW COR OF NE 1/4 OF SW 1/4 TH N 77 DEG 39 MIN 35 SEC E ETC AS DESC OR 1181/1433	28
TAYLOR MARIAN B TR 27704 ORR RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.0060 27710/720 ORR RD BONITA SPRINGS FL 34135	BEG SW COR OF NE 1/4 OF SW 1/4 TH E 300 FT TH N 60 FT TH E 18.35 FT TH NWLY ALG	29
TAYLOR MARIAN B TR 27704 ORR RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.006B 27704 ORR RD BONITA SPRINGS FL 34135	FR SW COR OF NE 1/4 OF SW 1/4 RUN E 299.77 FT TH N 60 FT TH E 85 FT TO POB TH	30
STATE OF FL DOT PO BOX 1249 BARTOW FL 33831	31-47-26-B4-00007.009A 27741 ORR RD BONITA SPRINGS FL 34135	A PARL OF LAND IN SW 1/4 OF SW 1/4 AS DESC IN OR 92 PG 26	31
STATE OF FL DOT PO BOX 1249 BARTOW FL 33831	31-47-26-B4-00007.0230 27776 BOONDOCK RD BONITA SPRINGS FL 34135	BEG 329.26 FT W + 1065 FT N OF SE COR OF SW 1/4 TH W 329.72 FT N 130 FT E	32
STATE OF FL DOT PO BOX 1249 BARTOW FL 33831	31-47-26-B4-00007.0240 27750 BOONDOCK RD BONITA SPRINGS FL 34135	BEG 329.26 FT W + 1195 FT N OF SE COR OF SW 1/4 TH W 329.78 FT N 124.61 FT E	33
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0250 ACCESS UNDETERMINED BONITA SPRINGS FL	A PARL OF LAND IN NE 1/4 OF SW 1/4 AS DESC IN OR 166 PG 390	34
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION	31-47-26-B4-00007.0260 ACCESS UNDETERMINED	A PARL OF LAND IN NE 1/4 OF SW 1/4 AS DESC IN OR	35

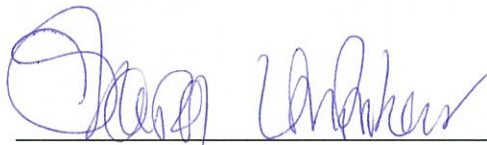
3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	BONITA SPRINGS FL	266 PG 390	
THITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0280 27775 BOONDOCK RD BONITA SPRINGS FL 34135	A PARL OF LAND IN SW 1/4 OF SW 1/4 AS DESC IN OR 404 PG 105	36
THITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0330 27751 BOONDOCK RD BONITA SPRINGS FL 34135	A PARL OF LAND IN SW 1/4 OF SW 1/4 AS DESC IN OR 404 PG 105	37
THITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0370 ACCESS UNDETERMINED BONITA SPRINGS FL	BEG SE COR OF NE 1/4 OF SW 1/4 TH W 419.56 FT TH N 60 FT TO POB TH N 125 FT	38
THOMPSON MARK D + 25251 PAPILLION DR BONITA SPRINGS FL 34135	31-47-26-B4-00007.0440 12385 CECIL LN BONITA SPRINGS FL 34135	A PARL OF LAND IN NE 1/4 OF SW 1/4 AS DESC IN OR 461 PG 858	39
THITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0560 12355 CECIL LN BONITA SPRINGS FL 34135	FR SE COR OF NE 1/4 OF SW 1/4 RUN S 88 DEG 41 MIN 50 SEC W 874.11 FT TH N 1 DEG	40
HORIZON PARK INVESTMENTS LLC 23190 FASHION DR STE 202 ESTERO FL 33928	06-48-26-B1-31000.0010 28051 HUNTERS RIDGE BLVD BONITA SPRINGS FL 34135	HORIZON PARK LOT 1 AS DESC IN INST# 2020000293053	41
NAPLES COMMUNITY HOSPITAL INC 350 7TH ST N NAPLES FL 34102	06-48-26-B1-31000.0020 12570-581 TRADE CENTER DR BONITA SPRINGS FL 34135	HORIZON PARK LOT 2 AS DESC IN INST# 2020000293053	42
BERNWOOD LAND HOLDINGS LLC 1110 EUCLID AVE STE 300 CLEVELAND OH 44115	06-48-26-B2-0220A.0010 12611 BONITA BEACH RD SE BONITA SPRINGS FL 34135	BERNWOOD PARK OF COMMERCE UT 3 PB 78 PGS 36 + 37 BLK A LOT 1	43

AFFIDAVIT OF COMPLIANCE

Bonita 75 Commercial Planned Development (CPD) Amendment

I hereby certify that pursuant to Ordinance No. 18-15, of the Bonita Springs Land Development Code, I did cause the attached newspaper advertisement to appear and I did give notice by mail to the property owners and/or condominium, home owner and civic associations within 1,000 feet from the subject property whose members may be affected by the proposed land use changes of an application request for a rezoning, PD amendment, variance or special exception, at least 21 days prior to the scheduled Neighborhood Information Meeting. *For the purposes of this requirement, the names and addresses of property owners shall be deemed those appearing on the latest tax rolls of Lee County.* The said notice contained the laymen's description of the site property of proposed change and the date, time, and place of a Neighborhood Information Meeting.

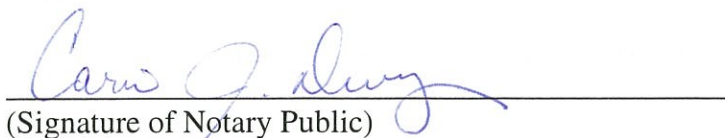
Per the attached letter, property owner's list, and copy of newspaper advertisement which are hereby made a part of this Affidavit of Compliance.



(Sharon Umpenhour as Senior Planning Technician for Q. Grady Minor & Associates, P.A.)

State of Florida
County of Lee

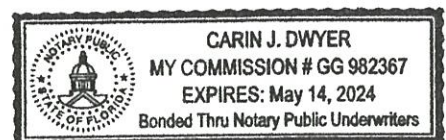
The foregoing Affidavit of compliance was acknowledged before me this 4th day of June 2021 by Sharon Umpenhour as Senior Planning Technician for Q. Grady Minor & Associates, P.A., who is personally known to me or who has produced _____ as identification.



(Signature of Notary Public)

Carin J. Dwyer
Printed Name of Notary

(Notary Seal)



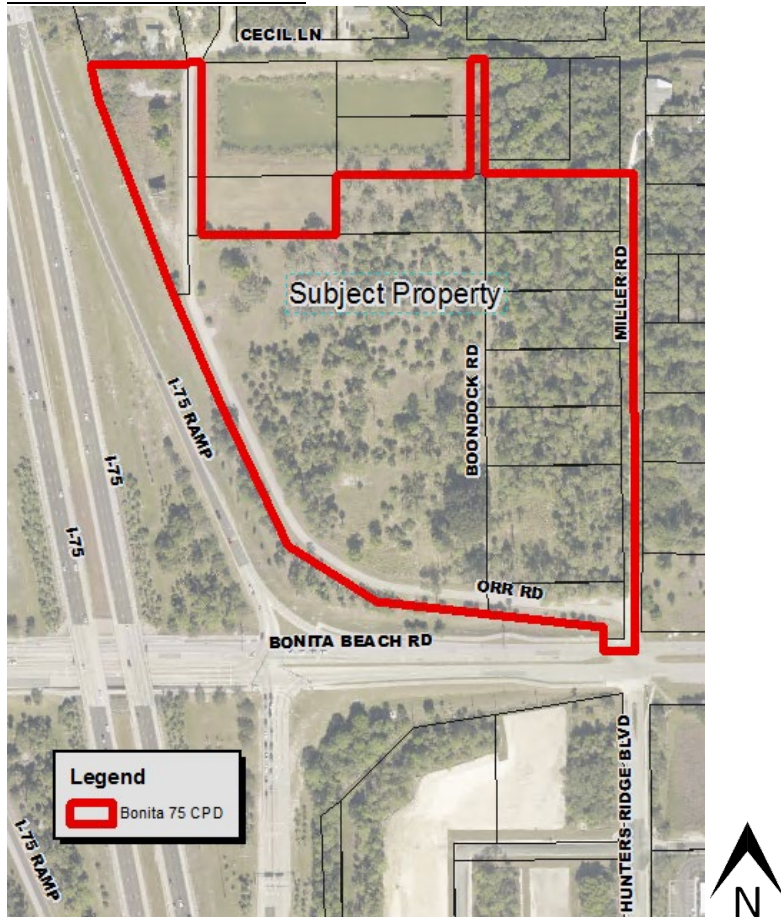
NEIGHBORHOOD INFORMATION MEETING

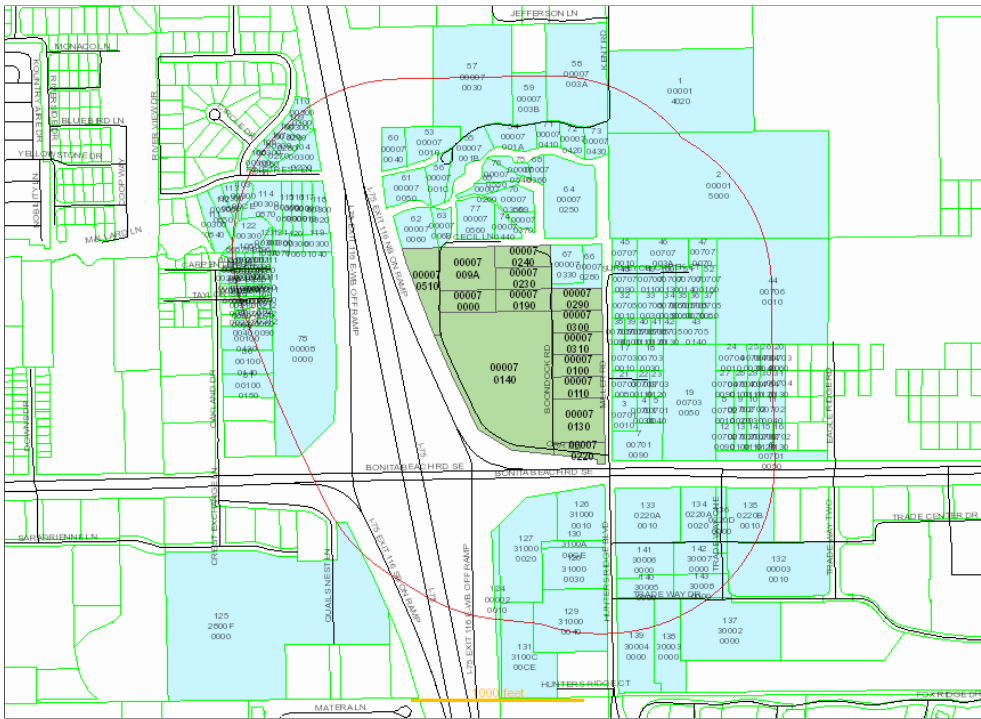
A neighborhood information meeting pursuant to Section 4-28, Bonita Springs Land Development Code will be held Thursday, June 28, 2021, 5:30 p.m. at Bonita Springs Fire Department, 27701 Bonita Grande Drive, Bonita Springs, FL 34135.

The neighborhood information meeting will provide general information regarding a proposed Planned Development (PD) amendment application for a project known as the Bonita 75 CPD. The applicant, Madison Capital Group Management, LLC, is requesting an amendment to the Bonita 75 CPD to reduce the commercial square footage and add multi-family residential as a use. The applicant is requesting bonus density units.

The subject property is located on the northeast quadrant of Bonita Beach Road and Interstate 75 (I-75) and is comprised of approximately 21± acres in Section 31, Township 47 South, Range 26 East, Bonita Springs, Florida. If you have questions or comments, please contact Sharon Umpenhour, Senior Planning Technician with Q. Grady Minor & Associates, P.A. by email: sumpenhour@gradyminor.com, phone: 239-947-1144, or mail: 3800 Via Del Rey, Bonita Springs, FL 34134. For project information or to participate remotely, please visit our website at gradyminor.com/planning. The Neighborhood Information Meeting is for informational purposes only, it is not a public hearing.

PROJECT LOCATION MAP





Date of Report: April 09, 2021
 Buffer Distance: 1000 feet

[Click here to download the map image, mailing labels \(Avery 5161\) and CSV formatted information.](#)

Parcels Affected: 143

Subject Parcels:

31-47-26-B4-0007.0000, 31-47-26-B4-0007.009A, 31-47-26-B4-0007.0100, 31-47-26-B4-0007.0110, 31-47-26-B4-0007.0130, 31-47-26-B4-0007.0140, 31-47-26-B4-0007.0190, 31-47-26-B4-0007.0220, 31-47-26-B4-0007.0230, 31-47-26-B4-0007.0240, 31-47-26-B4-0007.0290, 31-47-26-B4-0007.0300, 31-47-26-B4-0007.0310, 31-47-26-B4-0007.0510

To change, add or remove subject parcels please change the parcel selection in [GeoView](#)

OWNER NAME AND ADDRESS	STRAP AND LOCATION	LEGAL DESCRIPTION	MAP INDEX
CITY OF BONITA SPRINGS 9101 BONITA BEACH RD BONITA SPRINGS FL 34135	31-47-26-B3-00001.4020 27601 KENT RD BONITA SPRINGS FL 34135	S 3/4 OF NW 1/4 OF NW 1/4 OF SE 1/4 + W 1/3 OF S 3/4 OF NE 1/4 OF NW 1/4 OF SE 1/4	1
CITY OF BONITA SPRINGS 9101 BONITA BEACH RD BONITA SPRINGS FL 34135	31-47-26-B3-00001.5000 KENT RD BONITA SPRINGS FL 34135	S 1/2 OF NW 1/4 OF SE 1/4	2
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00701.0010 27925 MILLER RD BONITA SPRINGS FL 34135	THE N 179 FT OF S 378 FT OF E 151 FT OF W 176 FT OF SE1/4(BLK 1 LTS 1+2)	3
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00701.0030 27951 MILLER RD BONITA SPRINGS FL 34135	TH W 75.5 FT OF THE N 154 FT OF THE S 378 FT OF THE E 151 FT OF THE W 327 FT	4
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00701.0040 27955 MILLER RD BONITA SPRINGS FL 34135	THE E 75 FT OF N 154 FT OF S 378 FT OF E 151 FT OF W 327 FT OF SE 1/4	5
THOMPSON ELAINE 27751 MILLER RD BONITA SPRINGS FL 34135	31-47-26-B3-00701.0050 ACCESS UNDETERMINED BONITA SPRINGS FL	SW 1/4 OF SE 1/4 LESS E 1/2 + PARCEL 1006.001 THRU 1006.0350+ 1-75 R/W LESS OR 3689 PG 2514	6
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00701.0090 27961 MILLER RD BONITA SPRINGS FL 34135	SUNCOAST LAKES UNREC UN 2 BLK 1 LTS9-12 OR 1944/3067 OR 1464/658 + OR 1689/4584	7
THOMPSON OLIVE ELAINE 27751 MILLER RD BONITA SPRINGS FL 34135	31-47-26-B3-00702.0010 ACCESS UNDETERMINED BONITA SPRINGS FL	A PARL OF LAND IN SE 1/4 OF SW 1/4 OF SE 1/4 AS DESC IN OR 825 PG 625	8
CONKLIN OLIVE ELAINE OLIVE ELAINE THOMPSON 27751 MILLER RD BONITA SPRINGS FL 34135	31-47-26-B3-00702.0020 ACCESS UNDETERMINED BONITA SPRINGS FL	FR SE COR TH W ALG S LI 1320.08 FT TH N 0 DEG 12 MIN 34 SEC E 224 FT TH S	9
BRIDGES T M & ALICE L 1911 SW 84TH AVE DAVIE FL 33324	31-47-26-B3-00702.0030 ACCESS UNDETERMINED BONITA SPRINGS FL	FR SE COR TH W ALG S LI 1320.08 FT TH N 0 DEG 12 MIN 34 SEC E 224 FT TH S	10
GOLDER W W + THELMA VICTORIA ECKMAN 129 WELLINGTON DR PERRY GA 31069	31-47-26-B3-00702.0040 ACCESS UNDETERMINED BONITA SPRINGS FL	FROM SE COR RUN W ALG S LI SEC 31 1320.08 FT TH N 224 FT TH W 251.5 FT FOR P2B	11
THOMPSON ELAINE 27751 MILLER RD BONITA SPRINGS FL 34135	31-47-26-B3-00702.0090 ACCESS UNDETERMINED BONITA SPRINGS FL	A PARL OF LAND IN SE 1/4 OF SW 1/4 OF SE 1/4 AS DESC IN OR 825 PG 626	12
KIERNAN GENEVIEVE C 3301 TOPPING RD MADISON WI 53705	31-47-26-B3-00702.0100 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN SE 1/4 OF SW 1/4 OF SE 1/4 S 31 T 47 R 26 DESC IN OR 1196 PG 1550	13
ANDRE EDWARD A 3800 N RIVERSIDE DR INDIALANTIC FL 32903	31-47-26-B3-00702.0110 ACCESS UNDETERMINED BONITA SPRINGS FL	FR SE COR TH W ALG S LI 1320.08 FT TH N 0 DEG 12 MIN 34 SEC E 224 FT TH S	14
MITCHELL REBECCA R 3524 RIPPLEGROVE DR	31-47-26-B3-00702.0120 ACCESS UNDETERMINED	PARL IN SW 1/4 OF SE 1/4 DESC IN OR 1792 PG 0359	15

CINCINNATI OH 45251	BONITA SPRINGS FL	LOT 12 BLK 2	
MITCHELL REBECCA 3524 RIPPLE GROVE DR CINCINNATI OH 45251	31-47-26-B3-00702.0130 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN SW 1/4 OF SE 1/4 DESC IN OR 1792 PG 0361 LOT 13 BLK 2	16
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00703.0010 27877 MILLER RD BONITA SPRINGS FL 34135	PAR IN SW 1/4 OF SE 1/4 DESC IN OR 1322 PG 1233 AKA LTS 1+2 BLK 3	17
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00703.0030 27887 MILLER RD BONITA SPRINGS FL 34135	PARL IN S W 1/4 OF S E 1/4 DESC IN OR 1373 PG 684 AKA LTS 3+4 BLK 3	18
CITY MATTRESS OF FLORIDA INC 12660 BONITA BEACH RD SE BONITA SPRINGS FL 34135	31-47-26-B3-00703.0050 12660 BONITA BEACH RD SE BONITA SPRINGS FL 34135	E 1/2 OF THE SW 1/4 OF THE SW 1/4 OF THE SE 1/4	19
HASLOUER DARLENE SUE 1552 SW CHEYENNE HILLS RD TOPEKA KS 66604	31-47-26-B3-00703.0060 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN S W 1/4 OF S E 1/4 DESC IN OR 1364 PG 1240	20
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00703.0080 MILLER RD BONITA SPRINGS FL 34135	FR SW CNR OF SE1/4 RUN N 532 TO POB E 176 S 149 W 176 N149 AKA LTS 9+10 BK 3	21
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00703.0110 MILLER RD BONITA SPRINGS FL 34135	PARC IN SW 1/4 OF SE 1/4 DESC IN OR 1327 PG 1769 AKA LT 11 BK 3 SUNCOAST LK	22
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00703.0120 MILLER RD BONITA SPRINGS FL 34135	PARL IN SW 1/4 OF SE 1/4 AS DESC OR 1445 PG 2255 AKA LT 12 BK 3 SUNCOAST LK	23
LOPEZ RENE J & 12407 LONDONDERRY LN BONITA SPRINGS FL 34135	31-47-26-B3-00704.0010 ACCESS UNDETERMINED BONITA SPRINGS FL	BEG SE COR SEC 31 TH S 89 DEG 48 MIN 15 SEC W 1320.08 FT TH N 0 DEG 12	24
MS HOME SOUTHWEST FLORIDA INC 3372 WOODS EDGE CIR # 103 BONITA SPRINGS FL 34134	31-47-26-B3-00704.0030 ACCESS UNDETERMINED BONITA SPRINGS FL	FR SE COR OF SEC RUN S 89 DEG 48 MIN W 1320.08 FT TH N 0 DEG 12 MIN E 532.0 FT	25
EMFIELD ROBERT M TR 27420 HICKORY BLVD BONITA SPRINGS FL 34134	31-47-26-B3-00704.0040 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN S W 1/4 OF S E 1/4 AS DESC IN OR 1392 PG 344 A/K/A LOT 4 BLK 4	26
REARDON ALLISON N 28507 LA PLUMA WAY BONITA SPRINGS FL 34135	31-47-26-B3-00704.0090 ACCESS UNDETERMINED BONITA SPRINGS FL	A PARL OF LAND IN SE 1/4 OF SW 1/4 OF SE 1/4 AS DESC IN OR 462 PG 777	27
REARDON ALLISON N 28507 LA PLUMA WAY BONITA SPRINGS FL 34135	31-47-26-B3-00704.0100 ACCESS UNDETERMINED BONITA SPRINGS FL	A PARL OF LAND IN SE 1/4 OF SW 1/4 OF SE 1/4 AS DESC IN OR 462 PG 777	28
BURCH JAMES N 6 MAUI CIR NAPLES FL 34112	31-47-26-B3-00704.0110 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN S W 1/4 OF S E 1/4 AS DESC IN OR 1392 PG 344 A/K/A LOT 11 BLK 4	29
BURCH JAMES N 6 MAUI CIR NAPLES FL 34112	31-47-26-B3-00704.0120 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN S W 1/4 OF S E 1/4 AS DESC IN OR 1392 PG 344 A/K/A LOT 12 BLK 4	30
BURCH JAMES N 6 MAUI CIR NAPLES FL 34112	31-47-26-B3-00704.0130 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN S W 1/4 OF S E 1/4 AS DESC IN OR 1392 PG 344 A/K/A LOT 13 BLK 4	31
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0010 27825 MILLER RD BONITA SPRINGS FL 34135	PAR IN W 1/2 OF SW 1/4 OF SE 1/4 DESC OR 1940/2656 AKA BLK 5 LOTS 1 + 2	32
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0030 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN SW1/4 OF SE1/4 IN OR1691/4560 AKA LTS3+4 BLK 5 UT2 SUNCOAST LAKES UNR	33
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0050 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN SW 1/4 OF SE 1/4 DESC IN OR 1687 PG 3276 AKA LT 5 BLK 5 UN 2 SUNCOAST LAKES UNREC	34
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0060 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN S W 1/4 OF S E 1/4 DESC IN OR 1354 PG 1264 AKA LT 6 BLK 5 UN 2 SUNCOAST LAKES UNREC	35
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0070 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN W 1/2 OF S W 1/4 OF S E 1/4 DESC IN OR 1537 PG 47	36
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0080 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN SW 1/4 OF SE 1/4 SEC 31 TWP 47 RGE 26 DESC IN OR 1403 PG 1431	37
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0090 27853 MILLER RD BONITA SPRINGS FL 34135	PARL IN S 1/2 OF S 1/2 OF NW 1/4 OF SW 1/4 OF SE 1/4	38
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0100 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL LOC IN THE SW 1/4 OF THE SE 1/4 AS DESC IN OR 3689 PG 2514 AKA LOT 10 BLK 5 UNIT 2 OF SUNCOAST LAKES UNREC SUBD	39
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0110 27857 MILLER RD BONITA SPRINGS FL 34135	PARL IN W 1/2 OF SW 1/4 OF S E 1/4 DESC OR 1584 PG 963 AKA LT 11 BLK 5 SUNCOAST LAKES	40
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0120 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN W 1/2 OF S W 1/4 OF S E 1/4 DESC OR 1584 PG 961 AKA LT 12 BLK 5 SUNCOAST LAKES	41
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0130 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN W 1/2 OF S W 1/4 OF S E 1/4 DESC OR 1584 PG 964 AKA LT 13 BLK 5 SUNCOAST LAKES	42
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0140 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN W 1/2 OF S W 1/4 OF S E 1/4 DESC IN OR 1537 PG 47	43

LYNX ZUCKERMAN AT BONITA GRAND 6131 LYONS RD STE 200 COCONUT CREEK FL 33073	31-47-26-B3-00706.0010 ACCESS UNDETERMINED BONITA SPRINGS FL	N 1/2 OF E 1/2 OF SW 1/4 OF SE 1/4	44
THOMPSON OLIVE ELAINE 27751 MILLER RD BONITA SPRINGS FL 34135	31-47-26-B3-00707.0010 27751 MILLER RD BONITA SPRINGS FL 34135	PARL IN W 1/2 OF SW 1/4 OF SE 1/4 AS DESC IN OR 1921 PG 2852	45
CITY OF BONITA SPRINGS 9101 BONITA BEACH RD BONITA SPRINGS FL 34135	31-47-26-B3-00707.003A ACCESS UNDETERMINED BONITA SPRINGS FL	PARL LYING IN W 1/2 OF SW 1/4 OF SE 1/4 AS DESC IN OR 467 PG 82 + OR 250 PG 911 + OR 1584 PG 961 AKA SUNCOAST LAKES UNIT II LOTS 3 THRU 6	46
BACHMAN JOSHUA R 12611 SURVEYOR CREEK CT BONITA SPRINGS FL 34135	31-47-26-B3-00707.0070 12611 SURVEYOR CREEK CT BONITA SPRINGS FL 34135	PARL IN NW 1/4 OF SW 1/4 OF SE 1/4 AS DESC IN OR 1925 PG 1432	47
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B3-00707.0090 12550 SURVEYOR CREEK CT BONITA SPRINGS FL 34135	PAR IN W 1/2 OF SW 1/4 OF SE 1/4 DESC OR 1940/2656 AKA BLK 7 LOTS 9 + 10	48
TIITF/SFWMD DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B3-00707.0110 12570 SURVEYOR CREEK CT BONITA SPRINGS FL 34135	PARL IN SW 1/4 OF SE 1/4 IN OR 1326/224 AKA LTS 11+12 BLK 7 U2 SUNCOAST LAKES UNR	49
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B3-00707.0130 12590 SURVEYOR CREEK CT BONITA SPRINGS FL 34135	PARL IN W 1/2 OF S W 1/4 OF S E 1/4 DESC IN OR 1537 PG 47	50
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B3-00707.0140 12610 SURVEYOR CREEK CT BONITA SPRINGS FL 34135	PARL IN S W 1/4 OF S E 1/4 DESC IN OR 1377 PG 1870	51
WBG SW FLORIDA INC 27800 OLD 41 RD BONITA SPRINGS FL 34135	31-47-26-B3-00707.0160 12630 SURVEYOR CREEK CT BONITA SPRINGS FL 34135	PARL IN NW 1/4 OF SW 1/4 OF SE 1/4 DESC OR 1894 PG 3722 UNREC PLAT BLK 7 LT 16	52
STRADER CHARLES EARL 27655 KENT RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.0010 27680 KENT RD BONITA SPRINGS FL 34135	PAR IN NE 1/4 OF SW 1/4 DESC IN OR 1552 PG 1678 + VAC ROW OR 2850/288 LESS PART S OF RD	53
CITY OF BONITA SPRINGS 9101 BONITA BEACH RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.001A 27671 KENT RD BONITA SPRINGS FL 34135	PARL IN NE 1/4 OF SW 1/4 DESC IN OR 1942 PG 2234 + COMM 689 FT N OF SE COR NE 1/4 OF SW 1/4 TH GO W ALG S LI OF R/W EASE TO PT ON + VAC ROW OR 2850/288	54
HASTY CULLUM L/E 27675 KENT RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.001B 27675 KENT RD BONITA SPRINGS FL 34135	PARL IN NE 1/4 OF SW 1/4 DESC IN OR 1942 PG 2238 + VAC ROW OR 2850/288	55
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.001C 27681 KENT RD BONITA SPRINGS FL 34135	PARL IN NE 1/4 OF SW 1/4 AS DESC IN OR 1956 PG 1597 + VAC ROW OR 2850/288	56
PETERSON DALE D & SUSAN C 42W-430 KEDEKA RD SUGAR GROVE IL 60554	31-47-26-B4-00007.0030 27660 KENT RD BONITA SPRINGS FL 34135	PAR IN NE 1/4 OF SW 1/4 DESC OR 2042 PG 641 + VAC ROW OR 2850 PG 288 LESS ROW TAKING INST#2006-290944	57
THOMSON GEORGE D + KIMBERLY B 27620 KENT RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.003A 27620 KENT RD BONITA SPRINGS FL 34135	PARL IN SW 1/4 AS DESC OR 1866 PG 807 + VAC ROW OR 2850 PG 288	58
PETERSON DALE DEAN 42W430 KEDEKA RD SUGAR GROVE IL 60554	31-47-26-B4-00007.003B 27640 KENT RD BONITA SPRINGS FL 34135	PARL IN N 1/2 OF NE 1/4 OF SW 1/4 AS DESC IN OR 1970 /284 + VAC ROW OR 2850/288	59
STATE OF FL DOT PO BOX 1249 BARTOW FL 33831	31-47-26-B4-00007.0040 27700 KENT RD BONITA SPRINGS FL 34135	A PARL OF LAND IN NE 1/4 OF SW 1/4 AS DESC IN OR 936 PG 666	60
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0050 27701 KENT RD BONITA SPRINGS FL 34135	BEG 424.99 FT N OF SW COR OF NE 1/4 OF SW 1/4 TH N 77 DEG 39 MIN 35 SEC E ETC AS DESC OR 1181/1433	61
TAYLOR MARIAN B TR 27704 ORR RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.0060 27710/720 ORR RD BONITA SPRINGS FL 34135	BEG SW COR OF NE 1/4 OF SW 1/4 TH E 300 FT TH N 60 FT TH E 18.35 FT TH NWLY ALG	62
TAYLOR MARIAN B TR 27704 ORR RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.006B 27704 ORR RD BONITA SPRINGS FL 34135	FR SW COR OF NE 1/4 OF SW 1/4 RUN E 299.77 FT TH N 60 FT TH E 85 FT TO POB TH	63
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0250 ACCESS UNDETERMINED BONITA SPRINGS FL	A PARL OF LAND IN NE 1/4 OF SW 1/4 AS DESC IN OR 166 PG 390	64
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0260 ACCESS UNDETERMINED BONITA SPRINGS FL	A PARL OF LAND IN NE 1/4 OF SW 1/4 AS DESC IN OR 266 PG 390	65
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0280 27775 BOONDOCK RD BONITA SPRINGS FL 34135	A PARL OF LAND IN SW 1/4 OF SW 1/4 AS DESC IN OR 404 PG 105	66
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0330 27751 BOONDOCK RD BONITA SPRINGS FL 34135	A PARL OF LAND IN SW 1/4 OF SW 1/4 AS DESC IN OR 404 PG 105	67
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION	31-47-26-B4-00007.0360 ACCESS UNDETERMINED	A PARL OF LAND IN NE 1/4 OF SW 1/4 AS DESC IN OR	68

3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	BONITA SPRINGS FL	438 PG 666	
TIFF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0370 ACCESS UNDETERMINED BONITA SPRINGS FL	BEG SE COR OF NE 1/4 OF SW 1/4 TH W 419.56 FT TH N 60 FT TO POB TH N 125 FT	69
TIFF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0380 ACCESS UNDETERMINED BONITA SPRINGS FL	BEG SE COR OF NE 1/4 OF SW 1/4 TH W 419.56 FT N 185 FT TO POB CONT N 115	70
TIFF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0410 27661 KENT RD BONITA SPRINGS FL 34135	BEG SE COR OF NE 1/4 OF SW 1/4 TH N 689.44 FT TH SWLY 21.01 FT TO PT OF CURV TH + VAC ROW OR 2850/288	71
STRADER CHARLES E 27655 KENT RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.0420 27655 KENT RD BONITA SPRINGS FL 34135	BEG SE COR OF NE 1/4 OF SW 1/4 TH N 689.44 FT TH S 09 DEG 13 MIN 10 SEC W 21.01 + VAC ROW OR 2850/288	72
STRADER CHARLES E 27655 KENT RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.0430 27651 KENT RD BONITA SPRINGS FL 34135	BEG SE COR OF NE 1/4 OF SW 1/4 TH N 689.44 FT TO POB TH S 09 DEG 13 MIN 10 SEC + VAC ROW OR 2850/288	73
THOMPSON MARK D + 25251 PAPHILLION DR BONITA SPRINGS FL 34135	31-47-26-B4-00007.0440 12385 CECIL LN BONITA SPRINGS FL 34135	A PARL OF LAND IN NE 1/4 OF SW 1/4 AS DESC IN OR 461 PG 858	74
TAYLOR PAUL N JR 27704 ORR RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.0540 ACCESS UNDETERMINED BONITA SPRINGS FL	A PARL OF LAND IN NE 1/4 OF SW 1/4 AS DESC IN OR 539 PG 574	75
TIFF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0550 ACCESS UNDETERMINED BONITA SPRINGS FL	A PARL OF LAND IN NE 1/4 OF SW 1/4 AS DESC IN OR 539 PG 578	76
TIFF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B4-00007.0560 12355 CECIL LN BONITA SPRINGS FL 34135	FR SE COR OF NE 1/4 OF SW 1/4 RUN S 88 DEG 41 MIN 50 SEC W 874.11 FT TH N 1 DEG	77
SCHROEDER WILLIAM E + 10800 SOUTHWEST HWY WORTH IL 60482	31-47-26-B4-00008.0000 ACCESS UNDETERMINED BONITA SPRINGS FL	E 1/2 OF SW 1/4 OF SW 1/4 LESS I-75 R/W	78
JOHN P KINGSTON SR TRUST LLC PO BOX 1083 BONITA SPRINGS FL 34133	31-47-26-B4-00100.0130 27966 OAKLAND DR BONITA SPRINGS FL 34135	OAKLAND ACRES MB 51 PG 470 LOT 13	79
SALDIVAR MARY ELLEN TR 27870 LIME ST BONITA SPRINGS FL 34135	31-47-26-B4-00100.0140 27974 OAKLAND DR BONITA SPRINGS FL 34135	OAKLAND ACRES PB 51 PG 470 LOT 14	80
BONITA LEE FLORIDA CO INC 622 DELAWARE AVE SAINT CLOUD FL 34769	31-47-26-B4-00100.0150 27986 OAKLAND DR BONITA SPRINGS FL 34135	OAKLAND ACRES PB 51 PG 471 LOT 15	81
LOPEZ LEONEL PO BOX 3391 BONITA SPRINGS FL 34135	31-47-26-B4-00210.0010 27761 OAKLAND DR BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 10 PB 51 PG 470 LOTS 1 +2	82
GOMEZ PAZ PEDRO J PO BOX 748 BONITA SPRINGS FL 34133	31-47-26-B4-00210.0030 12076 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 10 PB 51 PG 470 LOT 3	83
JOHN P KINGSTON SR TRUST LLC PO BOX 1083 BONITA SPRINGS FL 34133	31-47-26-B4-00210.0040 12075 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITE BLK 10 PB 51 PG 470 LOT 4	84
HALESWORTH ERIC 1257 CALOOSA DR FORT MYERS FL 33901	31-47-26-B4-00210.0050 12067 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITE BLK 10 PB 51 PG 467 LOT 5	85
WEISS LORETTA S 14511 OCEAN BLUFF DR FORT MYERS FL 33908	31-47-26-B4-00210.0080 12067 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 10 PB 51 PG 470 LOT 8	86
GOMEZ FIDENCIO REYES 26861 PALM ST BONITA SPRINGS FL 34135	31-47-26-B4-00210.0090 12077 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 10 MB 51 PG 470 LOT 9	87
FOREMAN PETER 5830 SW 64TH AVE DAVIE FL 33314	31-47-26-B4-00211.0010 12086 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 11 PB 51 PG 470 LOT 1	88
BEACH ROAD & 29 LLC PO BOX 366748 BONITA SPRINGS FL 34136	31-47-26-B4-00211.0020 12096 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND TRAILER SITES TRACT 11 MB 51 PG 470 LOT 2	89
BOTTCHER ANNE 9 BRAMBLE LANE DENNIS MA 02638	31-47-26-B4-00211.0030 12106 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 11 MB 51 PG 470 LOT 3	90
GALLAGHER GAVIN A 126 MILL RIVER RD SOUTH SALEM NY 10590	31-47-26-B4-00211.0040 12101 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 11 MB 51 PG 470 ALL LOT 4	91
BOTTCHER ANNE 9 BRAMBLE LN DENNIS MA 02638	31-47-26-B4-00211.0050 12093 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK.11 MB 51 PG 470 LOT 5	92
JOHN P KINGSTON SR TRUST LLC PO BOX 1083 BONITA SPRINGS FL 34133	31-47-26-B4-00211.0060 12085 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITE BLK 11 MB 51 PG 470 LOT 6	93
GOMEZ FIDENCIO REYES 26861 PALM ST BONITA SPRINGS FL 34135	31-47-26-B4-00211.0070 12087 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 11 PB 51 PG 470 LOT 7	94

LEON ERNESTINO M + PO BOX 979 BONITA SPRINGS FL 34133	31-47-26-B4-00211.0080 12097 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 11 MB 51 PG 470 LOT 8	95
LOYALTY 2016 LLC 3510 25TH AVE SW NAPLES FL 34117	31-47-26-B4-00211.0090 12107 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND PARK TRL.PK. BLK 11 PB 51 PG 470 LOT 9	96
JOHN P KINGSTON SR TRUST LLC PO BOX 1083 BONITA SPRINGS FL 34133	31-47-26-B4-00212.0040 12076 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 12 MB 51 PG 470 LOT 4	97
JOHN P KINGSTON SR TRUST LLC PO BOX 1083 BONITA SPRINGS FL 34133	31-47-26-B4-00212.0050 12072 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 12 PB 51 PG 470 LOT 5	98
JOHN P KINGSTON SR TRUST LLC PO BOX 1083 BONITA SPRINGS FL 34133	31-47-26-B4-00212.0060 12068 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 12 MB 51 PG 470 LOT 6	99
JOHN P KINGSTON SR TRUST LLC PO BOX 1083 BONITA SPRINGS FL 34133	31-47-26-B4-00212.0070 12088 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITE BLK 12 PB 51 PG 470 LOT 7	100
PATINO MARIA G + 12084 TAYLOR ST BONITA SPRINGS FL 34135	31-47-26-B4-00212.0080 12084 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PK TRLR SITES BLK 12 PB 51 PG 470 LOT 8	101
PATINO MARIA G + 12084 TAYLOR ST BONITA SPRINGS FL 34135	31-47-26-B4-00212.0090 12080 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRILER SITES BLK 12 PB 51 PG 470 LOT 9	102
IMPERIAL RIVER ESTATES BONITA SPRINGS FL 34135	31-47-26-B4-00300.00CE IMPERIAL RIVER ESTATES C/E BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 387 PARK AREA LYING EAST OF LOT 87 + WEST OF LOT 86	103
NICOLAS ANTONIO + 27693 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0220 27693 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 387 LOTS 22 + 23 24 + PT OF 21 LESS I/75 RW	104
RUIZ JOSEPH J+ 27631 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0250 27631 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER EST OR 28 GP 387 LOT 25	105
PEREZ CONSUELO & 27623 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0260 27623 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER EST OR 28 PG 387 LOT 26	106
HERRERA FRANCISCO & 27617 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0270 27617 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER EST OR 28 PG 387 LOT 27	107
ARGUETA EDWARD C CASTANEDA & 27611 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0280 27611 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER EST OR 28 PG 387 LOT 28	108
LOUIS ORIOL & 27605 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0290 27605 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER EST OR 28 PG 387 LOT 29	109
DE ARCIA SAUL RANDY 27599 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0300 27599 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER EST OR 28 PG 387 LOT 30 + 31	110
LAYMAN BRUCE 12040 RIVER VIEW DR BONITA SPRINGS FL 34135	31-47-26-B4-00300.0840 12040 RIVER VIEW DR BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 84	111
TEEMS BRITTON R + LYNN 12050 RIVER VIEW DR BONITA SPRINGS FL 34135	31-47-26-B4-00300.0850 12050 RIVER VIEW DR BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 85	112
KELLEY BRIAN P + RENATA 27053 JARVIS RD BONITA SPRINGS FL 34135	31-47-26-B4-00300.0860 12060 RIVER VIEW DR BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 86	113
MEJIA STEPHANIE L 27670 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0870 27670 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 87 + 88	114
MACCLUGAGE MITCHELL B 27688 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0890 27682 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 89	115
MACCLUGAGE MITCHEL B 471 NE 25TH ST APT 202 MIAMI FL 33137	31-47-26-B4-00300.0900 27688 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 90	116
DICKERSON MARISOL 27694 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0910 27694 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 38 LOT 91	117
EWERT NANCY C 27700 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0920 27700 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER EST OR 28 PG 387 LOTS 92 + 93 LESS I-75 R/W	118
PRI-CAR LLP 6601 BAYSHORE RD NORTH FORT MYERS FL 33917	31-47-26-B4-00300.1040 ACCESS UNDETERMINED BONITA SPRINGS FL	IMPERIAL RIVER EST OR 28 PG 387 LOTS 104 + 105	119
IMPERIAL RIVER OAKS LLC 1185 IMMOKALEE RD STE 110 NAPLES FL 34110	31-47-26-B4-00300.1060 ACCESS UNDETERMINED BONITA SPRINGS FL	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 106	120
IMPERIAL RIVER OAKS LLC 1185 IMMOKALEE RD STE 110 NAPLES FL 34110	31-47-26-B4-00300.1070 ACCESS UNDETERMINED BONITA SPRINGS FL	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 107	121
WEISS LORETTA S 14511 OCEAN BLUFF DR FORT MYERS FL 33908	31-47-26-B4-00300.1080 ACCESS UNDETERMINED BONITA SPRINGS FL	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 108 LESS ELY PT AS DESC IN INST# 2011000179689	122

CITY OF BONITA SPRINGS 9101 BONITA BEACH RD BONITA SPRINGS FL 34135	31-47-26-B4-00300.108A ACCESS UNDETERMINED BONITA SPRINGS FL	IMPERIAL RIVER ESTATES OR 28 PG 387 PT OF LOT 108 AS DESC IN INST# 2011000179689	123
STATE OF FL DOT PO BOX 1249 BARTOW FL 33831	06-48-26-B1-00002.0010 ACCESS UNDETERMINED BONITA SPRINGS FL	W 1/2 OF NE 1/4 OF NW 1/4 LYING EAST OF I-75 LESS I-75 R/W	124
CREST BONITA SPRINGS APARTMENT 7000 PEACHTREE DUNWOODY RD NE BLD 14 ATLANTA GA 30328	06-48-26-B1-2800F.0000 28000/190 CREST PRESERVE CIR BONITA SPRINGS FL 34135	BONITA EXCHANGE AS DESC IN INST #2017000053188 TRACT F + PORTION OF ROW IN #2018-145795	125
HORIZON PARK INVESTMENTS LLC 23190 FASHION DR STE 202 ESTERO FL 33928	06-48-26-B1-31000.0010 28051 HUNTERS RIDGE BLVD BONITA SPRINGS FL 34135	HORIZON PARK LOT 1 AS DESC IN INST# 2020000293053	126
NAPLES COMMUNITY HOSPITAL INC 350 7TH ST N NAPLES FL 34102	06-48-26-B1-31000.0020 12570-581 TRADE CENTER DR BONITA SPRINGS FL 34135	HORIZON PARK LOT 2 AS DESC IN INST# 2020000293053	127
HORIZON PARK INVESTMENTS LLC 23190 FASHION DR STE 202 ESTERO FL 33928	06-48-26-B1-31000.0030 28071 HUNTERS RIDGE BLVD BONITA SPRINGS FL 34135	HORIZON PARK LOT 3 AS DESC IN INST# 2020000293053	128
HORIZON PARK INVESTMENTS LLC 23190 FASHION DR STE 202 ESTERO FL 33928	06-48-26-B1-31000.0040 28125 HUNTERS RIDGE BLVD BONITA SPRINGS FL 34135	HORIZON PARK LOT 4 AS DESC IN INST# 2020000293053	129
HORIZON PARK INVESTMENTS LLC 23190 FASHION DR STE 202 ESTERO FL 33928	06-48-26-B1-3100A.00CE RIGHT OF WAY BONITA SPRINGS FL 34135	HORIZON PARK TRACT A AS DESC IN INST# 2020000293053	130
HORIZON PARK INVESTMENTS LLC 23190 FASHION DR STE 202 ESTERO FL 33928	06-48-26-B1-3100C.00CE HORIZON PARK C/E BONITA SPRINGS FL 34135	HORIZON PARK TRACT C AS DESC IN INST# 2020000293053	131
JSF TRADE CENTER DRIVE CMPII L PTA - JD # 1028 PO BOX 320099 ALEXANDRIA VA 22320	06-48-26-B2-00003.0010 12750 TRADE CENTER DR BONITA SPRINGS FL 34135	N 1/2 OF NE 1/4 GOVT LOTS 1+2 LOT CS-1 AS DESC IN INST #2017000069345	132
BERNWOOD LAND HOLDINGS LLC 1110 EUCLID AVE STE 300 CLEVELAND OH 44115	06-48-26-B2-0220A.0010 12611 BONITA BEACH RD SE BONITA SPRINGS FL 34135	BERNWOOD PARK OF COMMERCE UT 3 PB 78 PGS 36 + 37 BLK A LOT 1	133
7-ELEVEN INC 1722 ROUTH ST DALLAS TX 75201	06-48-26-B2-0220A.0020 12661 BONITA BEACH RD SE BONITA SPRINGS FL 34135	BERNWOOD PARK OF COMMERCE UT 3 PB 78 PGS 36 + 37 BLK A LOT 2	134
BELLEZZA SHOPS OF BONITA LLC 19350 NW 123RD CT MICANOPY FL 32667	06-48-26-B2-0220B.0010 12721 BONITA BEACH RD SE BONITA SPRINGS FL 34135	BERNWOOD PARK OF COMMERCE UT 3 PB 78 PGS 36 + 37 BLK B LOT 1 LESS ELY 153 FT AS DESC IN INST#2006-305898	135
BERNWOOD LAND HOLDINGS LLC 1110 EUCLID AVE STE 300 CLEVELAND OH 44115	06-48-26-B2-0220D.0000 RIGHT OF WAY BONITA SPRINGS FL	BERNWOOD PARK OF COMMERCE UT 3 PB 78 PGS 36 + 37 TRACT D	136
SOUTHLINKS ASSOCIATES 12752 TRADE WAY DRIVE #1 BONITA SPRINGS FL 34135	06-48-26-B2-30002.0000 12702 TRADE WAY DR BONITA SPRINGS FL 34135	SOUTHLINKS COMMONS OF BONITA PHASE 2 TRACT 2 AS DESC IN INST 2018000266075	137
SOUTHLINKS ASSOCIATES 12752 TRADE WAY DRIVE #1 BONITA SPRINGS FL 34135	06-48-26-B2-30003.0000 12622 TRADE WAY DR BONITA SPRINGS FL 34135	SOUTHLINKS COMMONS OF BONITA PHASE 2 TRACT 3 AS DESC IN INST 2018000266075	138
SOUTHLINKS ASSOCIATES 12752 TRADE WAY DRIVE #1 BONITA SPRINGS FL 34135	06-48-26-B2-30004.0000 28120 HUNTERS RIDGE BLVD BONITA SPRINGS FL 34135	SOUTHLINKS COMMONS OF BONITA PHASE 2 TRACT 4 AS DESC IN INST 2018000266075	139
SOUTHLINKS ASSOCIATES 12752 TRADE WAY DRIVE #1 BONITA SPRINGS FL 34135	06-48-26-B2-30005.0000 12601 TRADE WAY DR BONITA SPRINGS FL 34135	SOUTHLINKS COMMONS OF BONITA PHASE 2 TRACT 5 AS DESC IN INST 2018000266075	140
SOUTHLINKS ASSOCIATES 12752 TRADE WAY DRIVE #1 BONITA SPRINGS FL 34135	06-48-26-B2-30006.0000 12600 TRADE CENTER DR BONITA SPRINGS FL 34135	SOUTHLINKS COMMONS OF BONITA PHASE 2 TRACT 6 AS DESC IN INST 2018000266075	141
SOUTHLINKS ASSOCIATES 12752 TRADE WAY DRIVE #1 BONITA SPRINGS FL 34135	06-48-26-B2-30007.0000 12620 TRADE CENTER DR BONITA SPRINGS FL 34135	SOUTHLINKS COMMONS OF BONITA PHASE 2 TRACT 7 AS DESC IN INST 2018000266075	142
SOUTHLINKS ASSOCIATES 12752 TRADE WAY DRIVE #1 BONITA SPRINGS FL 34135	06-48-26-B2-30008.0000 12621 TRADE WAY DR BONITA SPRINGS FL 34135	SOUTHLINKS COMMONS OF BONITA PHASE 2 TRACT 8 AS DESC IN INST 2018000266075	143

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JUN 10 2021

Q. Grady Minor
P. Associates, P.A.

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media group
news-press.com A GANNETT COMPANY

0004756858 / FNP-0000001673

Attn:

Q. GRADY MINOR & ASSOCIATES, P
3800 VIA DEL REY
BONITA SPRINGS, FL 34134

STATE OF FLORIDA COUNTY OF LEE:

Before the undersigned authority personally appeared _____, who on oath says that he or she is a Legal Assistant of the News-Press, a daily newspaper published at Fort Myers in Lee County, Florida; that the attached copy of advertisement, being a Legal Ad in the matter of

NOTICE OF PUBLIC NEIGHBORHOOD MEETING A neighborhood information meeting pursuant to Section 4-28, Bonita Springs Land Development Code will be held Thursday,

In the Twentieth Judicial Circuit Court was published in said newspaper in the issues of:

06/04/2021

Affiant further says that the said News-Press is a paper of general circulation daily in Lee County and published at Fort Myers, in said Lee County, Florida, and that the said newspaper has heretofore been continuously published in said Lee County, Florida each day and has been entered as periodicals matter at the post office in Fort Myers, in said Lee County, Florida, for a period of one year next preceding the first publication of the attached copy of advertisement; and affiant further says that he or she has never paid nor promised any person, firm or corporation any discount, rebate, commission or refund for the purpose of securing this advertisement for publication in the said newspaper.

Sworn to and Subscribed before me this 4th of June 2021, by legal clerk who is personally known to me.

Affiant

Notary State of Wisconsin, County of Brown

8-25-23

My commission expires

of Affidavits 1

This is not an invoice

SHELLY HORA
Notary Public
State of Wisconsin

NOTICE OF PUBLIC NEIGHBORHOOD MEETING

A neighborhood information meeting pursuant to Section 4-28, Bonita Springs Land Development Code will be held Thursday, June 28, 2021, 5:30 p.m. at Bonita Springs Fire Department, 27701 Bonita Grande Drive, Bonita Springs, FL 34135. The neighborhood information meeting will provide general information regarding a proposed Planned Development (PD) amendment application for a project known as the Bonita 75 CPD. The applicant, Madison Capital Group Management, LLC, is requesting an amendment to the Bonita 75 CPD to reduce the commercial square footage and add multi-family residential as a use. The applicant is requesting bonus density units. The subject property is located on the northeast quadrant of Bonita Beach Road and Interstate 75 (I-75) and is comprised of approximately 21± acres in Section 31, Township 47 South, Range 26 East, Bonita Springs, Florida. If you have questions or comments, please contact Sharon Umphenour, Senior Planning Technician with Q. Grady Minor & Associates, P.A. by email: sumpenhour@gradymenor.com, phone: 239-947-1144, or mail: 3800 Via Del Rey, Bonita Springs, FL 34134. For project information or to participate remotely, please visit our website at gradymenor.com/planning. The Neighborhood Information Meeting is for informational purposes only, it is not a public hearing.
AD# 4756858 6/4/21

NEIGHBORHOOD INFORMATION MEETING

Bonita 75 CPD Amendment

June 28, 2021

PLEASE PRINT CLEARLY

NAME: MARIAN TAYLOR EMAIL: _____
ADDRESS: 27704 ORR Rd. PHONE: 239-258-2554

NAME: _____ EMAIL: _____
ADDRESS: _____ PHONE: _____

NAME: John W. Costello EMAIL: _____
ADDRESS: 27660 Kent Rd B.S. FL PHONE: _____
Hogs Breath Farm Starvo 34138

NAME: Josh & Valerie Bachman EMAIL: josh@wbgsufl.com
ADDRESS: 12611 Surveyor Creek Ct PHONE: 239 340 1100
Bonita Springs FL 34135

NAME: _____ EMAIL: _____
ADDRESS: _____ PHONE: _____

NAME: _____ EMAIL: _____
ADDRESS: _____ PHONE: _____

NAME: _____ EMAIL: _____
ADDRESS: _____ PHONE: _____

NAME: _____ EMAIL: _____
ADDRESS: _____ PHONE: _____

NAME: _____ EMAIL: _____
ADDRESS: _____ PHONE: _____

NAME: _____ EMAIL: _____
ADDRESS: _____ PHONE: _____

Wayne:

All right. Well, good evening everybody. I'm Wayne Arnold. I'm a planner with Q. Grady Minor & Associates. We're here representing the applicant tonight. We are proposing to rezone the property from a commercial planned development to a mixed-use planned development located at the northeast corner of Bonita Beach Road and I-75. It's known as the Bonita 75 project. I'm going to make some introductions. Tonight, we have Andy Swadel, who is purchasing the property. We have Neale Montgomery, who is the attorney representing us. We have Yury Bykau, who is with T.R. Transportation, a traffic consultant, and this is Sharon Umpenhour, with our office, and she is recording the meeting tonight. We are required to create a transcript of the meeting, so we'll be recording it as required by the City. And then, Mike Delate is our civil engineer with our firm, and he is doing the site civil engineering for the project.

So, I'm going to spend a few minutes telling you a little bit about what's been approved there, and what we are proposing to do, and then we'll be happy to answer any questions that you might have. The property is highlighted here on the screen. It's previously been zoned all for commercial development. It's approved for a variety of retail uses, hotel, business services, indoor self-storage, restaurants. You name it, it's been approved. A little over 200,000 square feet for those uses. This is the approved Master Concept Plan. This is a conceptual development of what the site would look like, and in this particular plan, this is what's approved today, and it allows a variety of outparcel uses right along the what was Orr Road and the I-75 ramp, and then it had a larger commercial tract behind it.

Orr Road was supposed to be vacated in that scenario, and what we're proposing to do is develop residential multi-family apartments on most of the site keeping a small retail component down on the southeast corner of the site closer to Bonita Beach Road and Miller Road. And then, we would have a series of a total of four or five apartment complex buildings, still keeping a large part of the area to the north as what's going to be preservation area. There's a flood plain compensation area we have to do. There was a flood map amendment done, and there was some mitigation required as part of the process. So, a good portion of the northern part of the site stays green.

The primary access point for the residential is going to be just west of the red retail building. We're showing an emergency access farther up on Orr Road that's probably going to just remain gated except for emergency vehicles. So, in that scenario we've got about

10,000 square feet of retail and 252 units replacing a couple hundred thousand square feet of commercial uses. We're going from that master plan which you can see there's a couple of different patch ins up there, part lake, part green space that are still going to be retained in that plan. This one just happens to be a colored version that Madison Capital is working on.

This is not the final plan. This is a conceptual plan that we're working from. They're going to be tightening up the product type and the building for prints, and we'll be going through that process at some point concurrent with the zoning change. So, from a process standpoint, this is the first step in the process. We can't submit an application to rezone the property to the City until we hold a neighborhood meeting. So, once we complete this meeting tonight, we'll be working toward putting the zoning application together, hopefully submitting in the next couple of weeks, and then the process is probably going to take us eight or nine months to wind our way through the process. We'll have another neighborhood information meeting once staff tells us that the application's sufficient for hearing, and then we'll go to the Zoning Board. They'll make a recommendation to City Council.

We're required to have two City Council hearings. They call it a first reading and a second reading. The first reading is simply a reading of the title into the record and telling that you're going to have another hearing on it, and then the second hearing is where we put on the case and make our presentation to Council and give them more of the details of the project. So, that's kind of in a nutshell what we're proposing. I know there's not a lot of detail here, but I'm happy to try to answer any questions or any of our team members that you have any questions.

Speaker 1: So, you said the entrance is going to be at the south –

Wayne: Does this have a pointer in it?

Sharon: I think so.

Speaker 1: Southwest corner, right there, right where Orr turns in, correct?

Wayne: Let me step back here. I'll try to talk to it.

Speaker 1: Southeast, yeah, southeast.

Speaker 2: Right as you come off of Orr Road, and you pull in there.

Speaker 1: What happens to Miller Road?

Wayne: So, Miller Road sits over here and it's going to continue on. This image isn't very great to see it, but there are connection points to Miller Road from both the retail and the residential. This is the primary access point to the residential project right here.

Speaker 1: Okay. Thank you.

Speaker 2: I think it's –

Speaker 1: Right. So, will Miller be paved?

Wayne: That I don't know. Mike, do you know?

Mike: Up to the property entrance.

Wayne: So, the answer was it'll be paved up to the project entrance.

Speaker: 1 Okay, which is?

Wayne: There's an access point shown right there to Miller Road, and another potential access point there.

Speaker 1: Yeah, I'm all the way to the top. So, would it be the second access right before the preserve is where it would end?

Wayne: I don't know. Mike, do we know that yet? Because we show a potential access point right there.

Mike: If that is the final location, that's where it would be paved to.

Speaker 1: Okay.

Speaker 1: Because I know that this, I think the main goal is to have it go from Bonita Grande almost all the way over there right behind City Mattress and to be able to have access like a back through road.

Wayne: Right.

Speaker 1: Like they have at Home Depot now from that whole way.

Wayne: Yeah, I think that's a challenge.

Neale Montgomery: There's been a lot of discussions about that. The problem is when the mid point project went through, they originally had the road on

their plans, but as I understand it, that got changed. I think everybody who owns property here agrees that it would be nice if Snell Lane continued over to Miller to have some kind of network, but I'm not, certain, at least not in my mind, that that's actually going to happen.

Speaker 1: My purpose is I only really care about my little dirt road that is not maintained at all, and I've been putting rock down and dirt down for eight years, and I just wanted, you know ... And right at that preserve point is where it gets better.

Wayne: Right there?

Speaker 1: Yeah. So, from that point on it doesn't have as many washouts and stuff like that. So, if you're going to pave up to there, that helps me.

Wayne: Okay. All right, we'll be working on those details like I said.

Speaker 1: Right.

Wayne: The final location of the access point isn't known tonight, but duly noted.

Speaker 2: A couple of questions. I can't read it from here. The northwest corner, yeah, on the road there. What's that? I can't read it.

Wayne: That is more preservation area and flood plain mitigation.

Speaker 2: They do flood there.

Wayne: Some of that is going to be straight down.

Speaker 2: So, you –

Wayne: And then replanted with vegetation. Some of it will keep retained vegetation in place.

Speaker 2: So, you bought Mrs. Taylor's property, that goes all the way to the creek, or somebody did?

Speaker 1: I think the state may have purchased that property.

Speaker 2: She ... Yeah, it was on/off with her. That goes all the way to the river, creek channel.

Mike: Yeah.

Speaker 2: Oh, cool. That's conservation land?

Wayne: I have other copies of this if somebody wants a presentation copy.

Speaker 1: I'll take a copy of that. That's a more likable photo **[inaudible]**
[00:08:31]

Speaker 2: And then, I guess the other question, just kind of a procedural thing, it's already commercial interchange, why do you have to go back through zoning?

Wayne: Because we didn't ask for any residential originally when we got it approved for commercial.

Speaker 2: So, the commercial interchange doesn't come with so much residential?

Wayne: It does. It allows residential development.

Speaker 2: Right.

Wayne: And we're using the density of that. It allows 10 units per acre. We're allowed to also buy bonus units which we probably will do to get up to about 15 units per acre.

Speaker 2: So, that's why you need –

Neale: No Commercial Interchange is one of the land use categories, the Commercial interchange is the future land use category in the comp plan, and so that allows it. But, then the zoning Resolution doesn't allow the use and so we have to change the zoning..

Speaker 2: You have to do that too. Good. It keeps everybody in business.

Neale: Well, I mean, it's just part of how the process works.

Speaker 2: Right.

Wayne: Anything else from anybody?

Speaker 5: **[Inaudible]** **[00:08:59]** bonus density units?

Wayne: Can you repeat that? I'm sorry. I heard bonus and density and that's about it.

Speaker 5: How much are they and do you have to provide workforce housing?

Wayne: We don't have to provide onsite workforce housing. The City offers a process whereby it's based on an appraisal price that you pay for the land. You then pay that amount per unit into a fund for affordable housing for the City, but we don't have to provide onsite affordable workforce housing. And I think under our proposal, we're acquiring 52 bonus units, something right there, 43. I can't remember the exact number. Sharon, do you remember?

Speaker 3: So, you are applying for bonus density for affordable housing?

Wayne: Yes, it's the mechanism to buy the bonus units. There are two mechanisms. You can do a transfer of development right type bonus, or you can pay toward their bonus pool, which is supposedly for affordable housing.

Speaker 3: Right. Cool.

Wayne: Forty-three, something like that, yeah. Anything else from anybody? No?

Speaker 5: This is an unfair question as most questions are. Ideal world, I assume there are investors involved in this, goes through the process. Somebody starts moving the dirt two years, one year, three years.

Wayne: I think it would be possible about a year.

Neale: If everything goes right.

Wayne: Right.

Speaker 5: Yeah, you know.

Wayne: Because we'll have to amend, we'll have to get local permitting. We have to amend the state water management district permits for the property.

Speaker 5: Oh.

Wayne: But a lot of those are done concurrently, so we can save some time, but just the zoning process alone will be easily a 10-month process in the City.

Speaker 5: Do you think they will make you do impervious surfaces or let you

pave it?

Wayne: Well, we hope we can pave parts of the property. We should be entitled to. I mean, it was –

Speaker 5: With that much conservation land.

Wayne: Yeah, all of that. That odd shape was established by FEMA, so we're working with them the footprint that they've previously approved.

Speaker 5: I see.

Speaker 2: And that has to be raised to 15?

Wayne: I don't know. Mike, do you know?

Speaker 2: The City is 14. The code is 14, I think, and then the City of Bonita Springs added an extra foot.

Wayne: Are you talking about finished floor elevation for the property?

Speaker 5: You'd probably use that.

Speaker 2: Yeah. They'll have to go up 15 NAVD, but the conservation land would stay the way it is.

Speaker 3: Right.

Wayne: Yeah, some of that area is actually going to get scraped down and become more like a marshy lake area. Some of it will be replanted with vegetation.

Speaker 1: I know in Irma, it was under water like the rest of Bonita.

Wayne: Yeah, like Bonita.

Speaker 1: You know, and we had to kayak to get back to the house for two weeks.

Speaker 5: Been there done that, '95 and '92.

Speaker 1: What else? There's no turning lane to get there, so would that be, would there be a turning lane? What's the entrance look like?

Speaker 6: From Bonita Beach Road.

Speaker 2: Yeah.

Wayne: I'm sure we'll have to have a right turn lane into the property, won't we, Mike?

Mike: There is very little room in the right-of-way, so we'll have to work that out with Lee County DOT.

Speaker: 1 Yeah, because I mean I get, it says no U-turns and I'm not making any U-turns, but I'm always getting almost rear ended to get into the middle to be able to make my turn to go down [inaudible] [00:12:39].

Wayne: You're talking a left turn lane to get into the property?

Speaker 1: Correct.

Wayne: Okay. Do we know that yet?

Mike: We will not have that.

Wayne: No, there would be no left turn.

Speaker 1: Then it would be the same as it is now.

Mike: The project as it's contemplated would be right in, right out until Lee County or the City of Bonita allows full access to.

Speaker 1: Okay. There's full access now.

Mike: Right.

Speaker 1: So, they would close that?

Mike: The state –

Neale: Well, they have been predicting for 20 years that they were going to close it.

Speaker 1: Right.

Neale: They probably will, but they haven't yet.

Speaker 1: Well, they talked about closing it at other projects.

Speaker 2: Right.

Wayne: Yes, sir.

Speaker 5: Well, do you have a coming state project on the interstate to correcting backup problems on traffic? Will that combat this development in any way?

Wayne: I don't know. Yury, are you aware of design improvements for the interchange?

Yury: Right now, I'm not. There is none.

Wayne: There are none, okay.

Yury: Not that I know of, no.

Wayne: Okay. So, there are no existing plans for that improvement.

Speaker: Thank you.

Wayne: All right. Anybody else? So, Sharon's got business cards, our contact information is available on our website. We probably have that information up there if anybody wants to take it. Oh, I didn't even get to the images. These are some other samples of what Madison has been involved in. These are some other depictions of three and four-story **[inaudible] [00:14:07]**.

Speaker 5: Madison being the investors?

Wayne: Yes, the contract purchaser.

Speaker 5: Okay.

Wayne: So, obviously it's supposed to be a market rate project.

Speaker 2: And they have built here in this region area?

Wayne: No. They were new to this market.

Speaker 2: No.

[Crosstalk]

Wayne: They've got a market portfolio on multi-families that they've been involved in.

Wayne: So, here's our contact info. Sharon, her email address if you want copies of anything we have.

Speaker 1: Thank you.

Wayne: Things on our website once we make an application submittal, it will be available for viewing. Everything that we've submitted is public right here.

Neale: Right here.

Wayne: All right. Well, goodnight everybody. Thank you.

Speaker 2: Awesome.

Neale: Thank you, guys. See you.

[End of Audio]

Duration: 15 minutes

Bonita 75 Mixed Use Planned Development (MPD) Amendment

June 28, 2021 Neighborhood Information Meeting (NIM)

Project Team

- **Madison Capital Group Management, LLC** – Applicant
- **Neale Montgomery, Esq., Land Use Attorney** – Pavese Law
- **D. Wayne Arnold, AICP, Professional Planner** – Q. Grady Minor & Associates, P.A.
- **Michael Delate, P.E., Civil Engineer** – Q. Grady Minor & Associates, P.A.
- **Yury Bykau, E.I, Traffic Engineer** – TR Transportation Consultants, Inc.
- **Marco A. Espinar, Environmental Planning/Biologist** – Collier Environmental Consultants, Inc.

Project Information

STRAP Number(s): 31-47-26-B4-00007.0100; .0110; .0130; .0140; .0190; .0220; .0290; .0300; .0310; and .0510

Current Zoning: Bonita 75 Commercial Planned Development (CPD)

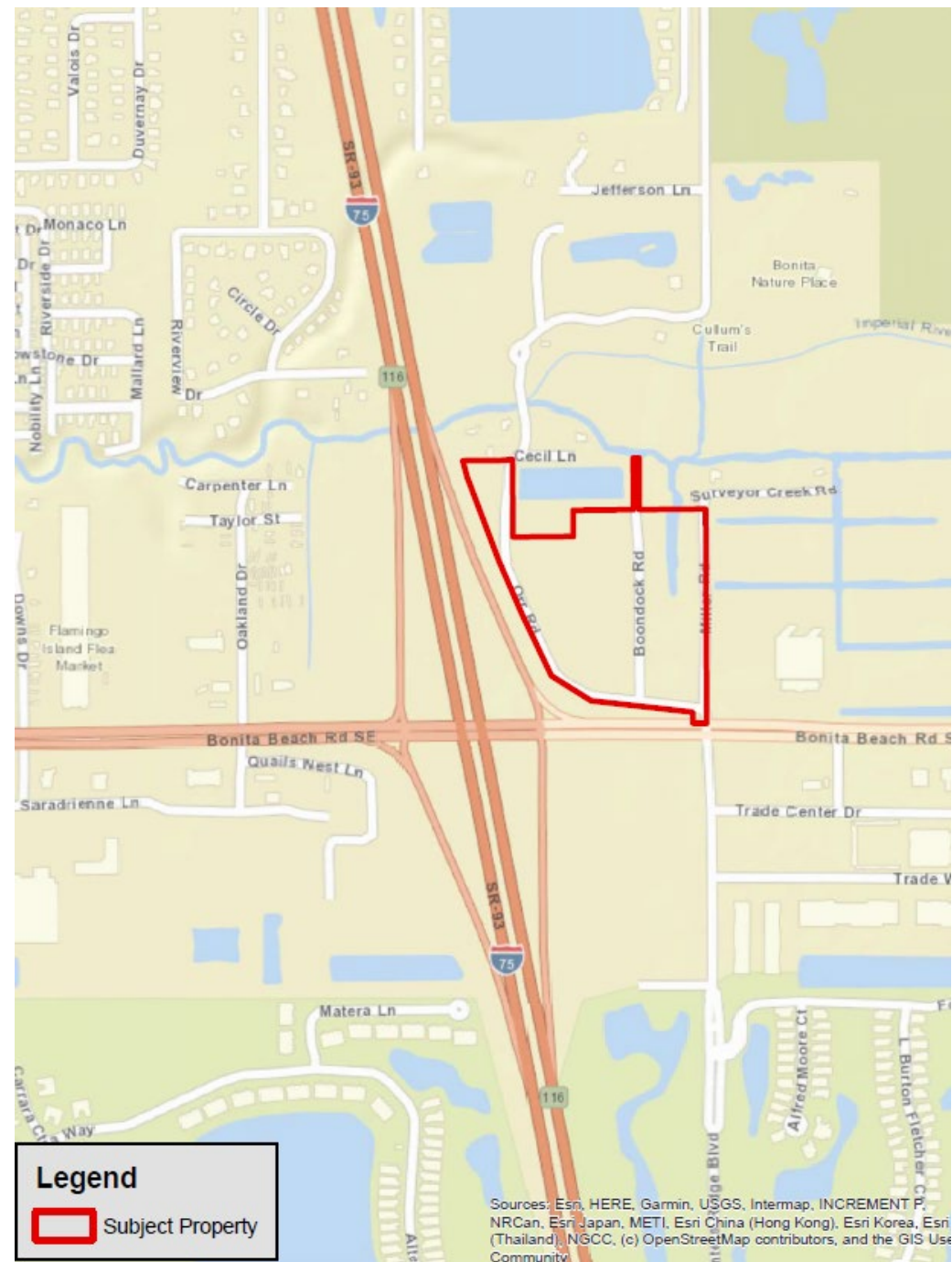
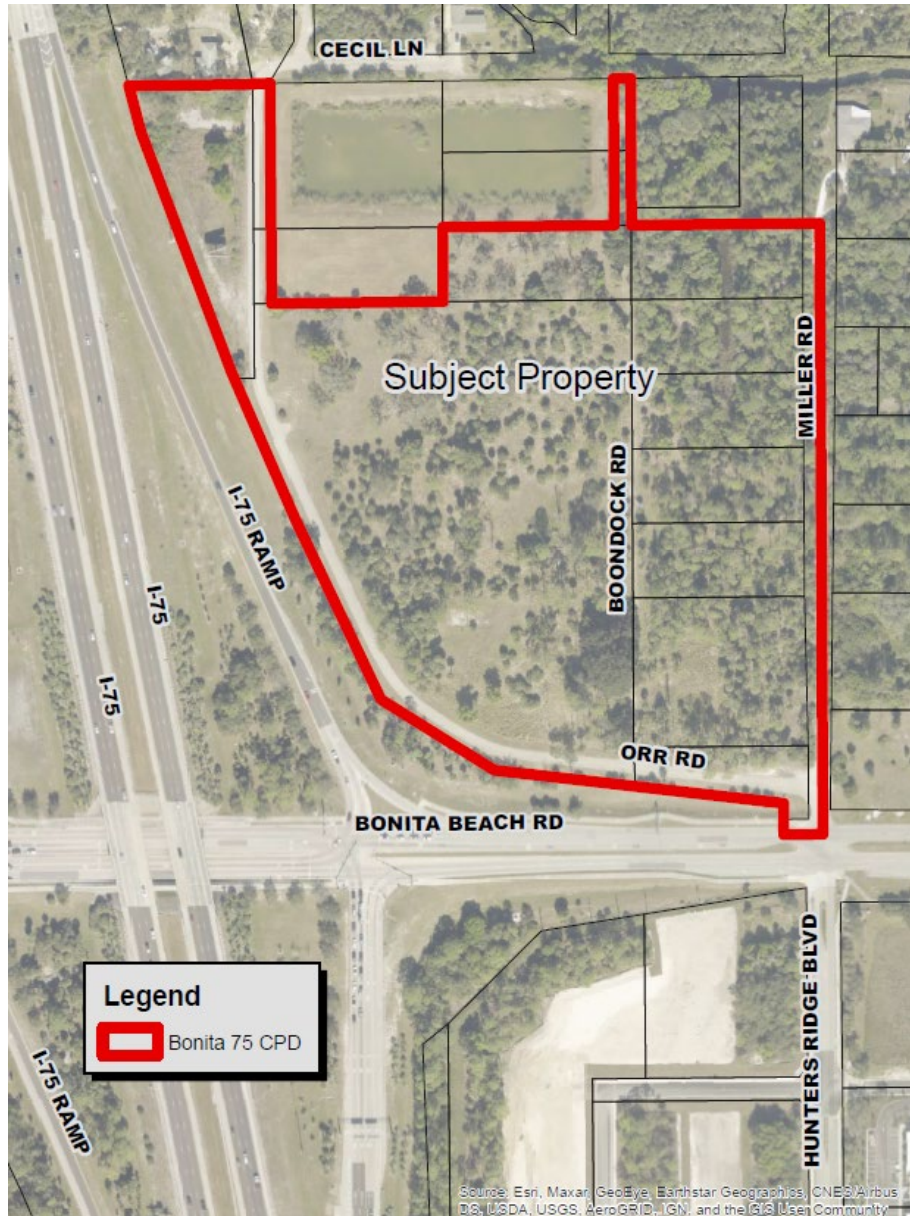
Proposed Zoning: Bonita 75 Mixed Use Planned Development (MPD)

Current Future Land Use Designation: Interchange Commercial

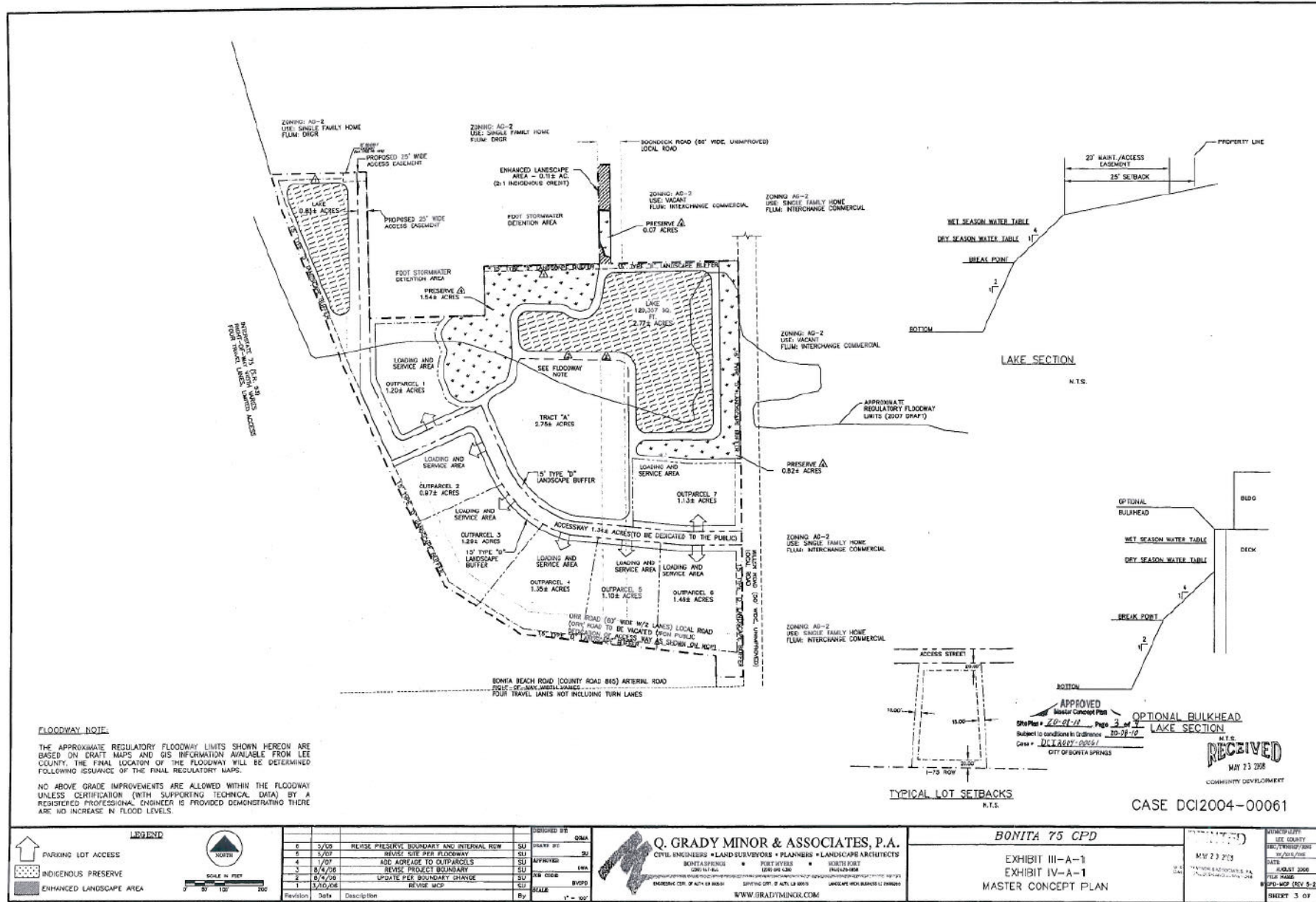
Project Acreage: 21+/- acres

Proposed Request: Amend the CPD to reduce the overall commercial square footage and add a maximum of 252 multi-family residential dwelling units.

Location Map



Approved Master Concept Plan



Proposed Conceptual Plan



COMMERCIAL

- Proposed Pad Ready Parcel for drive-thru retail

Total Site: Approx.. 20.69 Acres
Retail Site: Approx.. 1.80 Acres

6,000 SF Proposed Restaurant

- Drive Through Access
- Trash Enclosure area
- 68 Parking Spaces

Primary Entrances on Miller Road

RESIDENTIAL

- Type V-A Wood Construction
- NFPA 13R Fire Protection @ Residential
- Stand Alone non-sprinklered Clubhouse

Total Site: Approx.. 20.69 Acres
Residential Site: Approx.. 18.89 Acres
 Approx.. Buildable Area: 8.97 Acres

Average Unit Size: 950 SF

- Building A** - 48 UNITS (2x)
- 4 Story Breezeway Building with Elevator
 - Horizontal Exit Via Fire Barrier/Wall Req'd.

- Building B** - 44 UNITS (1x)
- 4 Story Breezeway Building with Elevator
 - Horizontal Exit Via Fire Barrier/Wall Req'd.

- Building C** - 56 UNITS (2x)
- 4 Story Breezeway Building with Elevator
 - Horizontal Exit Via Fire Barrier/Wall Req'd.

TOTAL: 252 Units
 (max allowed)

7,500 SF Stand-Alone Leasing & Amenity Bldg.

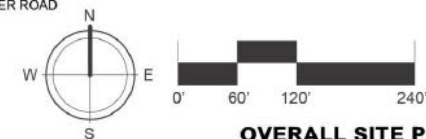
Estimated Unit Mix:

126-1BD / 100-2BD / 26-3BD (50%/40%/10%)

TOTAL PARKING: 453 Spaces
 (~1.80 spaces/unit)

- 431 Surface Spaces
- 22 Garage Spaces
 - 5-Car Garage w/ 4 storage closets ea. (4X)
 - Maintenance w/ 2-Car Garage & 4 storage closets (1X)

- Code Required Parking: 442 Spaces
- 1.5sp/1BD
- 1.75sp/2BD
- 2.0sp/3BD
- 10%DU Visitor



OVERALL SITE PLAN
Orr Road Density Study - Bonita Springs, Florida

June 22, 2021

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Conceptual Multi-Family Renderings



Conceptual Clubhouse Renderings



Conclusion

Documents and information can be found online:

- *Gradyminor.com/Planning*
- *Community Development Public Portal:*
<https://cityofbonitaspringscd.org/welcome/community2/>

Next Steps

- *File application with the City of Bonita Springs Community Development Department*

Contact:

- **Q. Grady Minor & Associates, P.A.:** Sharon Umpenhour
sumpenhour@gradyminor.com or 239.947.1144 extension 1249

The sufficiency Neighborhood Meeting was advertised and scheduled for May 17, 2022, 5:30 PM at the Bonita Springs Fire Department, 27701 Bonita Grande Drive, Bonita Springs, FL. Notification letters were mailed to adjacent property owners within 1,000 feet of the subject properties April 22, 2022 and an advertisement was published in the News-Press April 25, 2022.

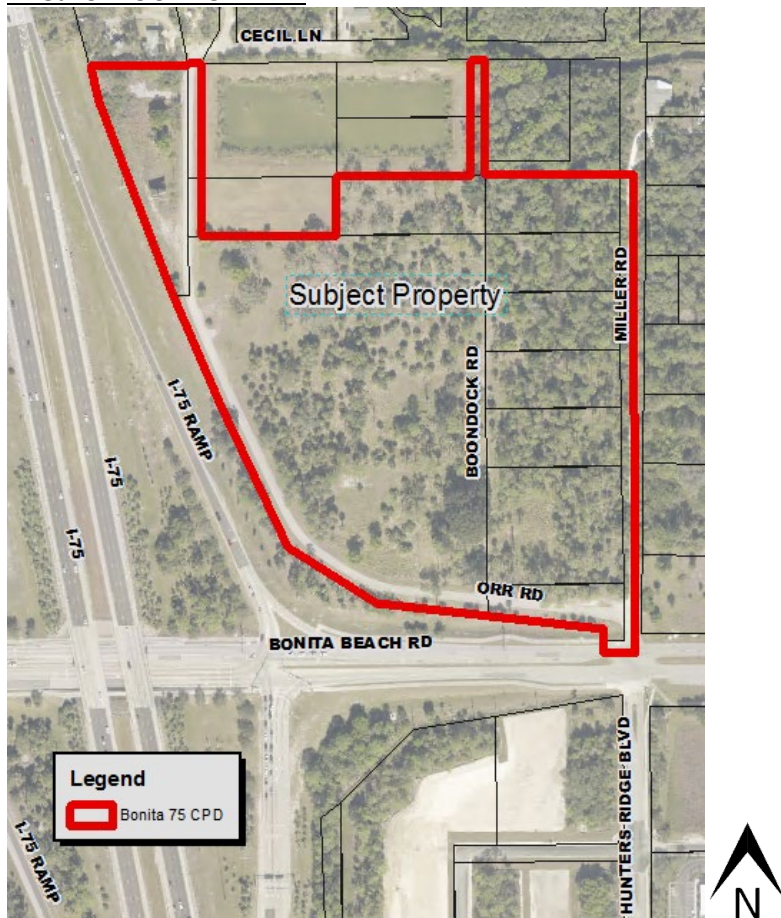
NEIGHBORHOOD MEETING

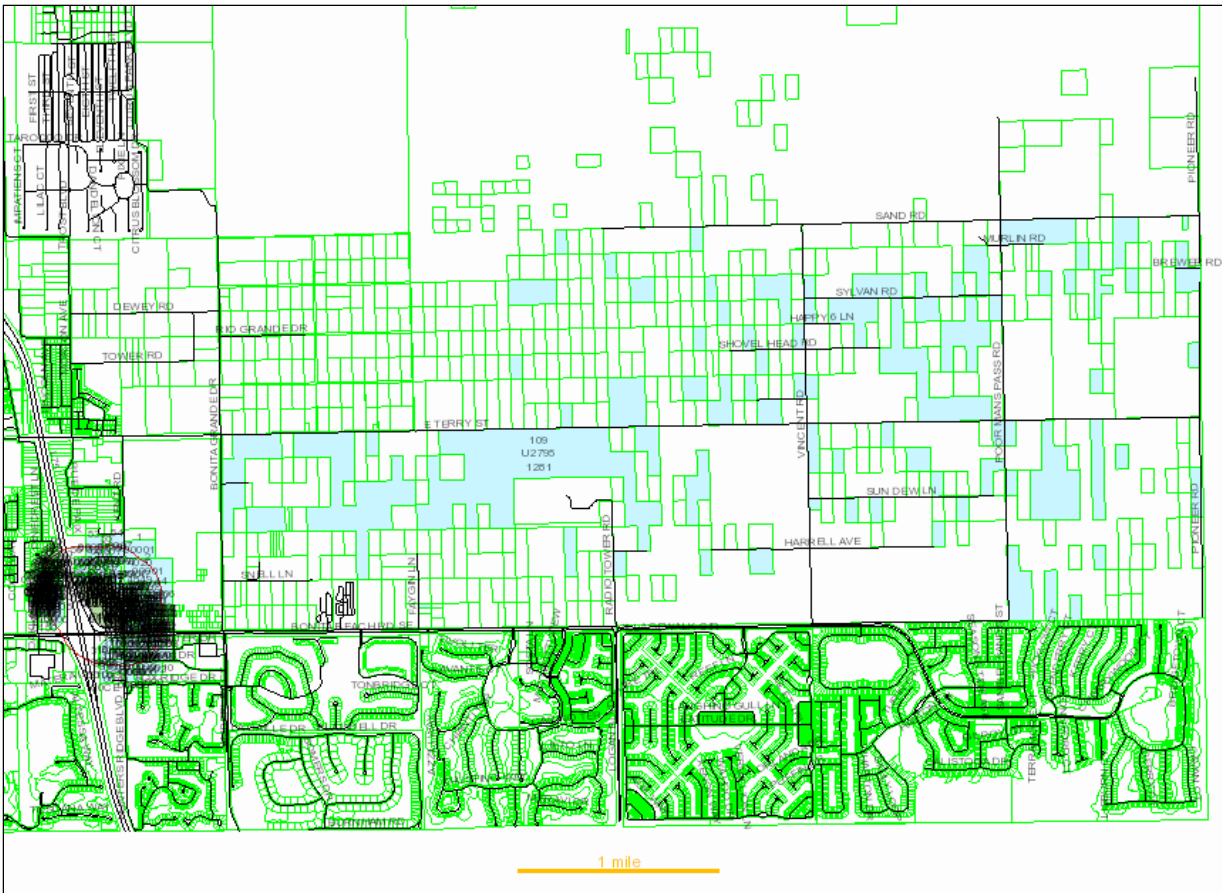
In compliance with Section 4-28 of the Bonita Springs Land Development Code (LDC) a Neighborhood Meeting will be held **May 17, 2022, 5:30 p.m.** at Bonita Springs Fire Department, 27701 Bonita Grande Drive, Bonita Springs, FL 34135.

The Neighborhood Meeting will provide general information regarding a proposed Planned Development (PD) amendment application for a project known as the Bonita 75 CPD. The applicant, Madison Capital Group Management, LLC, is requesting an amendment to the Bonita 75 CPD to reduce the commercial square footage and add multi-family residential as a use. The applicant is requesting bonus density units.

The subject property is located on the northeast quadrant of Bonita Beach Road and Interstate 75 (I-75) and is comprised of approximately 21± acres in Section 31, Township 47 South, Range 26 East, Bonita Springs, Florida. If you have questions or comments, please contact Sharon Umpenhour with Q. Grady Minor and Associates, P.A., 3800 Via Del Rey, Bonita Springs, Florida 34134 by email: sumpenhour@gradymenor.com or phone: 239-947-1144. Project information is posted online at www.gradymenor.com/planning. Please be advised that any information provided is subject to change until final approval by the governing authority. The Neighborhood Meeting is for informational purposes, it is not a public hearing.

PROJECT LOCATION MAP





Date of Report: April 11, 2022

Buffer Distance: feet

Parcels Affected: 129

Subject Parcels: **31-47-26-B4-00007.0000, 31-47-26-B4-00007.009A, 31-47-26-B4-00007.0100, 31-47-26-B4-00007.0110, 31-47-26-B4-00007.0130, 31-47-26-B4-00007.0140, 31-47-26-B4-00007.0190, 31-47-26-B4-00007.0220, 31-47-26-B4-00007.0230, 31-47-26-B4-00007.0240, 31-47-26-B4-00007.0290, 31-47-26-B4-00007.0300, 31-47-26-B4-00007.0310, 31-47-26-B4-00007.0510**
 To change, add or remove subject parcels please change the parcel selection in [GeoView](#)

[Click here to download the map image, mailing labels \(Avery 5161\) and CSV formatted information.](#)

OWNER NAME AND ADDRESS	STRAP AND LOCATION	LEGAL DESCRIPTION	MAP INDEX
CITY OF BONITA SPRINGS 9101 BONITA BEACH RD BONITA SPRINGS FL 34135	31-47-26-B3-00001.4020 27601 KENT RD BONITA SPRINGS FL 34135	S 3/4 OF NW 1/4 OF NW 1/4 OF SE 1/4 + W 1/3 OF S 3/4 OF NE 1/4 OF NW 1/4 OF SE 1/4	1
CITY OF BONITA SPRINGS 9101 BONITA BEACH RD BONITA SPRINGS FL 34135	31-47-26-B3-00001.5000 KENT RD BONITA SPRINGS FL 34135	S 1/2 OF NW 1/4 OF SE 1/4	2
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00701.0010 27925 MILLER RD BONITA SPRINGS FL 34135	THE N 179 FT OF S 378 FT OF E 151 FT OF W 176 FT OF SE1/4(BLK 1 LTS 1+2)	3
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00701.0030 27951 MILLER RD BONITA SPRINGS FL 34135	TH W 75.5 FT OF THE N 154 FT OF THE S 378 FT OF THE E 151 FT OF THE W 327 FT	4
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00701.0040 27955 MILLER RD BONITA SPRINGS FL 34135	THE E 75 FT OF N 154 FT OF S 378 FT OF E 151 FT OF W 327 FT OF SE 1/4	5
THOMPSON ELAINE 27751 MILLER RD BONITA SPRINGS FL 34135	31-47-26-B3-00701.0050 ACCESS UNDETERMINED BONITA SPRINGS FL	SW 1/4 OF SE 1/4 LESS E 1/2 + PARCEL 1006.001 THRU 1006.0350+ I-75 R/W LESS OR 3689 PG 2514	6
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00701.0090 27961 MILLER RD BONITA SPRINGS FL 34135	SUNCOAST LAKES UNREC UN 2 BLK 1 LTS9-12 OR 1944/3067 OR 1464/658 + OR 1689/4584	7
THOMPSON OLIVE ELAINE 27751 MILLER RD BONITA SPRINGS FL 34135	31-47-26-B3-00702.0010 ACCESS UNDETERMINED BONITA SPRINGS FL	A PARL OF LAND IN SE 1/4 OF SW 1/4 OF SE 1/4 AS DESC IN OR 825 PG 625	8
CONKLIN OLIVE ELAINE OLIVE ELAINE THOMPSON	31-47-26-B3-00702.0020 ACCESS UNDETERMINED BONITA SPRINGS FL	FR SE COR TH W ALG S LI 1320.08 FT TH N 0 DEG 12 MIN 34 SEC E 224 FT TH S	9

27751 MILLER RD BONITA SPRINGS FL 34135			
BONITA LOTS LLC 2255 GLADES RD STE 324A BOCA RATON FL 33431	31-47-26-B3-00702.0030 ACCESS UNDETERMINED BONITA SPRINGS FL	FR SE COR TH W ALG S LI 1320.08 FT TH N 00 DEG 12 MIN 34 SEC E 224 FT TH S	10
GOLDER W W + THELMA VICTORIA ECKMAN 129 WELLINGTON DR PERRY GA 31069	31-47-26-B3-00702.0040 ACCESS UNDETERMINED BONITA SPRINGS FL	FROM SE COR RUN W ALG S LI SEC 31 1320.08 FT TH N 224 FT TH W 251.5 FT FOR POB	11
THOMPSON ELAINE 27751 MILLER RD BONITA SPRINGS FL 34135	31-47-26-B3-00702.0090 ACCESS UNDETERMINED BONITA SPRINGS FL	A PARL OF LAND IN SE 1/4 OF SW 1/4 OF SE 1/4 AS DESC IN OR 825 PG 626	12
KIERNAN GENEVIEVE C 3301 TOPPING RD MADISON WI 53705	31-47-26-B3-00702.0100 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN SE 1/4 OF SW 1/4 OF SE 1/4 S 31 T 47 R 26 DESC IN OR 1196 PG 1550	13
BONITA LOTS LLC 2255 GLADES RD STE 324A BOCA RATON FL 33431	31-47-26-B3-00702.0110 ACCESS UNDETERMINED BONITA SPRINGS FL	FR SE COR TH W ALG S LI 1320.08 FT TH N 00 DEG 12 MIN 34 SEC E 224 FT TH S	14
MITCHELL REBECCA R 3524 RIPPLEGROVE DR CINCINNATI OH 45251	31-47-26-B3-00702.0120 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN SW 1/4 OF SE 1/4 DESC IN OR 1792 PG 0359 LOT 12 BLK 2	15
MITCHELL REBECCA 3524 RIPPLE GROVE DR CINCINNATI OH 45251	31-47-26-B3-00702.0130 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN SW 1/4 OF SE 1/4 DESC IN OR 1792 PG 0361 LOT 13 BLK 2	16
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00703.0010 27877 MILLER RD BONITA SPRINGS FL 34135	PAR IN SW 1/4 OF SE 1/4 DESC IN OR 1322 PG 1233 AKA LTS 1+2 BLK 3	17
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00703.0030 27887 MILLER RD BONITA SPRINGS FL 34135	PARL IN S W 1/4 OF S E 1/4 DESC IN OR 1373 PG 684 AKA LTS 3+4 BLK 3	18
CITY MATTRESS OF FLORIDA INC 12660 BONITA BEACH RD SE BONITA SPRINGS FL 34135	31-47-26-B3-00703.0050 12660 BONITA BEACH RD SE BONITA SPRINGS FL 34135	E 1/2 OF THE SW 1/4 OF THE SW 1/4 OF THE SE 1/4	19
BONITA LOTS LLC 2255 GLADES RD STE 324A BOCA RATON FL 33431	31-47-26-B3-00703.0060 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN S W 1/4 OF S E 1/4 DESC IN OR 1364 PG 1240	20
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00703.0080 MILLER RD BONITA SPRINGS FL 34135	FR SW CNR OF SE1/4 RUN N 532 TO POB E 176 S 149 W 176 N149 AKA LTS 9+10 BK 3	21
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00703.0110 MILLER RD BONITA SPRINGS FL 34135	PARC IN SW 1/4 OF SE 1/4 DESC IN OR 1327 PG 1769 AKA LT 11 BK 3 SUNCOAST LK	22
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00703.0120 MILLER RD BONITA SPRINGS FL 34135	PARL IN SW 1/4 OF SE 1/4 AS DESC OR 1445 PG 2255 AKA LT 12 BK 3 SUNCOAST LK	23
LOPEZ RENE J & 12407 LONDONDERRY LN BONITA SPRINGS FL 34135	31-47-26-B3-00704.0010 ACCESS UNDETERMINED BONITA SPRINGS FL	BEG SE COR SEC 31 TH S 89 DEG 48 MIN 15 SEC W 1320.08 FT TH N 0 DEG 12	24
MS HOME SOUTHWEST FLORIDA INC 3372 WOODS EDGE CIR # 103 BONITA SPRINGS FL 34134	31-47-26-B3-00704.0030 ACCESS UNDETERMINED BONITA SPRINGS FL	FR SE COR OF SEC RUN S 89 DEG 48 MIN W 1320.08 FT TH N 0 DEG 12 MIN E 532.0 FT	25
NEW DASA LLC 4951 TAMIAAMI TRL N #103 NAPLES FL 34103	31-47-26-B3-00704.0040 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN S W 1/4 OF S E 1/4 AS DESC IN OR 1392 PG 344 A/K/A LOT 4 BLK 4	26
REARDON ALLISON N & 4090 WOODBRIER DR FORT MYERS FL 33905	31-47-26-B3-00704.0090 ACCESS UNDETERMINED BONITA SPRINGS FL	A PARL OF LAND IN SE 1/4 OF SW 1/4 OF SE 1/4 AS DESC IN OR 462 PG 777	27
REARDON ALLISON N & 4090 WOODBRIER DR FORT MYERS FL 33905	31-47-26-B3-00704.0100 ACCESS UNDETERMINED BONITA SPRINGS FL	A PARL OF LAND IN SE 1/4 OF SW 1/4 OF SE 1/4 AS DESC IN OR 462 PG 777	28
BURCH JAMES N 6 MAUI CIR NAPLES FL 34112	31-47-26-B3-00704.0110 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN S W 1/4 OF S E 1/4 AS DESC IN OR 1392 PG 344 A/K/A LOT 11 BLK 4	29
BURCH JAMES N 6 MAUI CIR NAPLES FL 34112	31-47-26-B3-00704.0120 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN S W 1/4 OF S E 1/4 AS DESC IN OR 1392 PG 344 A/K/A LOT 12 BLK 4	30
BURCH JAMES N 6 MAUI CIR NAPLES FL 34112	31-47-26-B3-00704.0130 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN S W 1/4 OF S E 1/4 AS DESC IN OR 1392 PG 344 A/K/A LOT 13 BLK 4	31
2XJ LLC	31-47-26-B3-00705.0010	PAR IN W 1/2 OF SW 1/4 OF	32

4323 SNOWBERRY LN NAPLES FL 34119	27825 MILLER RD BONITA SPRINGS FL 34135	SE 1/4 DESC OR 1940/2656 AKA BLK 5 LOTS 1 + 2	
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0030 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN SW1/4 OF SE1/4 IN OR1691/4560 AKA LTS3+4 BLK 5 UT2 SUNCOAST LAKES UNR	33
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0050 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN SW 1/4 OF SE 1/4 DESC IN OR 1687 PG 3276 AKA LT 5 BLK 5 UN 2 SUNCOAST LAKES UNREC	34
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0060 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN S W 1/4 OF S E 1/4 DESC IN OR 1354 PG 1264 AKA LT 6 BLK 5 UN 2 SUNCOAST LAKES UNREC	35
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0070 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN W 1/2 OF S W 1/4 OF S E 1/4 DESC IN OR 1537 PG 47	36
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0080 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN SW 1/4 OF SE 1/4 SEC 31 TWP 47 RGE 26 DESC IN OR 1403 PG 1431	37
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0090 27853 MILLER RD BONITA SPRINGS FL 34135	PARL IN S 1/2 OF S 1/2 OF NW 1/4 OF SW 1/4 OF SE 1/4	38
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0100 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL LOC IN THE SW 1/4 OF THE SE 1/4 AS DESC IN OR 3689 PG 2514 AKA LOT 10 BLK 5 UNIT 2 OF SUNCOAST LAKES UNREC SUBD	39
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0110 27857 MILLER RD BONITA SPRINGS FL 34135	PARL IN W 1/2 OF SW 1/4 OF S E 1/4 DESC OR 1584 PG 963 AKA LT 11 BLK 5 SUNCOAST LAKES	40
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0120 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN W 1/2 OF S W 1/4 OF S E 1/4 DESC OR 1584 PG 961 AKA LT 12 BLK 5 SUNCOAST LAKES	41
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0130 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN W 1/2 OF S W 1/4 OF S E 1/4 DESC OR 1584 PG 964 AKA LT 13 BLK 5 SUNCOAST LAKES	42
2XJ LLC 4323 SNOWBERRY LN NAPLES FL 34119	31-47-26-B3-00705.0140 ACCESS UNDETERMINED BONITA SPRINGS FL	PARL IN W 1/2 OF S W 1/4 OF S E 1/4 DESC IN OR 1537 PG 47	43
LYNX ZUCKERMAN AT BONITA GRAND 6131 LYONS RD STE 200 COCONUT CREEK FL 33073	31-47-26-B3-00706.0010 ACCESS UNDETERMINED BONITA SPRINGS FL	N 1/2 OF E 1/2 OF SW 1/4 OF SE 1/4	44
THOMPSON OLIVE ELAINE 27751 MILLER RD BONITA SPRINGS FL 34135	31-47-26-B3-00707.0010 27751 MILLER RD BONITA SPRINGS FL 34135	PARL IN W 1/2 OF SW 1/4 OF SE 1/4 AS DESC IN OR 1921 PG 2852	45
CITY OF BONITA SPRINGS 9101 BONITA BEACH RD BONITA SPRINGS FL 34135	31-47-26-B3-00707.003A ACCESS UNDETERMINED BONITA SPRINGS FL	PARL LYING IN W 1/2 OF SW 1/4 OF SE 1/4 AS DESC IN OR 467 PG 82 + OR 250 PG 911 + OR 1584 PG 961 AKA SUNCOAST LAKES UNIT II LOTS 3 THRU 6	46
BACHMAN JOSHUA R 12611 SURVEYOR CREEK CT BONITA SPRINGS FL 34135	31-47-26-B3-00707.0070 12611 SURVEYOR CREEK CT BONITA SPRINGS FL 34135	PARL IN NW 1/4 OF SW 1/4 OF SE 1/4 AS DESC IN OR 1925 PG 1432	47
TIITF/SFWMD DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	31-47-26-B3-00707.0110 12570 SURVEYOR CREEK CT BONITA SPRINGS FL 34135	PARL IN SW1/4 OF SE1/4 IN OR1326/224 AKA LTS11+12 BLK7 U2 SUNCOAST LAKES UNR	48
WBG SW FLORIDA INC 27800 OLD 41 RD BONITA SPRINGS FL 34135	31-47-26-B3-00707.0160 12630 SURVEYOR CREEK CT BONITA SPRINGS FL 34135	PARL IN NW1/4 OF SW1/4 OF SE14 DESC OR 1894 PG 3722 UNREC PLAT BLK 7 LT 16	49
ARAYA NATALIE LYNN 12320 NOTTING HILL LN BONITA SPRINGS FL 34135	31-47-26-B4-00007.0010 27680 KENT RD BONITA SPRINGS FL 34135	PAR IN NE1/4 OF SW1/4 DESC IN OR 1552 PG 1678 + VAC ROW OR 2850/288 LESS PART S OF RD	50
CITY OF BONITA SPRINGS 9101 BONITA BEACH RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.001A 27671 KENT RD BONITA SPRINGS FL 34135	PARL IN NE 1/4 OF SW 1/4 DESC IN OR 1942 PG 2234 + COMM 689 FT N OF SE COR NE 1/4 OF SW 1/4 TH GO W ALG S LI OF R/W EASE TO PT ON + VAC ROW OR 2850/288	51
HASTY CULLUM L/E 27675 KENT RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.001B 27675 KENT RD BONITA SPRINGS FL 34135	PARL IN NE 1/4 OF SW 1/4 DESC IN OR 1942 PG 2238 + VAC ROW OR 2850/288	52

PETERSON DALE D & SUSAN C TR 42W-430 KEDEKA RD SUGAR GROVE IL 60554	31-47-26-B4-00007.0030 27660 KENT RD BONITA SPRINGS FL 34135	PAR IN NE 1/4 OF SW 1/4 DESC OR 2042 PG 641 + VAC ROW OR 2850 PG 288 LESS ROW TAKING INST#2006-290944	53
THOMSON GEORGE D + KIMBERLY B 27620 KENT RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.003A 27620 KENT RD BONITA SPRINGS FL 34135	PARL IN SW 1/4 AS DESC OR 1866 PG 807 + VAC ROW OR 2850 PG 288	54
PETERSON DALE DEAN 42W430 KEDEKA RD SUGAR GROVE IL 60554	31-47-26-B4-00007.003B 27640 KENT RD BONITA SPRINGS FL 34135	PARL IN N 1/2 OF NE 1/4 OF SW 1/4 AS DESC IN OR 1970 /284 + VAC ROW OR 2850/288	55
STATE OF FL DOT PO BOX 1249 BARTOW FL 33831	31-47-26-B4-00007.0040 27700 KENT RD BONITA SPRINGS FL 34135	A PARL OF LAND IN NE 1/4 OF SW 1/4 AS DESC IN OR 936 PG 666	56
TAYLOR MARIAN B TR 27704 ORR RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.0060 27710/720 ORR RD BONITA SPRINGS FL 34135	BEG SW COR OF NE 1/4 OF SW 1/4 TH E 300 FT TH N 60 FT TH E 18.35 FT TH NWLY ALG	57
TAYLOR MARIAN B TR 27704 ORR RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.006B 27704 ORR RD BONITA SPRINGS FL 34135	FR SW COR OF NE 1/4 OF SW 1/4 RUN E 299.77 FT TH N 60 FT TH E 85 FT TO POB TH	58
STRADER CHARLES E 27655 KENT RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.0420 27655 KENT RD BONITA SPRINGS FL 34135	BEG SE COR OF NE 1/4 OF SW 1/4 TH N 689.44 FT TH S 09 DEG 13 MIN 10 SEC W 21.01 + VAC ROW OR 2850/288	59
STRADER CHARLES E 27655 KENT RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.0430 27651 KENT RD BONITA SPRINGS FL 34135	BEG SE COR OF NE 1/4 OF SW 1/4 TH N 689.44 FT TO POB TH S 09 DEG 13 MIN 10 SEC + VAC ROW OR 2850/288	60
THOMPSON MARK D + 25251 PAPPILLION DR BONITA SPRINGS FL 34135	31-47-26-B4-00007.0440 12385 CECIL LN BONITA SPRINGS FL 34135	A PARL OF LAND IN NE 1/4 OF SW 1/4 AS DESC IN OR 461 PG 858	61
TAYLOR PAUL N JR 27704 ORR RD BONITA SPRINGS FL 34135	31-47-26-B4-00007.0540 ACCESS UNDETERMINED BONITA SPRINGS FL	A PARL OF LAND IN NE 1/4 OF SW 1/4 AS DESC IN OR 539 PG 574	62
SCHROEDER WILLIAM E + 10800 SOUTHWEST HWY WORTH IL 60482	31-47-26-B4-00008.0000 ACCESS UNDETERMINED BONITA SPRINGS FL	E 1/2 OF SW 1/4 OF SW 1/4 LESS 1-75 R/W	63
JOHN P KINGSTON SR TRUST LLC PO BOX 1083 BONITA SPRINGS FL 34133	31-47-26-B4-00100.0130 27966 OAKLAND DR BONITA SPRINGS FL 34135	OAKLAND ACRES MB 51 PG 470 LOT 13	64
SALDIVAR MARY ELLEN TR 27870 LIME ST BONITA SPRINGS FL 34135	31-47-26-B4-00100.0140 27974 OAKLAND DR BONITA SPRINGS FL 34135	OAKLAND ACRES PB 51 PG 470 LOT 14	65
BONITA LEE FLORIDA CO INC 622 DELAWARE AVE SAINT CLOUD FL 34769	31-47-26-B4-00100.0150 27986 OAKLAND DR BONITA SPRINGS FL 34135	OAKLAND ACRES PB 51 PG 471 LOT 15	66
LOPEZ LEONEL PO BOX 3391 BONITA SPRINGS FL 34135	31-47-26-B4-00210.0010 27761 OAKLAND DR BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 10 PB 51 PG 470 LOTS 1 +2	67
GOMEZ PAZ PEDRO J PO BOX 748 BONITA SPRINGS FL 34133	31-47-26-B4-00210.0030 12076 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 10 PB 51 PG 470 LOT 3	68
JOHN P KINGSTON SR TRUST LLC PO BOX 1083 BONITA SPRINGS FL 34133	31-47-26-B4-00210.0040 12075 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITE BLK 10 PB 51 PG 470 LOT 4	69
HERNANDEZ MARIBEL CHAVEZ + 12067 TAYLOR ST BONITA SPRINGS FL 34135	31-47-26-B4-00210.0050 12067 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITE BLK 10 PB 51 PG 467 LOT 5	70
WEISS LORETTA S 14511 OCEAN BLUFF DR FORT MYERS FL 33908	31-47-26-B4-00210.0080 12067 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 10 PB 51 PG 470 LOT 8	71
GOMEZ FIDENCIO REYES 26861 PALM ST BONITA SPRINGS FL 34135	31-47-26-B4-00210.0090 12077 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 10 MB 51 PG 470 LOT 9	72
FOREMAN PETER 5830 SW 64TH AVE DAVIE FL 33314	31-47-26-B4-00211.0010 12086 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 11 PB 51 PG 470 LOT 1	73
BEACH ROAD & 29 LLC PO BOX 366748 BONITA SPRINGS FL 34136	31-47-26-B4-00211.0020 12096 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND TRAILER SITES TRACT 11 MB 51 PG 470 LOT 2	74
BOTTCHER ANNE	31-47-26-B4-00211.0030	OAKLAND PARK TRAILER SITES	75

PO BOX 233 BONITA SPRINGS FL 34133	12106 CARPENTER LN BONITA SPRINGS FL 34135	BLK 11 MB 51 PG 470 LOT 3	
GALLAGHER GAVIN A 126 MILL RIVER RD SOUTH SALEM NY 10590	31-47-26-B4-00211.0040 12101 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 11 MB 51 PG 470 ALL LOT 4	76
BOTTCHER ANNE PO BOX 233 BONITA SPRINGS FL 34133	31-47-26-B4-00211.0050 12093 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK.11 MB 51 PG 470 LOT 5	77
JOHN P KINGSTON SR TRUST LLC PO BOX 1083 BONITA SPRINGS FL 34133	31-47-26-B4-00211.0060 12085 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITE BLK 11 MB 51 PG 470 LOT 6	78
GOMEZ FIDENCIO REYES 26861 PALM ST BONITA SPRINGS FL 34135	31-47-26-B4-00211.0070 12087 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 11 PB 51 PG 470 LOT 7	79
LEON ERNESTINO M + PO BOX 979 BONITA SPRINGS FL 34133	31-47-26-B4-00211.0080 12097 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 11 MB 51 PG 470 LOT 8	80
LOYALTY 2016 LLC 3510 25TH AVE SW NAPLES FL 34117	31-47-26-B4-00211.0090 12107 CARPENTER LN BONITA SPRINGS FL 34135	OAKLAND PARK TRL.PK. BLK 11 PB 51 PG 470 LOT 9	81
JOHN P KINGSTON SR TRUST LLC PO BOX 1083 BONITA SPRINGS FL 34133	31-47-26-B4-00212.0040 12076 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 12 MB 51 PG 470 LOT 4	82
JOHN P KINGSTON SR TRUST LLC PO BOX 1083 BONITA SPRINGS FL 34133	31-47-26-B4-00212.0050 12072 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 12 PB 51 PG 470 LOT 5	83
JOHN P KINGSTON SR TRUST LLC PO BOX 1083 BONITA SPRINGS FL 34133	31-47-26-B4-00212.0060 12068 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITES BLK 12 MB 51 PG 470 LOT 6	84
JOHN P KINGSTON SR TRUST LLC PO BOX 1083 BONITA SPRINGS FL 34133	31-47-26-B4-00212.0070 12088 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRAILER SITE BLK 12 PB 51 PG 470 LOT 7	85
PATINO MARIA G + 12084 TAYLOR ST BONITA SPRINGS FL 34135	31-47-26-B4-00212.0080 12084 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PK TRLR SITES BLK 12 PB 51 PG 470 LOT 8	86
PATINO MARIA G + 12084 TAYLOR ST BONITA SPRINGS FL 34135	31-47-26-B4-00212.0090 12080 TAYLOR ST BONITA SPRINGS FL 34135	OAKLAND PARK TRILER SITES BLK 12 PB 51 PG 470 LOT 9	87
IMPERIAL RIVER ESTATES BONITA SPRINGS FL 34135	31-47-26-B4-00300.00CE IMPERIAL RIVER ESTATES C/E BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 387 PARK AREA LYING EAST OF LOT 87 + WEST OF LOT 86	88
NICOLAS ANTONIO + 27693 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0220 27693 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 387 LOTS 22 + 23 24 + PT OF 21 LESS I/75 RW	89
RUIZ JOSEPH J+ 27631 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0250 27631 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER EST OR 28 GP 387 LOT 25	90
PEREZ CONSUELO & 27623 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0260 27623 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER EST OR 28 PG 387 LOT 26	91
HERRERA FRANCISCO & 27617 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0270 27617 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER EST OR 28 PG 387 LOT 27	92
ARGUETA EDWARD C CASTANEDA & 27611 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0280 27611 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER EST OR 28 PG 387 LOT 28	93
LOUIS ORIOL & 27605 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0290 27605 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER EST OR 28 PG 387 LOT 29	94
DE ARCIA SAUL RANDY 27599 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0300 27599 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER EST OR 28 PG 387 LOT 30 + 31	95
LAYMAN BRUCE 12040 RIVER VIEW DR BONITA SPRINGS FL 34135	31-47-26-B4-00300.0840 12040 RIVER VIEW DR BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 84	96
TEEMS BRITTON R + LYNN 12050 RIVER VIEW DR BONITA SPRINGS FL 34135	31-47-26-B4-00300.0850 12050 RIVER VIEW DR BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 85	97
KELLEY BRIAN P + RENATA	31-47-26-B4-00300.0860	IMPERIAL RIVER ESTATES	98

27053 JARVIS RD BONITA SPRINGS FL 34135	12060 RIVER VIEW DR BONITA SPRINGS FL 34135	OR 28 PG 387 LOT 86	
MEJIA STEPHANIE L 27670 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0870 27670 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 87 + 88	99
MACCLUGAGE MITCHELL B 27688 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0890 27682 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 89	100
MACCLUGAGE MITCHEL B 471 NE 25TH ST APT 202 MIAMI FL 33137	31-47-26-B4-00300.0900 27688 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 90	101
DICKERSON MARISOL 27694 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0910 27694 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER ESTATES OR 28 PG 38 LOT 91	102
EWERT NANCY C 27700 PINECREST LN BONITA SPRINGS FL 34135	31-47-26-B4-00300.0920 27700 PINECREST LN BONITA SPRINGS FL 34135	IMPERIAL RIVER EST OR 28 PG 387 LOTS 92 + 93 LESS I-75 R/W	103
PRI-CAR LLP 6601 BAYSHORE RD NORTH FORT MYERS FL 33917	31-47-26-B4-00300.1040 ACCESS UNDETERMINED BONITA SPRINGS FL	IMPERIAL RIVER EST OR 28 PG 387 LOTS 104 + 105	104
IMPERIAL RIVER OAKS LLC 1185 IMMOKALEE RD STE 110 NAPLES FL 34110	31-47-26-B4-00300.1060 ACCESS UNDETERMINED BONITA SPRINGS FL	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 106	105
IMPERIAL RIVER OAKS LLC 1185 IMMOKALEE RD STE 110 NAPLES FL 34110	31-47-26-B4-00300.1070 ACCESS UNDETERMINED BONITA SPRINGS FL	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 107	106
WEISS LORETTA S 14511 OCEAN BLUFF DR FORT MYERS FL 33908	31-47-26-B4-00300.1080 ACCESS UNDETERMINED BONITA SPRINGS FL	IMPERIAL RIVER ESTATES OR 28 PG 387 LOT 108 LESS ELY PT AS DESC IN INST# 2011000179689	107
CITY OF BONITA SPRINGS 9101 BONITA BEACH RD BONITA SPRINGS FL 34135	31-47-26-B4-00300.108A ACCESS UNDETERMINED BONITA SPRINGS FL	IMPERIAL RIVER ESTATES OR 28 PG 387 PT OF LOT 108 AS DESC IN INST# 2011000179689	108
TIITF STATE OF FLORIDA DEPT OF ENVIR PROTECTION 3900 COMMONWEALTH BLVD TALLAHASSEE FL 32399	33-47-26-L4-U2795.1281 ACCESS UNDETERMINED BONITA SPRINGS FL 34135	PORT OF TWP 47 RNG 26 LYING IN SEC 25 + 26 + 27 + 28 + 31 + 32 + 33 + 34 + 35 + 36	109
STATE OF FL DOT PO BOX 1249 BARTOW FL 33831	06-48-26-B1-00002.0010 ACCESS UNDETERMINED BONITA SPRINGS FL	W 1/2 OF NE 1/4 OF NW 1/4 LYING EAST OF I-75 LESS I-75 R/W	110
HORIZON PARK INVESTMENTS LLC 23190 FASHION DR STE 202 ESTERO FL 33928	06-48-26-B1-31000.0010 28051 HUNTERS RIDGE BLVD BONITA SPRINGS FL 34135	HORIZON PARK LOT 1 AS DESC IN INST# 2020000293053	111
NAPLES COMMUNITY HOSPITAL INC 350 7TH ST N NAPLES FL 34102	06-48-26-B1-31000.0020 12570-581 TRADE CENTER DR BONITA SPRINGS FL 34135	HORIZON PARK LOT 2 AS DESC IN INST# 2020000293053	112
HORIZON PARK INVESTMENTS LLC 23190 FASHION DR STE 202 ESTERO FL 33928	06-48-26-B1-31000.0030 28071 HUNTERS RIDGE BLVD BONITA SPRINGS FL 34135	HORIZON PARK LOT 3 AS DESC IN INST# 2020000293053	113
HORIZON PARK INVESTMENTS LLC 23190 FASHION DR STE 202 ESTERO FL 33928	06-48-26-B1-31000.0040 28125 HUNTERS RIDGE BLVD BONITA SPRINGS FL 34135	HORIZON PARK LOT 4 AS DESC IN INST# 2020000293053	114
HORIZON PARK INVESTMENTS LLC 23190 FASHION DR STE 202 ESTERO FL 33928	06-48-26-B1-3100A.00CE RIGHT OF WAY BONITA SPRINGS FL 34135	HORIZON PARK TRACT A AS DESC IN INST# 2020000293053	115
HORIZON PARK INVESTMENTS LLC 23190 FASHION DR STE 202 ESTERO FL 33928	06-48-26-B1-3100C.00CE HORIZON PARK C/E BONITA SPRINGS FL 34135	HORIZON PARK TRACT C AS DESC IN INST# 2020000293053	116
JSF TRADE CENTER DRIVE CMPII L PO BOX 71870 6890 S 2300 E SALT LAKE CITY UT 84171	06-48-26-B2-00003.0010 12750 TRADE CENTER DR BONITA SPRINGS FL 34135	N 1/2 OF NE 1/4 GOVT LOTS 1+2 LOT CS-1 AS DESC IN INST #2017000069345	117
BERNWOOD LAND HOLDINGS LLC 1110 EUCLID AVE STE 300 CLEVELAND OH 44115	06-48-26-B2-0220A.0010 12611 BONITA BEACH RD SE BONITA SPRINGS FL 34135	BERNWOOD PARK OF COMMERCE UT 3 PB 78 PGS 36 + 37 BLK A LOT 1	118
7-ELEVEN INC 1722 ROUTH ST	06-48-26-B2-0220A.0020 12661 BONITA BEACH RD SE	BERNWOOD PARK OF COMMERCE UT 3	119

DALLAS TX 75201	BONITA SPRINGS FL 34135	PB 78 PGS 36 + 37 BLK A LOT 2	
BONITA SPRINGS SELF STORAGE LL 1200 ASHWOOD PKWY ATLANTA GA 30338	06-48-26-B2-0220B.0010 12721 BONITA BEACH RD SE BONITA SPRINGS FL 34135	BERNWOOD PARK OF COMMERCE UT 3 PB 78 PGS 36 + 37 BLK B LOT 1 LESS ELY 153 FT AS DESC IN INST#2006-305898	120
DISCOUNT AUTO PARTS 5008 AIRPORT RD NW ROANOKE VA 24012	06-48-26-B2-0220B.001B 12731 BONITA BEACH RD SE BONITA SPRINGS FL 34135	BERNWOOD PARK OF COMMERCE UT 3 PB 78 PGS 36 + 37 BLK B ELY 153 FT OF LOT 1 DESC IN INST#2006-305898	121
BERNWOOD LAND HOLDINGS LLC 1110 EUCLID AVE STE 300 CLEVELAND OH 44115	06-48-26-B2-0220D.0000 RIGHT OF WAY BONITA SPRINGS FL	BERNWOOD PARK OF COMMERCE UT 3 PB 78 PGS 36 + 37 TRACT D	122
SOUTHLINKS ASSOCIATES 12752 TRADE WAY DRIVE #1 BONITA SPRINGS FL 34135	06-48-26-B2-30002.0000 12702 TRADE WAY DR BONITA SPRINGS FL 34135	SOUTHLINKS COMMONS OF BONITA PHASE 2 TRACT 2 AS DESC IN INST 2018000266075	123
SOUTHLINKS ASSOCIATES 12702 TRADE WAY DRIVE STE 8 BONITA SPRINGS FL 34135	06-48-26-B2-30003.0000 12622 TRADE WAY DR BONITA SPRINGS FL 34135	SOUTHLINKS COMMONS OF BONITA PHASE 2 TRACT 3 AS DESC IN INST 2018000266075	124
SOUTHLINKS ASSOCIATES 12752 TRADE WAY DRIVE #1 BONITA SPRINGS FL 34135	06-48-26-B2-30004.0000 28120 HUNTERS RIDGE BLVD BONITA SPRINGS FL 34135	SOUTHLINKS COMMONS OF BONITA PHASE 2 TRACT 4 AS DESC IN INST 2018000266075	125
SOUTHLINKS ASSOCIATES 12750 TRADE WAY DRIVE STE 8 BONITA SPRINGS FL 34135	06-48-26-B2-30005.0000 12601 TRADE WAY DR BONITA SPRINGS FL 34135	SOUTHLINKS COMMONS OF BONITA PHASE 2 TRACT 5 AS DESC IN INST 2018000266075	126
SOUTHLINKS ASSOCIATES 12702 TRADE WAY DRIVE STE 8 BONITA SPRINGS FL 34135	06-48-26-B2-30006.0000 12600 TRADE CENTER DR BONITA SPRINGS FL 34135	SOUTHLINKS COMMONS OF BONITA PHASE 2 TRACT 6 AS DESC IN INST 2018000266075	127
SOUTHLINKS ASSOCIATES 12752 TRADE WAY DRIVE #1 BONITA SPRINGS FL 34135	06-48-26-B2-30007.0000 12620 TRADE CENTER DR BONITA SPRINGS FL 34135	SOUTHLINKS COMMONS OF BONITA PHASE 2 TRACT 7 AS DESC IN INST 2018000266075	128
SOUTHLINKS ASSOCIATES 12702 TRADE WAY DRIVE STE 8 BONITA SPRINGS FL 34135	06-48-26-B2-30008.0000 12621 TRADE WAY DR BONITA SPRINGS FL 34135	SOUTHLINKS COMMONS OF BONITA PHASE 2 TRACT 8 AS DESC IN INST 2018000266075	129

RECEIVED

MAY 03 2022

Q. Grady Minor
& Associates, P.A.

Attn:

Q. GRADY MINOR & ASSOCIATES, P
3800 VIA DEL REY
BONITA SPRINGS, FL 34134

STATE OF FLORIDA COUNTY OF LEE:

Before the undersigned authority personally appeared
Nicole Jacobs, who on oath says that
he or she is a Legal Assistant of the News-Press, a daily
newspaper published at Fort Myers in Lee County, Florida; that
the attached copy of advertisement, being a Legal Ad in the
matter of

NOTICE OF NEIGHBORHOOD MEETING In compliance with
Section 4-28 of the Bonita Springs Land Development Code
(LDC) a Neighborhood Meeting will be held May 17, 20

In the Twentieth Judicial Circuit Court was published in said
newspaper editions dated in the issues of or by publication on
the newspaper's website, if authorized, on :

04/25/2022

Affiant further says that the said News-Press is a paper of
general circulation daily in Lee County and published at Fort
Myers, in said Lee County, Florida, and that the said newspaper
has heretofore been continuously published in said Lee County,
Florida each day and has been entered as periodicals matter at
the post office in Fort Myers, in said Lee County, Florida, for a
period of one year next preceding the first publication of the
attached copy of advertisement; and affiant further says that
he or she has never paid nor promised any person, firm or
corporation any discount, rebate, commission or refund for the
purpose of securing this advertisement for publication in said
newspaper editions dated:

Sworn to and Subscribed before me this 25th of April 2022, by
legal clerk who is personally known to me.

Nicole Jacobs
Affiant

Shelly Hora
Notary State of Wisconsin, County of Brown

8-25-23
My commission expires

of Affidavits 1

This is not an invoice

SHELLY HORA
Notary Public
State of Wisconsin

NOTICE OF NEIGHBORHOOD MEETING
In compliance with Section 4-28 of the
Bonita Springs Land Development
Code (LDC) a Neighborhood Meeting
will be held May 17, 2022, 5:30 p.m. at
Bonita Springs Fire Department, 27701
Bonita Grande Drive, Bonita Springs, FL
34135. The Neighborhood Meeting
will provide general information re-
garding a proposed Planned Develop-
ment (PD) amendment application for
a project known as the Bonita 75 CPD.
The applicant, Madison Capital Group
Management, LLC, is requesting an
amendment to the Bonita 75 CPD to
reduce the commercial square footage
and add multi-family residential as a
use. The applicant is requesting bonus
density units. The subject property is
located on the northeast quadrant of
Bonita Beach Road and Interstate 75 (I-
75) and is comprised of approximately
21± acres in Section 31, Township 47
South, Range 26 East, Bonita Springs,
Florida. If you have questions or com-
ments, please contact Sharon
Umpenhour with Q. Grady Minor and
Associates, P.A., 3800 Via Del Rey,
Bonita Springs, Florida 34134 by email:
sumpenhour@gradymenor.com or
phone: 239-947-1144. Project Informa-
tion is posted online at www.gradymenor.com/planning. Please be advised
that any information provided is sub-
ject to change until final approval by
the governing authority. The Neigh-
borhood Meeting is for informational
purposes, it is not a public hearing.
AD#5215157 3/25/22

NEIGHBORHOOD MEETING
Bonita 75 CPD Amendment
May 17, 2022

PLEASE PRINT CLEARLY

NAME:	Josh Bachman	EMAIL:	josh@wbgsufl.com
ADDRESS:	12611 Surveyors Creek Ct Bonita Springs FL 34135	PHONE:	2393401100
NAME:	Valerie Bachman	EMAIL:	VALMCKIN@GMAIL.COM
ADDRESS:	same	PHONE:	239-601-0982

NAME:	Jim Hamilton	EMAIL:	
ADDRESS:	3001 Tamiami Trail suite 206 Naples, FL 34103	PHONE:	239-398-4482

NAME:	John Callis	EMAIL:	
ADDRESS:	3001 Tamiami Trail S. 206 Naples, FL 34103	PHONE:	

NAME:		EMAIL:	
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Meeting Summary:

The required post sufficiency information meeting for the Bonita 75 CPD/RPD was held at 5:30 p.m. on May 17, 2022 at the Bonita Springs Fire Department at 27701 Bonita Grande Drive.

Representatives in attendance for the applicant were:

Wayne Arnold - Q. Grady Minor & Associates

Todd Bleakley - Madison Capital Group

Mike Delate - Q. Grady Minor & Associates

Sharon Umpenhour - Q. Grady Minor & Associates

Four members of the public were in attendance. A sign in sheet was provided and it has been attached to the meeting transcript.

Mr. Arnold opened the meeting and described the request for the rezone from CPD to CPD/RPD, which would reduce the commercial uses to 10,000 square feet and permit 252 multi-family residential dwelling units.

Mr. Arnold provided a copy of the site plan and highlighted commercial tract, residential areas, preserve areas buffers and access points. Additional visual details of the site and proposed buildings were displayed and discussed.

Members of the public asked questions regarding access from Bonita Beach Road and into the site and improvements to Miller Road.

Mr. Arnold provided the attendees with contact information and links to the project information on their company website.

The meeting was adjourned at approximately 6:00 p.m.

Meeting Transcript:

Wayne Arnold: All right. Well, good evening. I'm Wayne Arnold. I'm with Grady Minor and Associates. This is Sharon Umpenhour. She's also with our firm. This is a required neighborhood for the city of Bonita. And it's for a rezoning opportunity for the property. And we're required to take minutes and record the meeting. So, just so you know, you're being recorded. But, with me tonight is Mike Delate. He's also from our office as well. He's the engineer that's been working on the project. And then Todd Bleakley is with Madison Capital Group who's under contract to purchase the property.

So, the subject property, you all are probably fairly aware of where it is. But it's about 20 acres, located right at the northbound onramp on I-75 and Bonita Beach Road. It's all of this. And it's currently zoned for CPD. It was going to allow about 120,000 square feet of commercial uses. And we're seeking to change the zoning to allow 252 rental apartments and up to 10,000 square feet of commercial.

[00:01:02] So, substantially changing the intensity of it and lessening the traffic impacts by doing that.

So, we've been in the process for almost a year. And we've been deemed sufficient. They're trying to schedule us for our zoning board hearing. We have to hold another neighborhood meeting, which is the meeting tonight, before they will get us to that zoning board agenda. So, we're going to – I'm going to spend a few minutes just talking to you about what our proposed changes are, try to answer any questions you may have and then, um, we will call it an evening.

So, the property is currently in your interchange commercial zoning or land use category. And it allows for us to seek up to 15 units an acre. We're coming in at about 12 units per acre. And its, 252 units. It's about 12 dwellings unit's an acre. And we're also acquiring bonus density units to get to the 252 units. That's a separate agreement process through council whereby, Todd's group will have to purchase units for a price per unit cost that will go to the city's some sort of trust fund that they've established.

[00:02:08] But that's the process for that. This is the existing master plan that showed commercial only. So, access was here on Bonita Beach Road where Orr Road and Miller Road sort of come together. And that contemplated a bunch of out parcels with one larger commercial tract in the back. The features were going to be,

obviously, a lake, some preserve, some open space. So, the plan has changed, obviously, to accommodate the residential. And right now, all of this area in the middle is becoming our residential tract. And the commercial tract is about 1.4 acres located where Orr Road touches Miller Road and Bonita Beach Road. So, that would support the 10,000 square feet of commercial uses. We've asked for a variety of commercial uses.

[00:03:00]

We have given up certain uses that the staff asked us to. They have a new Bonita Beach corridor looking to, you know, make it a little bit more beautiful. And so we have done that.

The more specific site plan that we've come up with shows, obviously, the commercial tract. We don't have an end user here. But you can see clubhouse building sits here, facing Miller Road. And then we've got a series of three larger residential buildings and maybe having what they call townhome rentals. They're not really a townhouse, but they function more like on in the rental community where each unit would have its own walkup and you don't really have a neighbor on top of you like you do on the conventional. These buildings are proposed to be four-stories, the larger buildings that we've shown there.

So, we also have an alternative plan because we're still trying to work out the access on Miller Road. So, Miller Road was originally planned – What happened to my pointer? It's going out on me.

[00:04:00]

But it was originally making improvements to a portion of Miller Road to access here. We may be required to access a shorter location or even using Orr Road at this location. But we're still working with staff to try to figure that out. Todd was meeting with some of the city staff people today to go over that – that very issue.

So, this is our list of uses. So, for the residential, we've asked for multi-family residential. And that's limited to the 252 units. And on the CPD portion, 10,000 square feet of those. So, we've asked for things like auto parts store, banks, boat parts store, business services, which is a whole host of, you know, insurance companies to engineering companies and things like that. We've asked for food stores, some specialty retail, things of that nature. Nothing too crazy. And I think staff seems supportive of least the use mix that we have proposed.

Some of our site development regulations that we're required to adopt, maximum building height is four stories, not to exceed 50

feet.

[00:05:03]

And the building height for the commercial is restricted to 35 feet. So, that might get a two-story office type building. But most likely, it will end up being a one-story, retail user.

We are asking for a few deviations from the code. I'm not going to go through a lot of those details. Some of those were kind of leftovers from the commercial. There's a lake set back issue that we had. Some permanent signage, we wanted some shared signage between the residential and commercial. Landscape buffer adjacent to the right of way to 10 feet on Orr Road, so we can leave Orr Road essentially where it is and not have to worry about relocating portions of that, and that will remain intact.

The City has a requirement for complete streets that we've asked for a deviation from. And that talks about having blocks and block lengths not exceeding certain – certain limits. And then providing for this walkability internal to the site. We will have internal walkability, but we're not going to have internal streets.

[00:6:02]

So, we've asked for a deviation not to have to comply with all of those requirements. And then we also asked for a deviation from the parking requirements. We've done this on several apartment complexes in Bonita. The Bonita code, in our opinion, and most of our client's opinion, tends to overpark multi-family buildings. So, we've asked for a deviation that will allow Madison to develop that ration that they've used other places successfully in the state of Florida. So, this highlights where we've asked for those deviations. So, this is an Orr Road potential relocation if we improve Miller Road, that takes away the conflict here that staff wanted us to. And the deviations reflect the locations requested for those.

This is some examples of what, Madison is doing. A few different architectural styles. This depicts three-story buildings. They also got four-story concepts.

[00:07:00]

They've worked with different architectural firms to come up with a style that's, you know, appropriate for our location. Todd, I don't know that you all have settled on a final architectural style out of this, have you?

Todd Bleakley:

No, we haven't. These are still pretty represented images for what the design intent is. It will be four-story in nature. I know we have a couple that are three-story there. But, ah, yeah, so, the residential buildings themselves are four. We have a clubhouse that will be

single-story. Probably interior elevations up to 14 foot in height. And then our [inaudible – crosstalk] [00:07:32] units will be two-stories, as well.

Wayne: So, these are represented as some of the pool amenity areas. So, I showed you clubhouse on the more illustrative plan, which was – let me go back to it just to point that out again. So, on that plan, the clubhouse, and the amenity that you were seeing are in that location. So, you see the pool, clubhouse will have recreational amenities there. Making provisions for a potential dog walk up on this part of the project.

[00:08:02] There's just sort of some leftover land. There's a floodplain area that runs through a portion of the site. And that's why we have so much of our preservation area located sort of north of this line, that coincides with the floodway line. So, we are allowed to put improvements there. We'll be revegetating some of the site. We'll be digging a lake and working with water management district on that.

So, that's where we are. We don't have a specific hearing date established yet. But we are working on staff now. They're – In their process, they have something they call substantive comments. We've drafted responses to that. And we'll be working with staff to try to refine, their recommendations and conditions they may impose. At this point, we're probably going to be on the July zoning board meeting, would be my guess if I had to guess. I think we're going to be too late to make the June meeting.

[00:09:00] So, it's probably going to be July for zoning board. And then we have two council hearings, a first hearing at which they don't really take any public input. And then the second reading is considered the final zoning hearing where they would actually take action on the project.

So, that's kind of, in a nutshell, what we've, been working on. We're happy to answer any questions that you have.

Male Speaker 1: So, you don't really know what your plan would be for Miller Road as of yet?

Wayne: Not exactly. I mean our –

Male Speaker 1: It could be a few different things because you could enter sooner or –

Wayne: Yeah, I think our ideal plan is sort of this. The cross-access shows, this being Miller Road. Our – Our idea would be to improve it here, ideally. And that would be the entrance to the residential community. This would be an entrance to the commercial. And then Orr Road gets sort of relocated around that commercial. There's still some question on the status of Miller Road. So, we may be working with staff to – to move an access point a little bit farther to the west.

[00:10:01] You know, it's something that Todd's had his title company looking and staff's been looking at it. As of today, we just don't have a definitive answer. But our intent and best-case scenario would be to improve Miller Road at least up to that entrance point.

Female Speaker: What about Bonita Beach Road?

Wayne: What, I'm sorry?

Female Speaker: What about Bonita Beach Road as far as like the entrance and the exit. Because I know they were talking about a light up the way. Are they going to get rid of that median where you can cut in?

Wayne: Yeah, Lee County has told us that they're in the process of obtaining bids to close the median opening at Hunter's Ridge. So, that would force our traffic to be right-in, right-out only. And then if you're eastbound, you'd have to make a U-turn movement and come back into the process. But we will have a right-in, right-out only condition. Sir?

Male Speaker 2: Is there any offsite decel lane?

Wayne: Mike?

Mike Delate: There's only a small length available for the decel lane eastbound to westbound.

[00:11:05] And we're not planning on that being a lefthand at the existing opening there. It's just a **[inaudible]** **[00:11:11]** improvements on –

Wayne: Hey, Mike, can you speak up a little?

Mike: Yeah. There'll be no improvements on eastbound Bonita Beach Road.

Male Speaker 2: Right, but on westbound, will there be a decel lane?

Mike: Yes.

Male Speaker 2: Okay. So, that's not shown on it?

Mike: No, that's just a zoning plan.

Wayne: Yeah, the zoning plan doesn't reflect the access conditions because they're always subject to change.

Mike: They might be on the other.

Male Speaker 2: So, essentially you have a turning lane in.

Mike: Correct.

Male Speaker 2: The right in would be a turning lane.

Mike: Yes.

Male Speaker 2: Because right now, you're about to get rear ended if you try and turn down the road.

Wayne: So, here's a depiction of, you know, right turn lane going into Orr Road. What else? Anything?

[00:12:00] Pretty straight forward.

Male Speaker 2: So, if you guys get approved, when would you, like break ground? When are you hoping to break ground?

Todd: So, our next – our next step, once we get vested through the two-city council hearing, we start the DO, development order process, which is our engineer site plan. That's, Mike, correct me if I'm wrong, about a five-to-seven-month process.

Mike: From the time of submittal.

Todd: From time of submittal. So, you know, it's – you know, mid '23 construction start, let's call it.

Wayne: About a year from now.

Todd: Yeah, hopefully sooner. But reality is probably that.

Male Speaker 1: Does any of this require LDOT approvals?

Mike: Yes, we'll have to get a what they call a limited review development order for the right turn lane in. But that's about the extent of it.

[00:13:01]

Male Speaker 1: And is there any, ah, zoning review by the county? Or is it all city?

Wayne: It's all city. They do share the zoning applications because it's a Lee County road, Bonita Beach Road. So, we receive comments from Lee County staff with regard to transportation comments. But other than that, no. Everybody good? Appreciate you coming out. So, our contact information is on the last page. This information's on our webpage, too. If you want to just check in. Sharon, emailable for her or phone call will get you whatever information we have that's public record.

Male Speaker 2: Do you guys have anything to do with the stuff east of Miller?

Wayne: No. I mean, Todd – Todd's been talking to a consortium of people about possible east-west roadway link.

[00:14:05] But I think that's, you know, it's talk at this point.

Todd: Very early on in the process.

Male Speaker 2: Because they're surveying that portion now.

Wayne: I think the St. James property, as we call it, may be under contract for sale. So, somebody may be surveying that for purchase. But we're not involved in that project.

Male Speaker 2: Okay.

Wayne: Good? Thanks, we're adjourned. Appreciate it, very much.

[End of Audio]

Duration: 15 minutes

Bonita 75 CPD/RPD AMENDMENT

May 17, 2022, Neighborhood Meeting

Project information and a copy
of this presentation can be
found on our website:



www.gradyminor.com/Planning

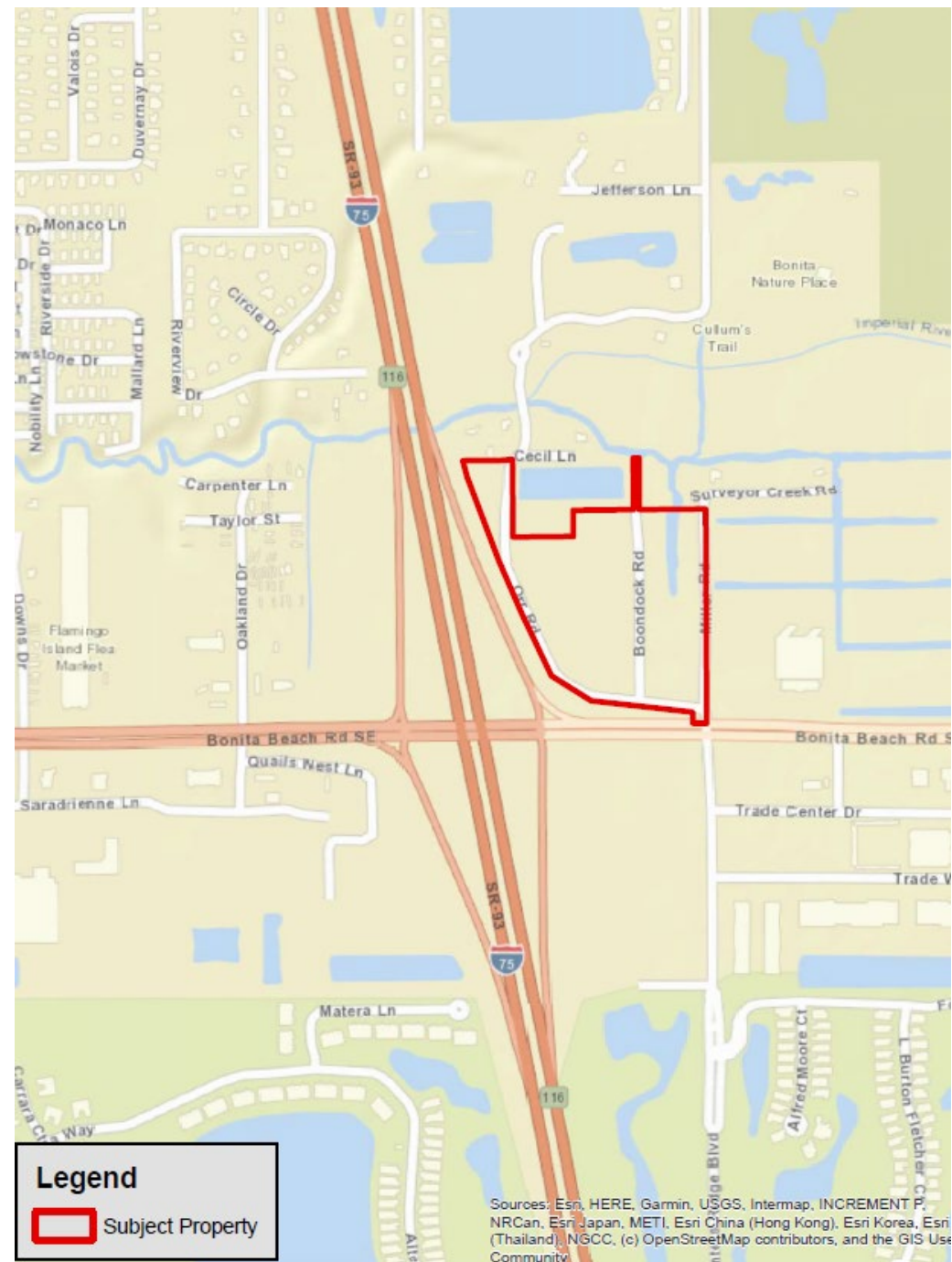
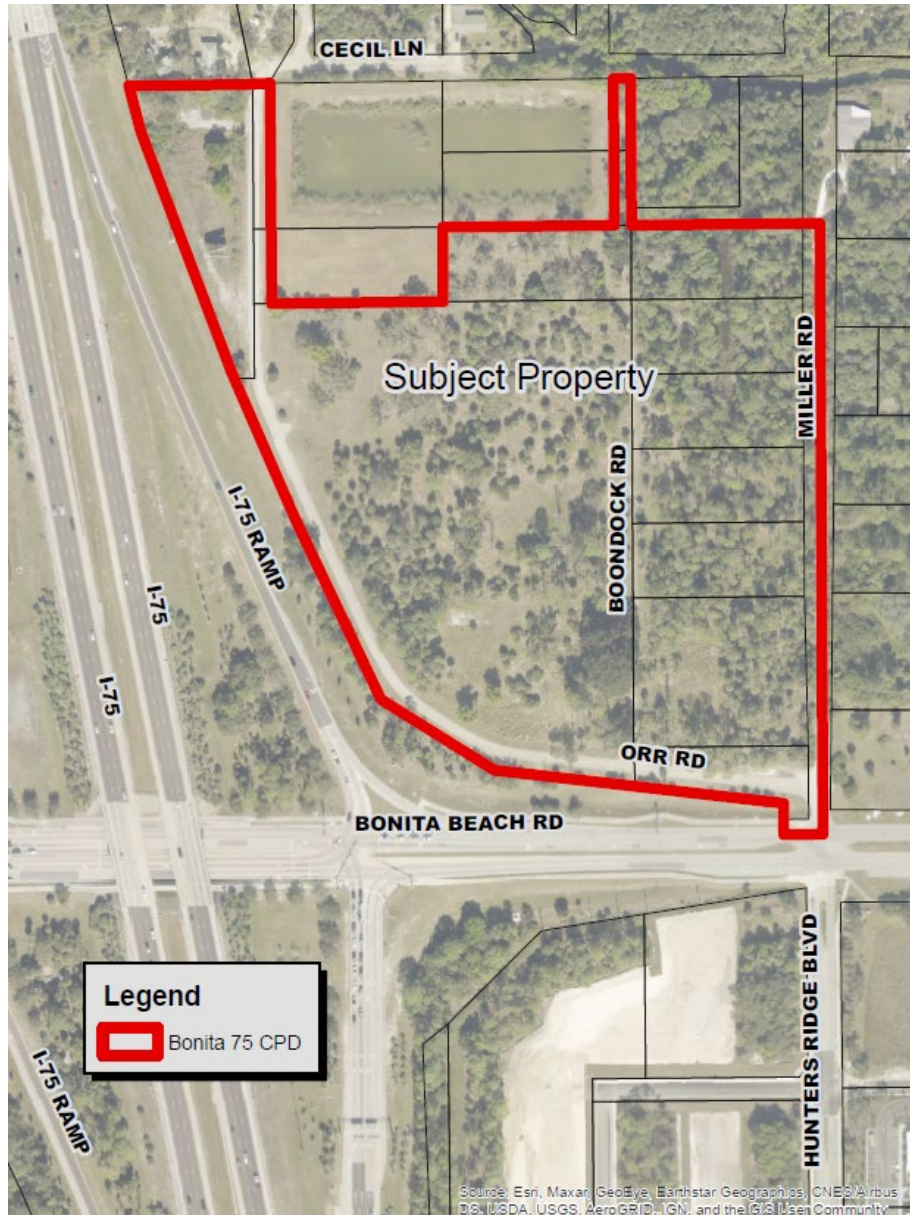
INTRODUCTION

PROJECT TEAM:

- **Madison Capital Group Management, LLC** – Applicant
- **Neale Montgomery, Esq., Land Use Attorney** – Pavese Law
- **D. Wayne Arnold, AICP, Professional Planner** – Q. Grady Minor & Associates, P.A.
- **Michael Delate, P.E., Civil Engineer** – Q. Grady Minor & Associates, P.A.
- **Yury Bykau, E.I, Traffic Engineer** – TR Transportation Consultants, Inc.
- **Marco A. Espinar, Environmental Planning/Biologist** – Collier Environmental Consultants, Inc.

**Please note, all information provided is subject to change until final approval by the governing authority.*

LOCATION MAP



PROJECT INFORMATION

STRAP Number(s): 31-47-26-B4-00007.0100; .0110; .0130; .0140; .0190; .0220; .0290; .0300; .0310; and .0510

Current Zoning: Bonita 75 Commercial Planned Development (CPD)

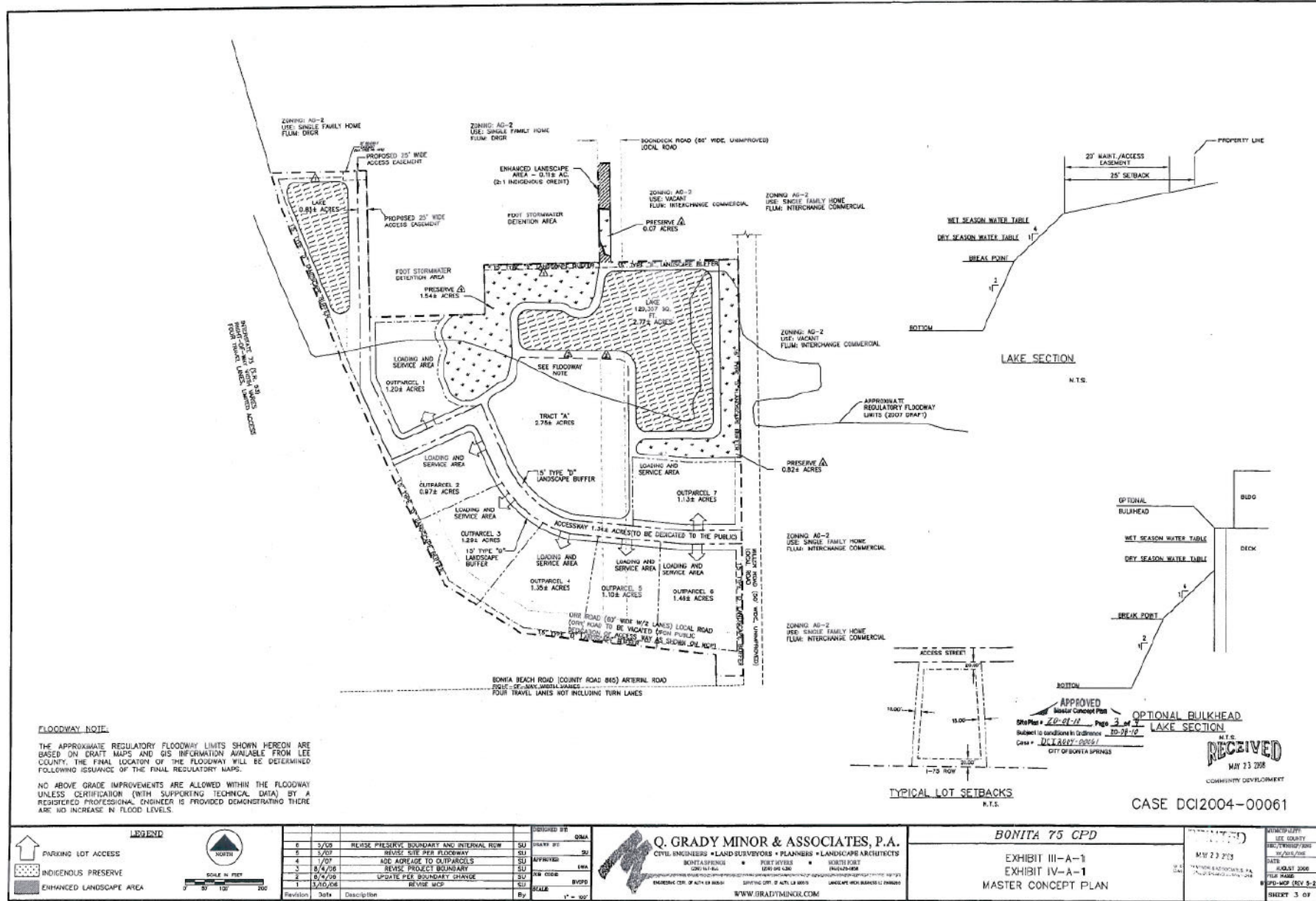
Proposed Zoning: Bonita 75 Commercial/Residential Planned Development (CPD/RPD)

Current Future Land Use Designation: Interchange Commercial

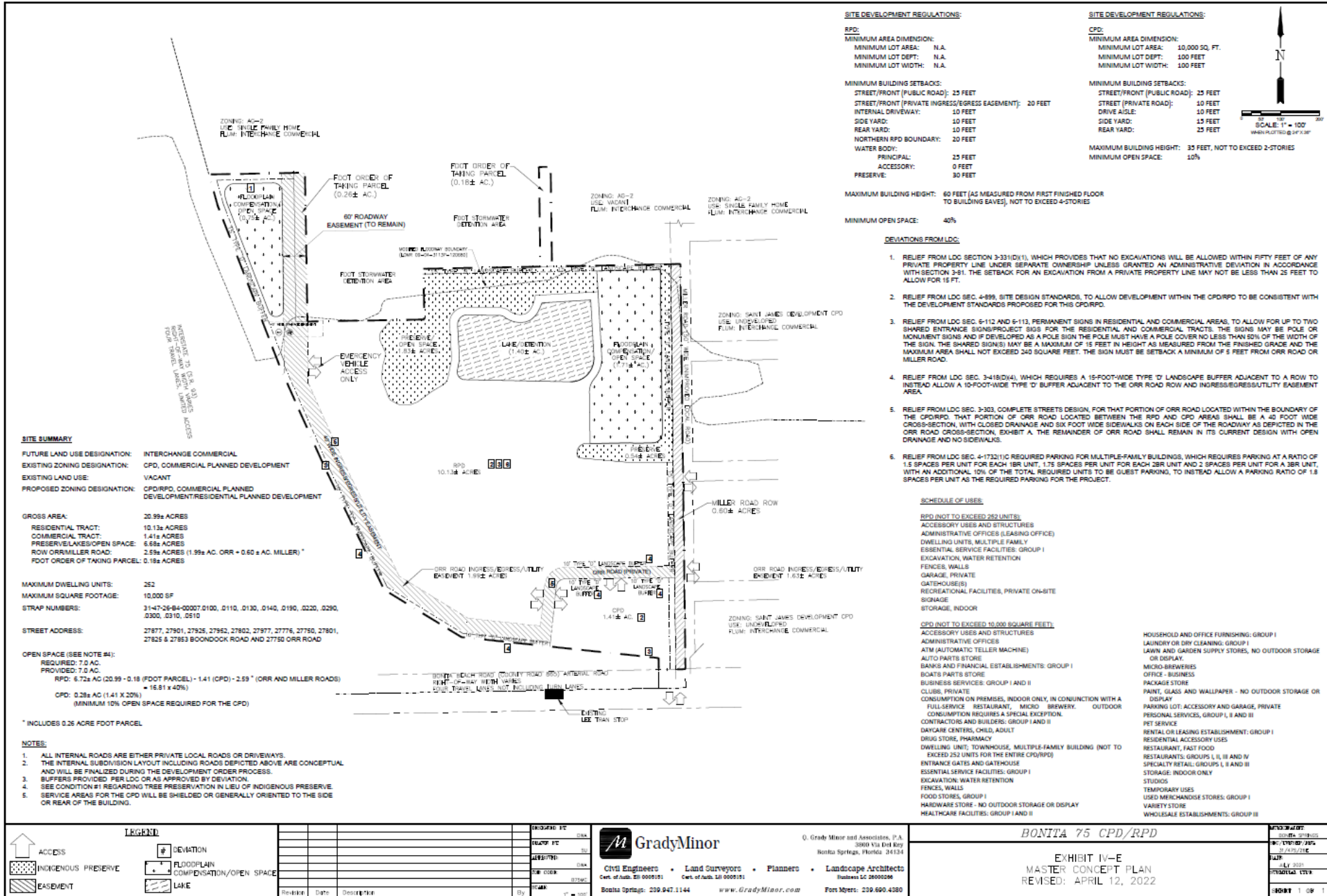
Project Acreage: 21+/- acres

Proposed Request: Amend the CPD to reduce the overall commercial square footage from 158,800 square feet To 10,000 square feet and add a maximum of 252 multi-family residential dwelling units, which is approximately 12 du/ac. The project at the standard maximum density of 10 du/ac is eligible for 209 dwelling units. An affordable housing bonus density agreement will be filed separately for 43 bonus units.

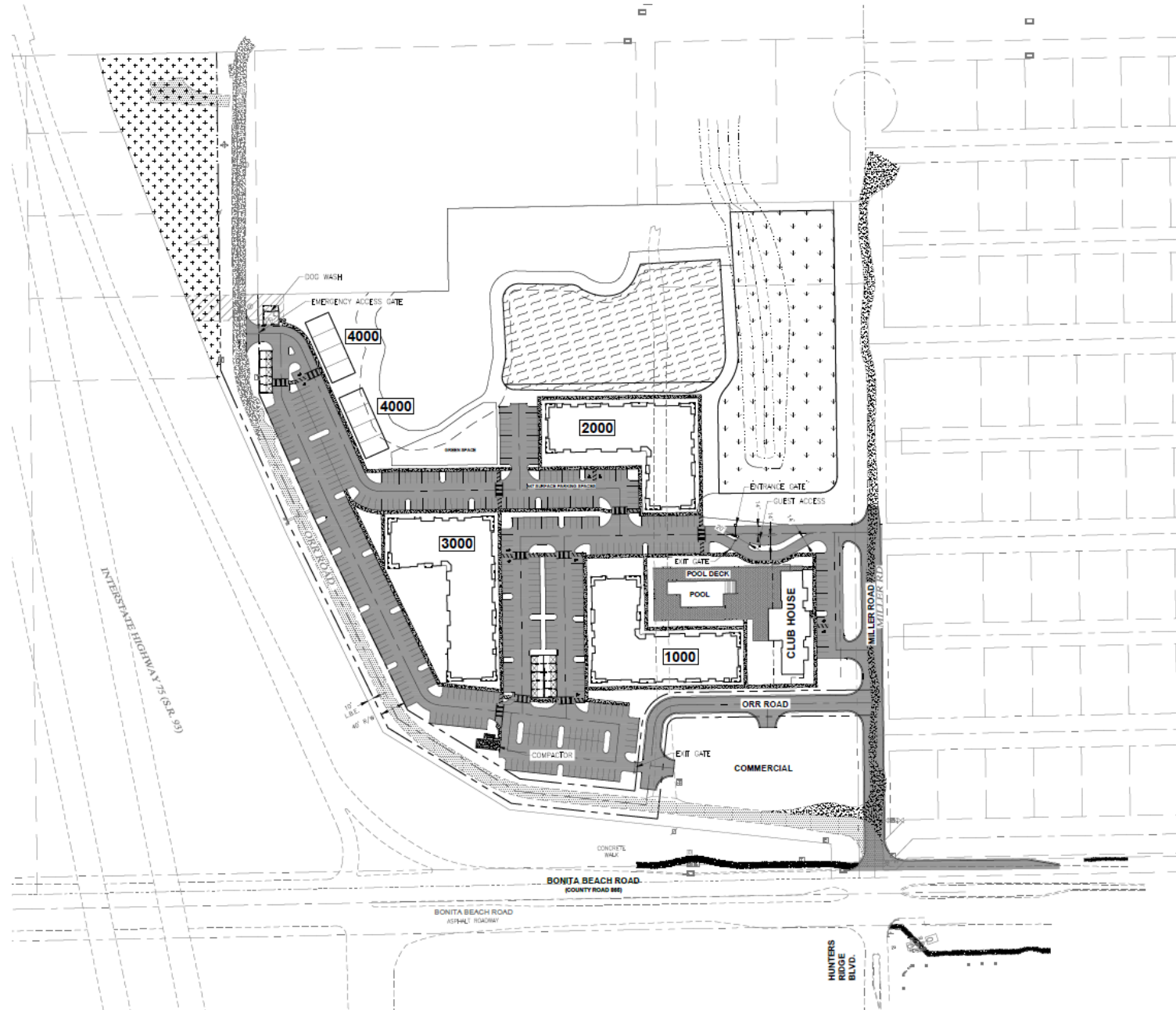
APPROVED MASTER PLAN



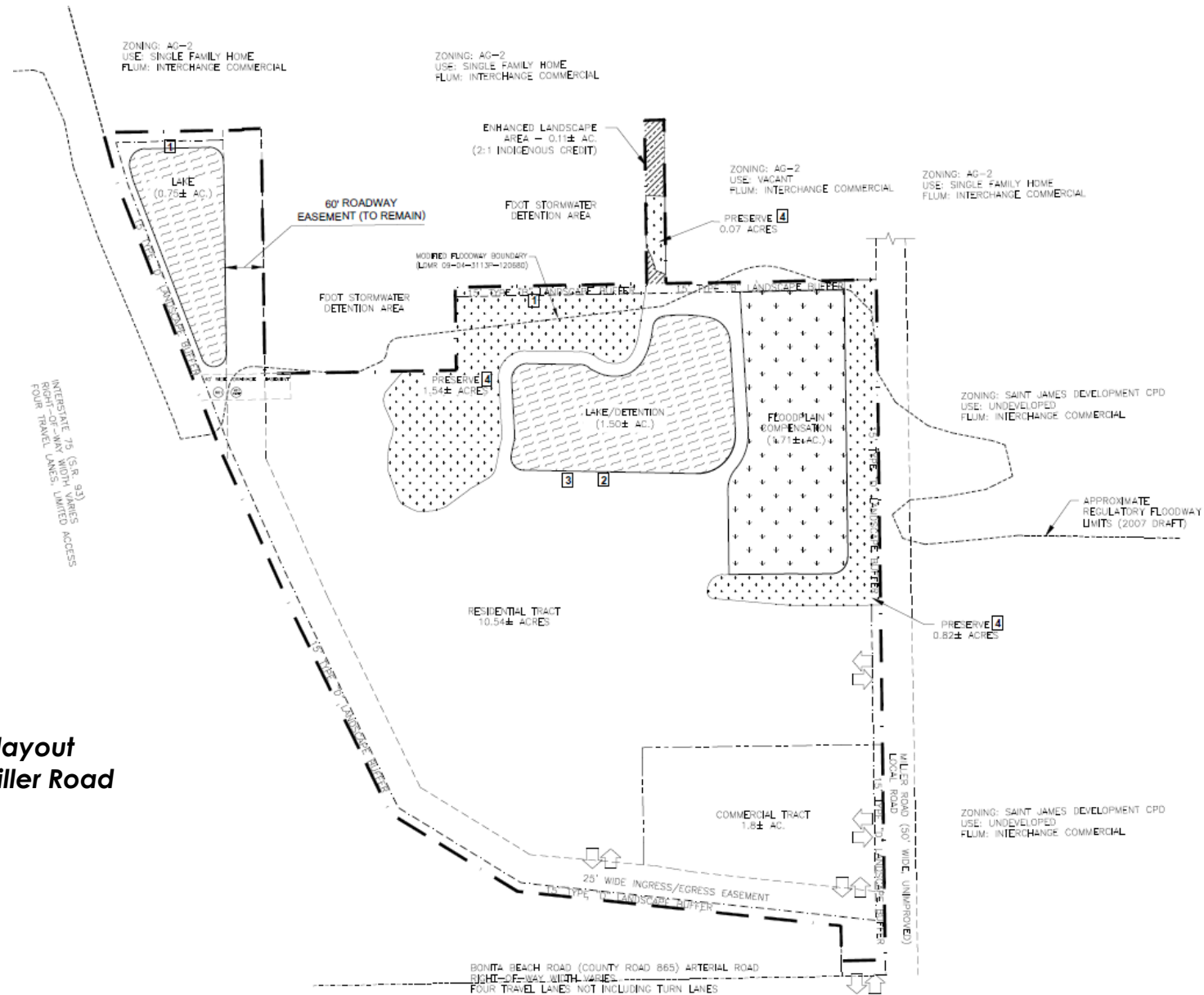
PROPOSED MASTER PLAN



PROPOSED SITE PLAN



ALTERNATIVE SITE PLAN



**Potential alternative site layout
In the event access to Miller Road
Is not granted.**

PROPOSED SCHEDULE OF USES

RPD (NOT TO EXCEED 252 UNITS):

ACCESSORY USES AND STRUCTURES
ADMINISTRATIVE OFFICES (LEASING OFFICE)
DWELLING UNITS, MULTIPLE FAMILY
ESSENTIAL SERVICE FACILITIES: GROUP I
EXCAVATION, WATER RETENTION
FENCES, WALLS
GARAGE, PRIVATE
GATEHOUSE(S)
RECREATIONAL FACILITIES, PRIVATE ON-SITE
SIGNAGE
STORAGE, INDOOR

CPD (NOT TO EXCEED 10,000 SQUARE FEET):

ACCESSORY USES AND STRUCTURES
ADMINISTRATIVE OFFICES
ATM (AUTOMATIC TELLER MACHINE)
AUTO PARTS STORE
BANKS AND FINANCIAL ESTABLISHMENTS: GROUP I
BOATS PARTS STORE
BUSINESS SERVICES: GROUP I AND II
CLUBS, PRIVATE
CONSUMPTION ON PREMISES, INDOOR ONLY, IN CONJUNCTION WITH A
FULL-SERVICE RESTAURANT, MICRO BREWERY. OUTDOOR
CONSUMPTION REQUIRES A SPECIAL EXCEPTION.
CONTRACTORS AND BUILDERS: GROUP I AND II
DAYCARE CENTERS, CHILD, ADULT
DRUG STORE, PHARMACY
DWELLING UNIT; TOWNHOUSE, MULTIPLE-FAMILY BUILDING (NOT TO
EXCEED 252 UNITS FOR THE ENTIRE CPD/RPD)
ENTRANCE GATES AND GATEHOUSE
ESSENTIAL SERVICE FACILITIES: GROUP I
EXCAVATION: WATER RETENTION
FENCES, WALLS
FOOD STORES, GROUP I
HARDWARE STORE - NO OUTDOOR STORAGE OR DISPLAY
HEALTHCARE FACILITIES: GROUP I AND II

HOUSEHOLD AND OFFICE FURNISHING: GROUP I
LAUNDRY OR DRY CLEANING: GROUP I
LAWN AND GARDEN SUPPLY STORES, NO OUTDOOR STORAGE
OR DISPLAY.
MICRO-BREWERIES
OFFICE - BUSINESS
PACKAGE STORE
PAINT, GLASS AND WALLPAPER - NO OUTDOOR STORAGE OR
DISPLAY
PARKING LOT: ACCESSORY AND GARAGE, PRIVATE
PERSONAL SERVICES, GROUP I, II AND III
PET SERVICE
RENTAL OR LEASING ESTABLISHMENT: GROUP I
RESIDENTIAL ACCESSORY USES
RESTAURANT, FAST FOOD
RESTAURANTS: GROUPS I, II, III AND IV
SPECIALTY RETAIL: GROUPS I, II AND III
STORAGE: INDOOR ONLY
STUDIOS
TEMPORARY USES
USED MERCHANDISE STORES: GROUP I
VARIETY STORE
WHOLESALE ESTABLISHMENTS: GROUP III

SITE DEVELOPMENT REGULATIONS

RPD:

MINIMUM AREA DIMENSION:

MINIMUM LOT AREA:	N.A.
MINIMUM LOT DEPT:	N.A.
MINIMUM LOT WIDTH:	N.A.

MINIMUM BUILDING SETBACKS:

STREET/FRONT (PUBLIC ROAD):	25 FEET
STREET/FRONT (PRIVATE INGRESS/EGRESS EASEMENT):	20 FEET
INTERNAL DRIVEWAY:	10 FEET
SIDE YARD:	10 FEET
REAR YARD:	10 FEET
NORTHERN RPD BOUNDARY:	20 FEET
WATER BODY:	
PRINCIPAL:	25 FEET
ACCESSORY:	0 FEET
PRESERVE:	30 FEET

MAXIMUM BUILDING HEIGHT: 60 FEET (AS MEASURED FROM FIRST FINISHED FLOOR TO BUILDING EAVES), NOT TO EXCEED 4-STORIES

MINIMUM OPEN SPACE: 40%

CPD:

MINIMUM AREA DIMENSION:

MINIMUM LOT AREA:	10,000 SQ. FT.
MINIMUM LOT DEPT:	100 FEET
MINIMUM LOT WIDTH:	100 FEET

MINIMUM BUILDING SETBACKS:

STREET/FRONT (PUBLIC ROAD):	25 FEET
STREET (PRIVATE ROAD):	10 FEET
DRIVE AISLE:	10 FEET
SIDE YARD:	15 FEET
REAR YARD:	25 FEET

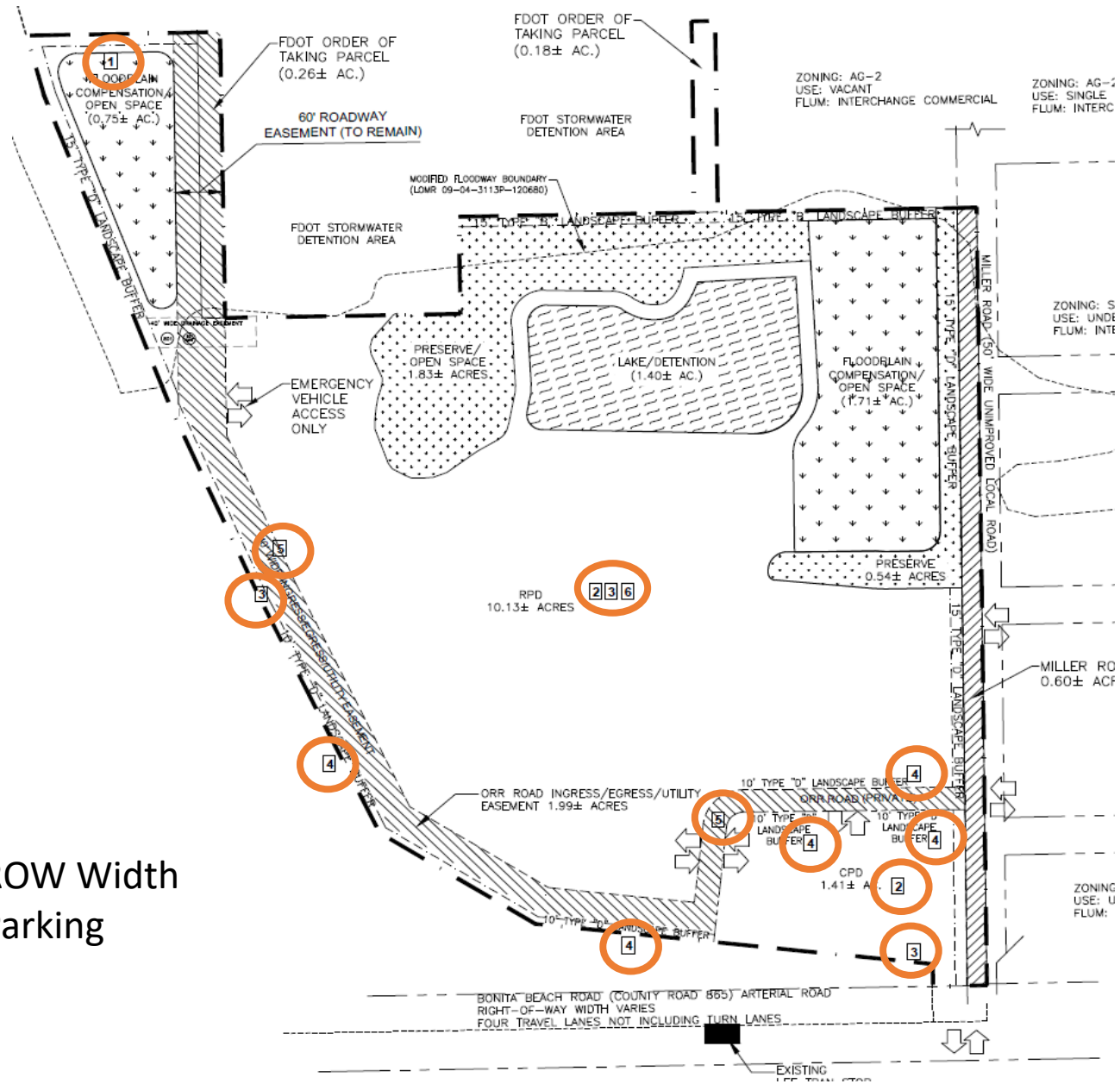
MAXIMUM BUILDING HEIGHT: 35 FEET, NOT TO EXCEED 2-STORIES

MINIMUM OPEN SPACE: 10%

PROPOSED DEVIATIONS FROM THE LDC

1. RELIEF FROM LDC SECTION 3-331(D)(1), WHICH PROVIDES THAT NO EXCAVATIONS WILL BE ALLOWED WITHIN FIFTY FEET OF ANY PRIVATE PROPERTY LINE UNDER SEPARATE OWNERSHIP UNLESS GRANTED AN ADMINISTRATIVE DEVIATION IN ACCORDANCE WITH SECTION 3-81. THE SETBACK FOR AN EXCAVATION FROM A PRIVATE PROPERTY LINE MAY NOT BE LESS THAN 25 FEET TO ALLOW FOR 15 FT.
2. RELIEF FROM LDC SEC. 4-899, SITE DESIGN STANDARDS, TO ALLOW DEVELOPMENT WITHIN THE CPD/RPD TO BE CONSISTENT WITH THE DEVELOPMENT STANDARDS PROPOSED FOR THIS CPD/RPD.
3. RELIEF FROM LDC SEC. 6-112 AND 6-113, PERMANENT SIGNS IN RESIDENTIAL AND COMMERCIAL AREAS, TO ALLOW FOR UP TO TWO SHARED ENTRANCE SIGNS/PROJECT SIGNS FOR THE RESIDENTIAL AND COMMERCIAL TRACTS. THE SIGNS MAY BE POLE OR MONUMENT SIGNS AND IF DEVELOPED AS A POLE SIGN THE POLE MUST HAVE A POLE COVER NO LESS THAN 50% OF THE WIDTH OF THE SIGN. THE SHARED SIGN(S) MAY BE A MAXIMUM OF 15 FEET IN HEIGHT AS MEASURED FROM THE FINISHED GRADE AND THE MAXIMUM AREA SHALL NOT EXCEED 240 SQUARE FEET. THE SIGN MUST BE SETBACK A MINIMUM OF 5 FEET FROM ORR ROAD OR MILLER ROAD.
4. RELIEF FROM LDC SEC. 3-418(D)(4), WHICH REQUIRES A 15-FOOT-WIDE TYPE 'D' LANDSCAPE BUFFER ADJACENT TO A ROW TO INSTEAD ALLOW A 10-FOOT-WIDE TYPE 'D' BUFFER ADJACENT TO THE ORR ROAD ROW AND INGRESS/EGRESS/UTILITY EASEMENT AREA.
5. RELIEF FROM LDC SEC. 3-303, COMPLETE STREETS DESIGN, FOR THAT PORTION OF ORR ROAD LOCATED WITHIN THE BOUNDARY OF THE CPD/RPD. THAT PORTION OF ORR ROAD LOCATED BETWEEN THE RPD AND CPD AREAS SHALL BE A 40 FOOT WIDE CROSS-SECTION, WITH CLOSED DRAINAGE AND SIX FOOT WIDE SIDEWALKS ON EACH SIDE OF THE ROADWAY AS DEPICTED IN THE ORR ROAD CROSS-SECTION, EXHIBIT A. THE REMAINDER OF ORR ROAD SHALL REMAIN IN ITS CURRENT DESIGN WITH OPEN DRAINAGE AND NO SIDEWALKS.
6. RELIEF FROM LDC SEC. 4-1732(1)C REQUIRED PARKING FOR MULTIPLE-FAMILY BUILDINGS, WHICH REQUIRES PARKING AT A RATIO OF 1.5 SPACES PER UNIT FOR EACH 1BR UNIT, 1.75 SPACES PER UNIT FOR EACH 2BR UNIT AND 2 SPACES PER UNIT FOR A 3BR UNIT, WITH AN ADDITIONAL 10% OF THE TOTAL REQUIRED UNITS TO BE GUEST PARKING, TO INSTEAD ALLOW A PARKING RATIO OF 1.8 SPACES PER UNIT AS THE REQUIRED PARKING FOR THE PROJECT.

DEVIATION LOCATIONS



- 1 Excavation
- 2 Site Design
- 3 Signs
- 4 Buffers
- 5 Private Road ROW Width
- 6 Multi-Family Parking

CONCEPTUAL MULTI-FAMILY RENDERINGS

dwell
design
studio

MADISON
CAPITAL GROUP



0454

CONCEPTUAL CLUBHOUSE RENDERINGS



CONCLUSION

Project information and a copy of this presentation can be found on our website:



www.gradyminor.com/Planning

Documents and information can be found online:

- Gradyminor.com/Planning
- *Community Development Public Portal:*
<https://cityofbonitaspringscd.org/welcome/community2/>



Next Steps

- *Zoning Board Hearing: TBD*
- *City Council: TBD*

Contact:

- **Q. Grady Minor & Associates, P.A.:** Sharon Umpenhour sumpenhour@gradyminor.com or 239.947.1144 extension 1249



CITY HALL, ROOM 118
9101 BONITA BEACH ROAD
BONITA SPRINGS, FLORIDA 34135

**TREE ADVISORY BOARD
MEETING MINUTES**
Monday, May 9, 2022
4:00 p.m.

I. Call to Order

Chairman Brown called the meeting to order at 4:00 pm.

II. Roll Call

Chairman Jack Brown and Board Members David Dore-Smith, Barbara Ogle, Suzy Valentine, Altony Lee and Phil Buck were in attendance.

Also in Attendance: Mayor Rick Steinmeyer, Carly Sanseverino and Laura Gibson

III. Public Comment

None.

IV. Approval of Minutes: April 11, 2022

Board Member Lee motioned to approve and passed unanimously.

V. New Business:

a. **Prestige Marble (DOS22-88322-BOS) – 25181 Bernwood Drive**

Austin Butts, Boral Engineering gave an overview of the project. There are two heritage trees on site that will need to be removed and replaced. This site was a previous project in 2019, Royal Scoop that did not go through.

Laura Gibson stated staff had no objection to the project. Chairman Brown asked if the mitigated trees are the right size and caliber. Ms. Gibson stated they were.

*Board Member **Valentine/Ogle** motioned to approve and passed unanimously.*

b. Official Site Visit Summary - Bonita I-75 Mixed Use Planned Development – Residential Commercial Project – PDI21-83966 BOS, 27801 – 27977 Boondock Road

Mike Delate, Engineer with Q. Grady Minor gave an overview of the project. Located at the North-East corner of Bonita Beach Road and I-75 and is currently zoned mix commercial use. They are asking for a rezone for commercial/residential mix use for apartments/condos, about 240 units. They will need to mitigate 12 heritage trees.

Laura Gibson stated she did visit the site to identify the location of a Laurel Oak. She also found a smaller live oak that staff is asking to retain both as part of the mitigation.

Board Member Valentine/Ogle was concerned with congestion and the number of parking spaces. Discussion was held regarding requirements for parking.

Board Member Dore-Smith made a motion to approve conceptually the removal of 12 heritage trees, bring back design plans, follow-up with staff regarding parking and bring back development order for mitigation. Passed 5-1. (Board Member Lee opposed)

VI. Staff Updates

- a. Laura Gibson sent the Chairman a nomination for “Tree of the Season”, a Paradise Tree.

at 40:46: Board Member Valentine suggested having the winner announced at a City Council Meeting. Discussion was held.

Member Dore-Smith motioned to approve the Paradise Tree as the first “Tree of the Season”. Passed Unanimously.

- b. Laura Gibson spoke to the Board regarding updating the code.

VII. Board Member Items

- a. Chairman Brown stated the poster contest was a success. Board would like the winners displayed in City Hall.

VIII. Establish next meeting: June 13, 2022 at 4:00pm.

IX. Adjournment

Respectfully submitted,

Teresa Cannon, Recording Secretary

APPROVED BY CITY OF BONITA SPRINGS TREE ADVISORY BOARD ON THE ____ DAY OF _____, 2022.

AUTHENTICATED:

Chairman Jack Brown

MEMORANDUM

TO: Mr. Wayne Arnold
Q. Grady Minor & Associates

FROM: Ted Treesh, PTP
President

Yury Bykau, P.E.
Transportation Consultant

DATE: June 13, 2022

RE: Bonita 75 CPDA
Bonita Beach Road & Trade Way One – Left Turn Lane Analysis
City of Bonita Springs, Florida

TR Transportation Consultants, Inc. has conducted a traffic analysis for the existing eastbound left turn lane at Bonita Beach Road and Trade Way One intersection located in the City of Bonita Springs, Florida. The traffic analysis as part of this Memorandum will discuss the impacts to the eastbound turn lane length at Trade Way One intersection as a result of the potential closing of the existing full median opening at the Bonita Beach Road and Hunters Ridge Boulevard intersection.

Attached **Figure 1A** illustrates the existing left turn and U-turn traffic at the Bonita Beach Road and Trade Way One intersection based on traffic counts conducted at this location on June 2, 2022.

Attached **Figure 2A** illustrates the reassignment of the Bonita 75 project related trips with the Hunters Ridge Boulevard median opening closed scenario. This figure was obtained from the traffic study prepared for the Bonita 75 CPDA project, dated December 2, 2021.

Attached **Figure 3A** illustrates the reassignment of the existing northbound to westbound left turning traffic on Hunters Ridge Boulevard at Bonita Beach Road to the Trade Way One intersection. For this analysis it was estimated that approximately 50% of the existing peak hour northbound left turning traffic (16 AM Pk Hr Vehicles & 23 PM Pk Hr Vehicles) on Hunters Ridge Boulevard northbound approach is anticipated to turn right onto Bonita Beach Road and subsequently make a U-turn at the Bonita Beach Road and Trade Way One intersection. The remaining trips would find alternative routes to travel back west such as utilizing internal roadways to make a direct left or utilize Bonita

Grande Drive to enter Bonita Beach Road. Traffic counts that were conducted at the Bonita Beach Road and Hunters Ridge Boulevard intersection are attached to this Memorandum for reference.

Attached **Figure 4A** (Figure 1A + Figure 2A + Figure 3A) illustrates cumulative left turning traffic anticipated at the Bonita Beach Road and Trade Way One intersection based on the existing full median opening on Hunters Ridge Boulevard closed scenario.

The existing eastbound left turn lane at the Bonita Beach Road and Trade Way One intersection is approximately 305 feet in total length. The projected peak hour left turn volume based on Figure 4A is 159 left/U-turn vehicles in the weekday PM peak hour conditions. Based on the turn lane storage computation utilizing the two-minute arriving formula, it was determined that approximately 150 feet of storage distance should be provided. This turn lane should also provide a taper/decel distance of 240 feet pursuant to *Exhibit 212-1* of the FDOT Design Manual and 50 mph design speed (posted speed limit + 5 mph) for a total turn lane length of 390 feet. Therefore, the existing eastbound left turn lane at this location is insufficient in length and would be required to be lengthened by additional 85 feet in order to accommodate traffic as a result of the potential closing of the existing full median opening at Bonita Beach Road and Hunters Ridge Boulevard intersection. The storage length calculation utilizing the two-minute arrival formula is shown below for reference.

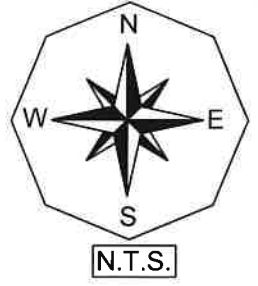
$$\begin{aligned} \text{Storage} &= (1\text{hour}/60\text{ minutes})*(2\text{ minutes})*(159\text{ veh})*25\text{ ft/veh} \\ &= 132.5\text{ ft of storage} \\ &\approx \mathbf{150\text{ ft of storage}} \end{aligned}$$

The Bonita 75 applicant should only be responsible for a proportionate share payment of this improvement as it is related to the closure of the median opening on the off-site roadway.

If you have any additional questions, please do not hesitate to contact us.

Attachments

FIGURES 1A – 4A



BONITA BEACH RD.

I-75

ORR RD.

MILLER RD.

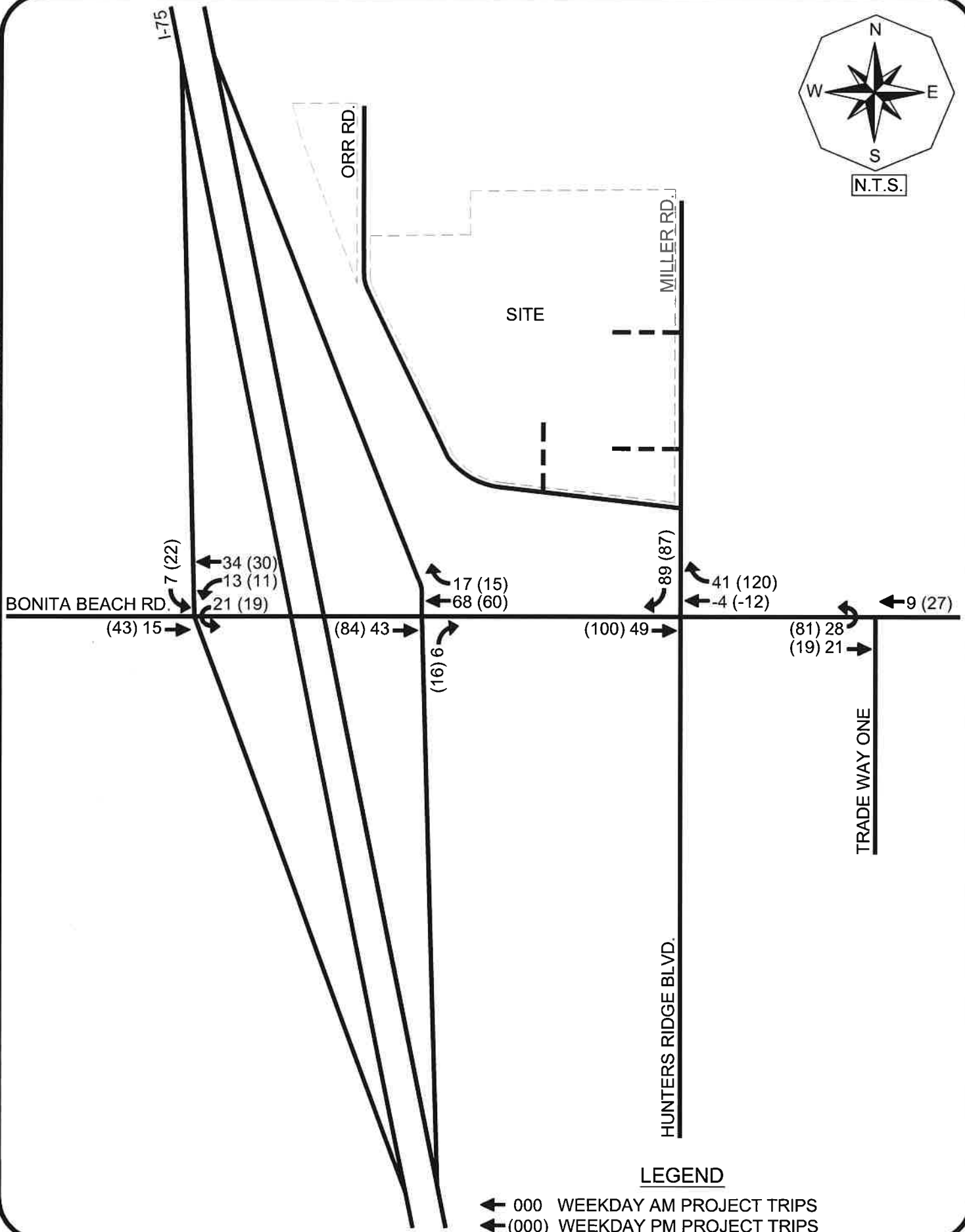
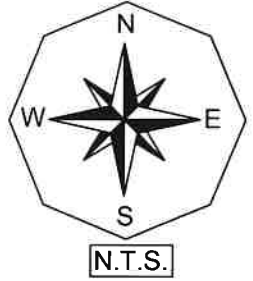
HUNTERS RIDGE BLVD.



LEGEND

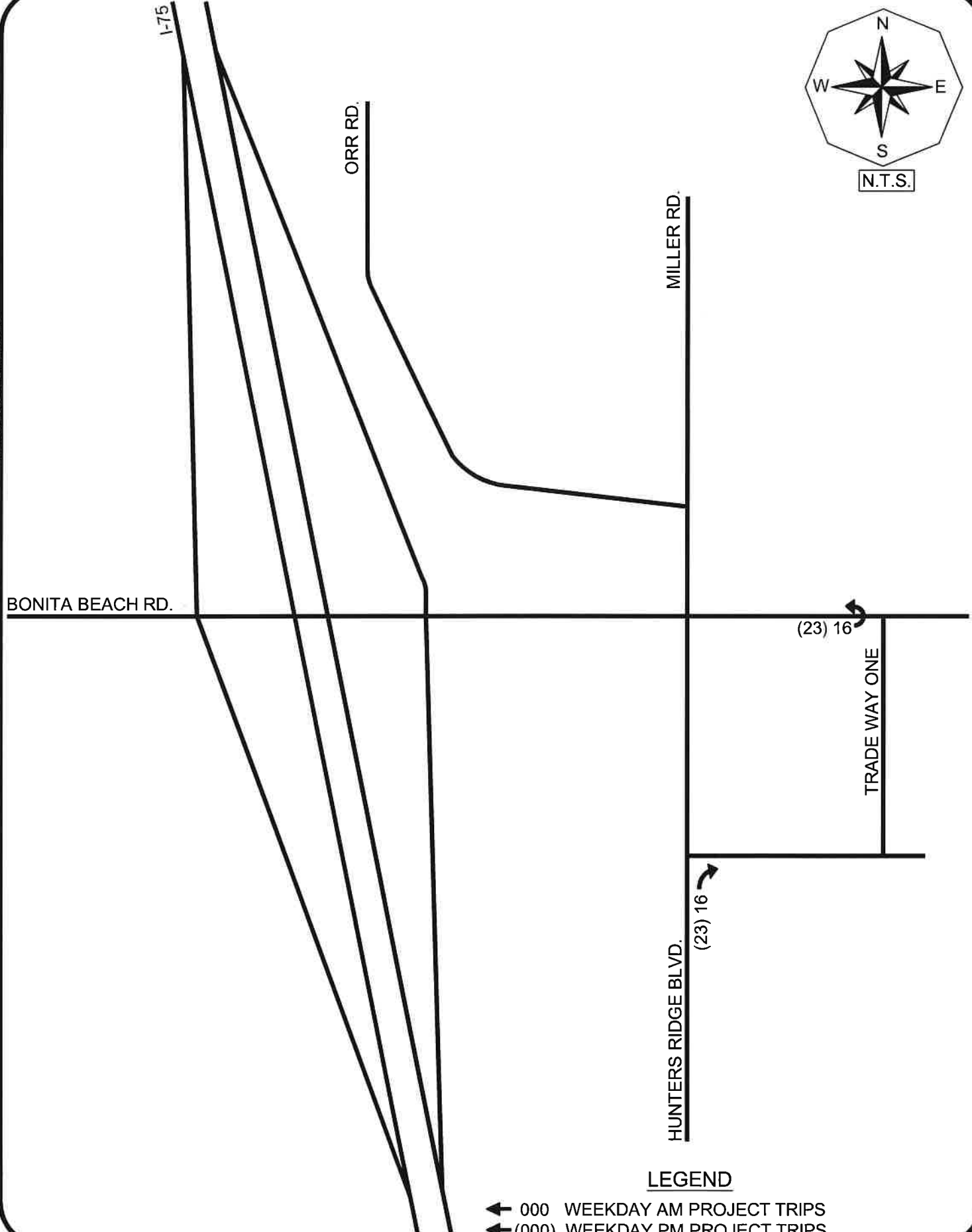
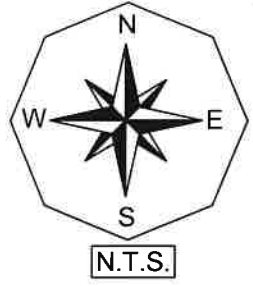
- ← 000 WEEKDAY AM PROJECT TRIPS
- ← (000) WEEKDAY PM PROJECT TRIPS

EXISTING LEFT TURNS
 BASED ON TRAFFIC COUNTS
 BONITA 75 CPDA



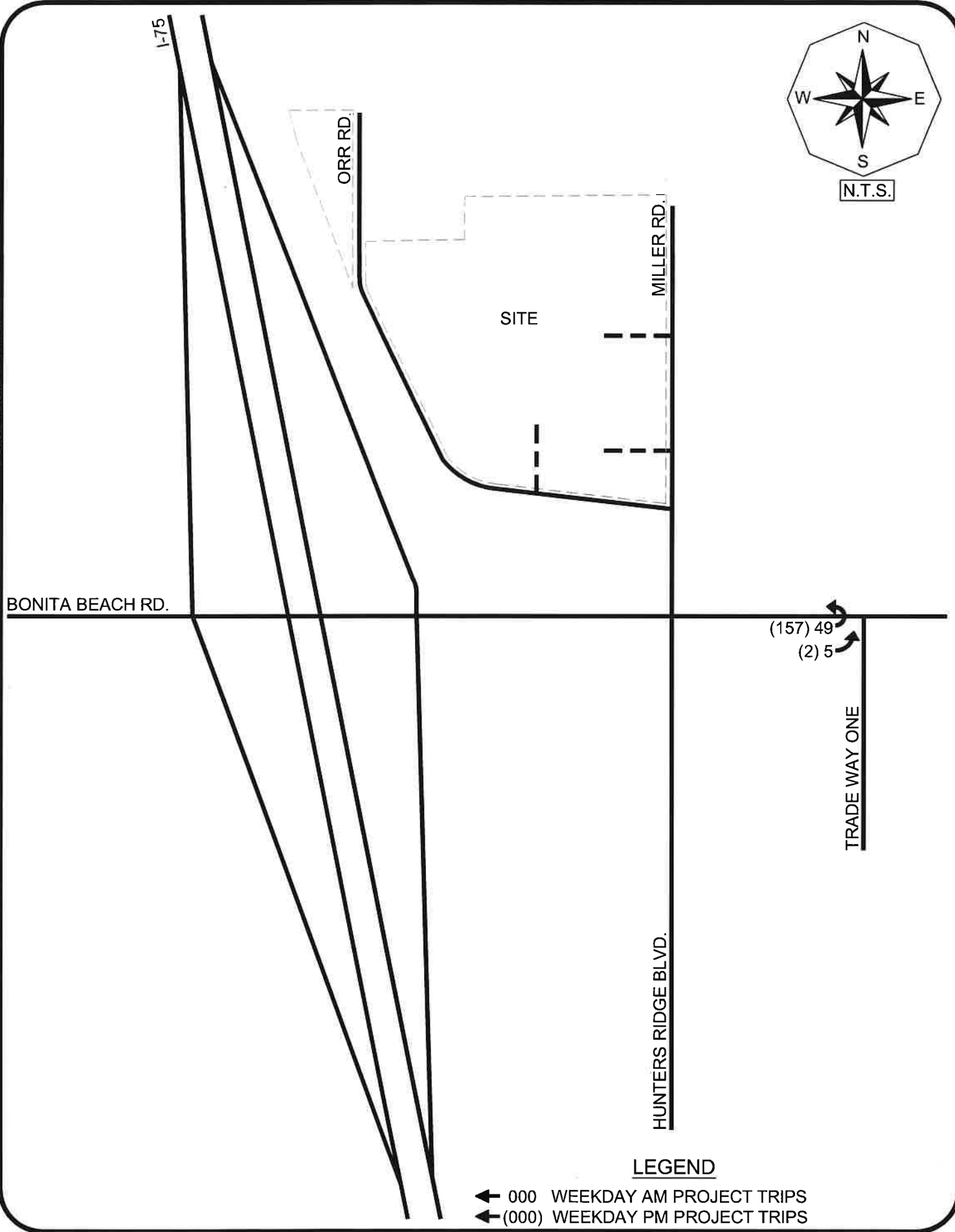
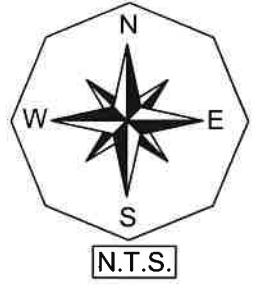
LEGEND

- ← 000 WEEKDAY AM PROJECT TRIPS
- ←(000) WEEKDAY PM PROJECT TRIPS



LEGEND

- ← 000 WEEKDAY AM PROJECT TRIPS
- ← (000) WEEKDAY PM PROJECT TRIPS



TRAFFIC COUNTS
BONITA BEACH ROAD
@ TRADE WAY ONE

Bonita Beach Rd @ Trade Way One 6-2-22 AM

File Name: Bonita Beach Rd @ Trade Way One 6-2-22 AM

Site Code:

Location:

Cars and Peds

Study Date: 06/02/2022

Time	Shared Access Southbound				Bonita Beach Rd Westbound				Trade Way One Northbound				Bonita Beach Rd Eastbound									
	Peds	Right	Thru	Left	U-Turn	Appr Total	Peds	Right	Thru	Left	U-Turn	Appr Total	Peds	Right	Thru	Left	U-Turn	Appr Total	Int Total			
07:00	0	0	0	0	0	0	0	132	3	0	135	4	0	5	0	9	12	108	3	0	123	267
07:15	0	0	0	0	0	0	0	138	9	0	147	5	0	1	0	6	15	115	3	0	133	286
07:30	1	0	0	0	0	1	1	204	4	0	209	3	0	9	0	12	16	150	5	2	173	395
07:45	0	0	0	0	0	0	0	159	3	0	162	7	0	7	0	14	11	166	0	2	179	355
Total	0	1	0	0	0	1	0	633	19	0	653	0	19	0	22	0	54	539	11	4	608	1303
08:00	1	0	0	0	0	1	0	201	1	0	202	6	0	7	0	13	12	162	1	2	177	393
08:15	0	0	0	0	0	0	1	175	2	0	178	2	0	7	0	9	5	163	0	1	169	356
08:30	0	0	0	0	0	0	0	217	3	0	220	0	0	7	0	7	17	178	3	1	199	426
08:45	0	0	0	0	0	0	0	179	4	0	183	7	0	9	0	16	20	161	1	1	183	382
Total	0	1	0	0	0	1	0	772	10	0	783	0	15	0	30	0	54	664	5	5	728	1557
Grand Total	0	2	0	0	0	2	0	1405	29	0	1436	0	34	0	52	0	108	1203	16	9	1336	2860
Appr %	100	0	0	0	0	0	0	97.8	2	0	39.5	0	60.5	0	8.1	0	8.1	90	1.2	0.7	1336	2860
Total %	0.1	0	0	0	0	0	0	49.1	1	0	1.2	0	1.8	0	3.8	0	3.8	42.1	0.6	0.3	728	1557
AM Pk Hr	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00
AM Pk Vol	1	0	0	0	0	1	0	772	10	0	783	0	15	0	30	0	54	664	5	5	728	1557
AM PHF	0.250	NaN	NaN	NaN	NaN	0.250	NaN	0.889	0.625	NaN	0.890	0.536	NaN	0.833	NaN	0.703	0.675	0.933	0.417	0.625	0.915	0.914

Bonita Beach Rd @ Trade Way One 6-2-22 AM

File Name: Bonita Beach Rd @ Trade Way One 6-2-22 AM

Location:

Site Code:

Study Date: 06/02/2022

All Vehicles

Shared Access			
1	0	0	0
Right	Thru	Left	U-Turn

Bonita Beach Rd			
5	5	664	54
U-Turn	Left	Thru	Right

Bonita Beach Rd			
1	772	10	0
Right	Thru	Left	U-Turn

AM Peak Hour Statistics
 AM Peak Hour Begins: 08:00
 AM Peak Hour Volume: 1557
 AM Peak Hour Factor: 0.914

Trade Way One			
0	30	0	15
U-Turn	Left	Thru	Right

Bonita Beach Rd @ Trade Way One 6-2-22 PM

File Name: Bonita Beach Rd @ Trade Way One 6-2-22 PM

Site Code:

Location:

Cars and Peds

Study Date: 06/02/2022

Time	Shared Access Southbound				Bonita Beach Rd Westbound				Trade Way One Northbound				Bonita Beach Rd Eastbound						
	Peds	Right	Thru	Left	U-Turn	Appr Total	Peds	Right	Thru	Left	U-Turn	Appr Total	Peds	Right	Thru	Left	U-Turn	Appr Total	Int Total
16:00	2	0	0	0	0	2	0	154	3	0	157	2	0	0	170	0	11	186	363
16:15	4	0	1	0	0	5	0	149	2	0	151	5	0	4	174	2	15	201	366
16:30	0	0	0	0	0	0	0	132	1	0	133	8	0	20	171	0	19	200	361
16:45	0	0	0	0	0	0	0	127	2	1	130	6	0	16	148	0	8	166	318
Total	6	0	1	0	0	7	0	562	8	1	571	21	0	56	663	2	53	753	1408
17:00	5	0	1	0	0	6	0	146	2	1	149	5	0	19	157	4	1	169	348
17:15	2	0	1	0	0	3	0	168	3	0	171	7	0	6	171	0	3	177	364
17:30	0	0	0	0	0	0	0	141	3	0	144	5	0	3	158	0	1	167	319
17:45	1	0	0	0	0	1	0	130	1	0	131	1	0	5	167	0	0	177	315
Total	8	0	2	0	0	10	0	585	9	1	595	18	0	33	653	4	5	690	1346
Grand Total	0	14	0	3	0	17	0	1147	17	2	1166	39	0	89	1316	6	58	1443	2754
Appr %	82.4	0	17.6	0	0	0	0	98.4	1.5	0.2	30.5	0	69.5	0	4.4	0.4	4	0	0
Total %	0.5	0	0.1	0	0	0	0	41.6	0.6	0.1	1.4	0	3.2	0	2.3	0.2	2.1	0	0
PM Pk Hr	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00
PM Pk Vol	6	0	1	0	0	7	0	562	8	1	571	21	0	56	663	2	53	753	1408
PM PHF	0.375	NaN	0.250	NaN	NaN	0.350	NaN	0.912	0.667	0.250	0.909	0.656	NaN	0.700	0.953	0.250	0.697	0.937	0.962

Bonita Beach Rd @ Trade Way One 6-2-22 PM

File Name: Bonita Beach Rd @ Trade Way One 6-2-22 PM
 Location:

Site Code:
 Study Date: 06/02/2022

All Vehicles

Shared Access				
6	0	1	0	
Right	Thru	Left	U-Turn	

Bonita Beach Rd			
53	2	663	35
U-Turn	Left	Thru	Right

PM Peak Hour Statistics
 PM Peak Hour Begins: 16:00
 PM Peak Hour Volume: 1408
 PM Peak Hour Factor: 0.962

Bonita Beach Rd			
0	562	8	1
Right	Thru	Left	U-Turn

Trade Way One				
0	56	0	21	
U-Turn	Left	Thru	Right	

TRAFFIC COUNTS
BONITA BEACH ROAD
@ HUNTERS RIDGE BOULEVARD

Bonita Beach Rd @ Hunters Ridge Blvd 6-2-22 AM

File Name: Bonita Beach Rd @ Hunters Ridge Blvd 6-2-22

Location: ...

Cars and Peds

Site Code: 06/02/2022
Study Date:

Time	Orr Rd Southbound				Bonita Beach Rd Westbound				Hunters Ridge Blvd Northbound				Bonita Beach Rd Eastbound											
	Peds	Right	Thru	Left	U-Turn	Appr Total	Peds	Right	Thru	Left	U-Turn	Appr Total	Peds	Right	Thru	Left	U-Turn	Appr Total	Int Total					
07:00	0	0	0	0	0	0	0	136	1	0	137	0	0	7	0	0	0	7	13	123	0	0	136	280
07:15	0	0	0	0	0	139	0	139	0	0	139	1	0	5	23	132	0	0	23	132	0	0	155	299
07:30	1	0	0	0	0	215	0	214	1	0	215	3	0	17	16	170	0	0	16	170	0	0	186	419
07:45	0	0	0	0	0	168	0	166	2	0	168	2	0	17	20	177	0	0	20	177	0	0	197	382
Total	0	1	0	0	0	559	0	655	4	0	659	6	0	46	72	602	0	0	72	602	0	0	674	1380
08:00	0	0	0	0	0	210	0	210	0	0	210	2	0	9	12	175	0	0	12	175	0	0	187	406
08:15	0	0	0	0	0	183	0	182	1	0	183	0	0	8	13	169	0	0	13	169	0	0	182	373
08:30	0	0	0	0	0	225	0	224	1	0	225	0	1	12	14	199	0	0	14	199	0	0	213	450
08:45	0	0	0	0	0	189	0	187	2	0	189	2	0	8	8	181	0	0	8	181	0	0	189	386
Total	0	0	0	0	0	807	0	803	4	0	807	4	1	37	47	724	0	0	47	724	0	0	771	1615
Grand Total	0	1	0	0	0	1466	0	1458	8	0	1466	10	1	83	119	1326	0	0	119	1326	0	0	1445	2995
Appr %	100	0	0	0	0	99.5	0.5	99.5	0.5	0	99.5	12	1.2	86.7	8.2	91.8	0	0	8.2	91.8	0	0	771	1615
Total %	0	0	0	0	0	48.7	0.3	48.7	0.3	0	48.7	0.3	0	2.4	4	44.3	0	0	4	44.3	0	0	0.905	0.897
AM Pk Hr	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00	08:00
AM Pk Vol	0	0	0	0	0	807	4	803	4	0	807	4	1	37	47	724	0	0	47	724	0	0	771	1615
AM PHF	NaN	NaN	NaN	NaN	NaN	0.896	0.500	0.896	0.500	NaN	0.897	0.500	0.250	0.727	0.839	0.910	NaN	NaN	0.839	0.910	NaN	NaN	0.905	0.897

Bonita Beach Rd @ Hunters Ridge Blvd 6-2-22 AM

File Name: Bonita Beach Rd @ Hunters Ridge Blvd 6-2-22
 Location: ****

Site Code:
 Study Date: 06/02/2022

All Vehicles

Orr Rd				
0	0	0	0	0
Right	Thru	Left	U-Turn	

Bonita Beach Rd				
47	0	724	0	
Right	Left	Thru	U-Turn	

Bonita Beach Rd				
0	803	4	0	
Right	Thru	Left	U-Turn	

AM Peak Hour Statistics
 AM Peak Hour Begins: 08:00
 AM Peak Hour Volume: 1615
 AM Peak Hour Factor: 0.897

Hunters Ridge Blvd				
0	32	1	4	
U-Turn	Left	Thru	Right	

Bonita Beach Rd @ Hunters Ridge Blvd 6-2-22 PM

File Name: Bonita Beach Rd @ Hunters Ridge Blvd 6-2-22

Site Code:

Location:

Cars and Peds

Study Date: 06/02/2022

Time	Orr Rd Southbound				Bonita Beach Rd Westbound				Hunters Ridge Blvd Northbound				Bonita Beach Rd Eastbound														
	Peds	Right	Thru	Left	U-Turn	Appr Total	Peds	Right	Thru	Left	U-Turn	Appr Total	Peds	Right	Thru	Left	U-Turn	Appr Total	Int Total								
16:00	2	0	0	0	0	2	0	0	0	0	181	0	0	0	181	3	0	8	0	11	14	183	2	4	203	397	
16:15	0	0	0	0	0	0	0	0	1	168	0	0	0	0	168	4	0	12	0	16	19	196	0	1	216	400	
16:30	0	0	0	0	0	0	0	0	1	170	0	0	0	0	171	4	0	12	0	16	8	196	1	3	208	395	
16:45	2	0	0	0	0	2	0	0	0	149	2	0	0	0	151	3	0	14	0	17	16	163	1	1	181	351	
Total	4	0	0	0	0	4	0	0	1	667	3	1	0	14	671	0	46	0	60	57	738	4	9	808	1543		
17:00	0	0	1	0	0	1	0	0	0	168	3	0	0	0	171	4	0	25	0	29	10	165	0	1	176	377	
17:15	0	0	0	0	0	0	0	1	1	175	2	1	0	0	179	4	0	11	0	15	17	172	0	0	189	383	
17:30	1	0	0	0	0	1	0	0	1	144	0	1	0	0	145	0	0	9	0	9	13	166	0	1	180	335	
17:45	0	0	0	0	0	0	0	0	0	133	3	0	0	1	136	1	0	10	0	11	15	176	0	0	191	338	
Total	1	0	1	0	0	2	0	1	2	620	8	2	0	9	631	0	55	0	64	55	679	0	2	736	1433		
Grand Total	0	5	0	1	0	6	0	1	3	1302	11	3	0	23	1302	0	101	0	124	0	112	1417	4	11	1544	2976	
Appr %	83.3	0	16.7	0	0	0	0	0.1	0.2	98.8	0.8	0.2	18.5	0	0	81.5	0	0	0	7.3	91.8	0.3	0.7	0.7	0.7	0.7	
Total %	0.2	0	0	0	0	0	0	0	0.1	43.2	0.4	0.1	0.8	0	0	3.4	0	0	0	3.8	47.6	0.1	0.4	0.4	0.4	0.4	
PM Pk Hr	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00	16:00
PM Pk Vol	4	0	0	0	4	0	0	0	1	667	3	1	14	0	671	14	46	0	60	57	738	4	9	808	1543	1543	
PM PHF	0.500	NaN	NaN	NaN	0.500	0.375	0.250	0.927	0.882	0.921	0.375	0.250	0.875	NaN	0.927	0.821	NaN	0.882	0.882	0.750	0.941	0.500	0.563	0.935	0.935	0.964	

Bonita Beach Rd @ Hunters Ridge Blvd 6-2-22 PM

File Name: Bonita Beach Rd @ Hunters Ridge Blvd 6-2-22
 Location:

Site Code:
 Study Date: 06/02/2022

All Vehicles

Orr Rd				
4	0	0	0	0
Right	Thru	Left	U-Turn	

Bonita Beach Rd				
9	4	738	57	
U-Turn	Left	Thru	Right	

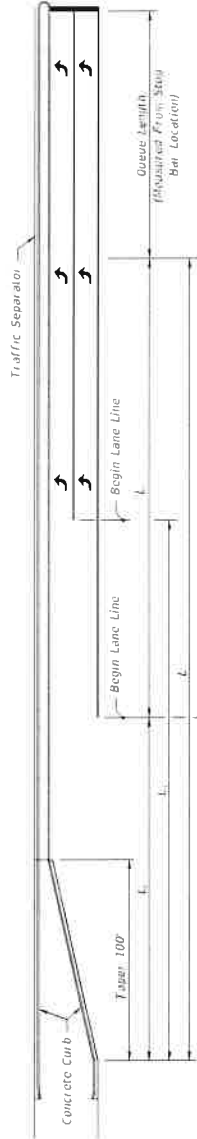
PM Peak Hour Statistics
 PM Peak Hour Begins: 16:00
 PM Peak Hour Volume: 1543
 PM Peak Hour Factor: 0.964

Bonita Beach Rd				
0	667	3	1	
Right	Thru	Left	U-Turn	

Hunters Ridge Blvd				
0	46	0	14	
U-Turn	Left	Thru	Right	

EXHIBIT 212-1
FDOT DESIGN MANUAL

MEDIAN TURN LANES MINIMUM DECELERATION LENGTHS



DOUBLE LEFT TURN

Brakes Applied After Turning
Vehicle Clears Through Lane;
Entry Speed:
10 mph Below Design Speed
For Urban Condition
Average Running Speed For
Rural Condition



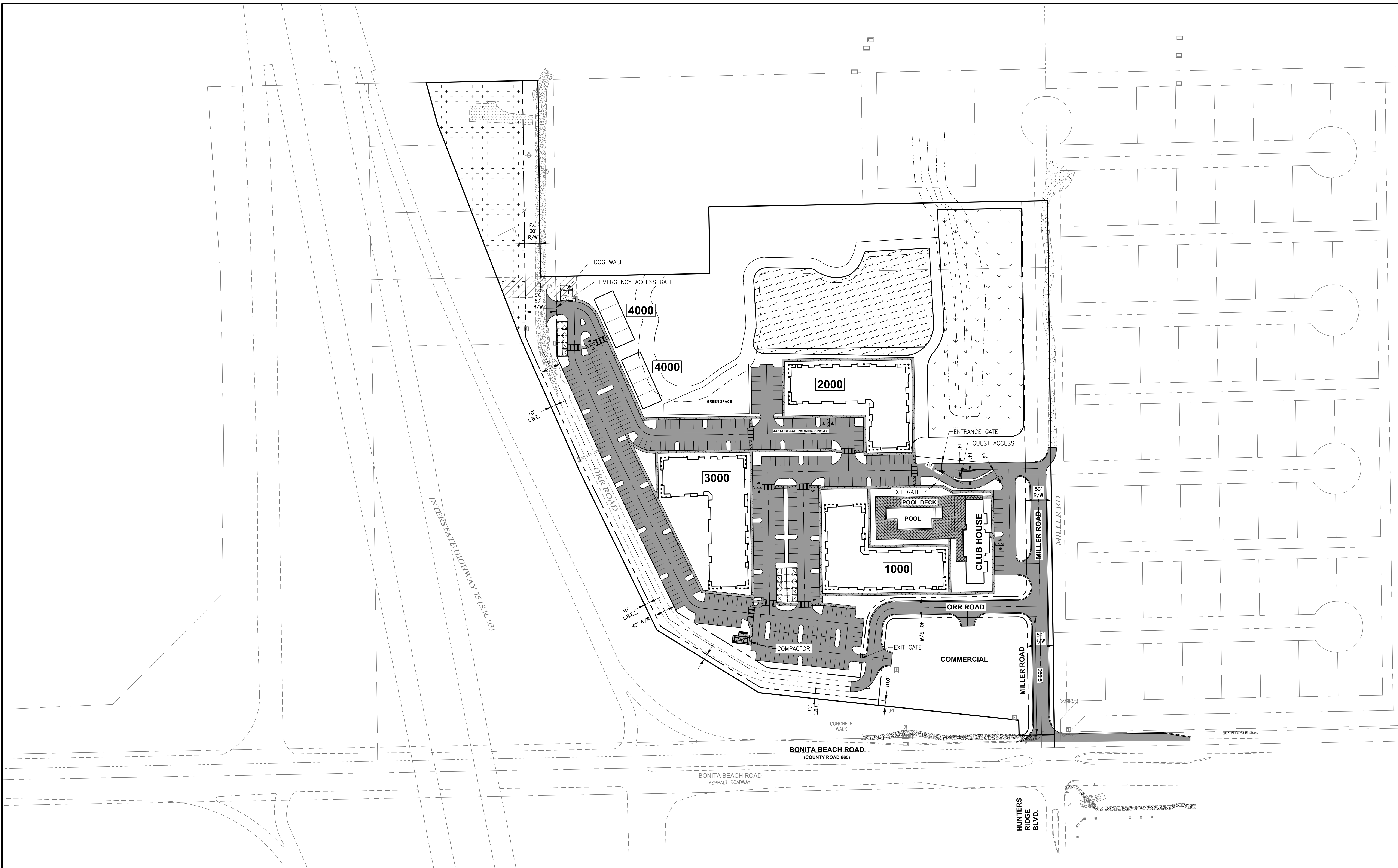
SINGLE LEFT TURN

Brakes Applied After Turning
Vehicle Clears Through Lane;
Entry Speed:
10 mph Below Design Speed
For Urban Condition
Average Running Speed For
Rural Condition

MEDIAN TURN LANES									
Design Speed (mph)	Clearance Distance L, (ft.)	URBAN CONDITIONS			RURAL CONDITIONS			Total Decel. Distance L, (ft.)	Clearance Distance L, (ft.)
		Brake To Stop Distance L, (ft.)	Total Decel. Distance L, (ft.)	Clearance Distance L, (ft.)	Brake To Stop Distance L, (ft.)	Total Decel. Distance L, (ft.)	Clearance Distance L, (ft.)		
35	25	70	75	145	110	—	—	—	—
40	30	80	75	155	120	—	—	—	—
45	35	85	100	185	135	—	—	—	—
50	40/44	105	135	240	160	185	290	160	160
55	48	125	—	—	—	225	350	195	195
60	52	145	—	—	—	260	405	230	230
65	55	170	—	—	—	290	460	270	270

NOT TO SCALE

EXHIBIT 212-1
01/01/2018



BONITA 75
CONCEPT PLAN

